

**Before the Housing and Community Development Chief of Planning
in and for the County of Monterey, State of California**

In the matter of the application of:

FOWLER JOHN F & ANNA M TRS (PLN220217)

RESOLUTION NO. 24 - 024

Resolution by the Monterey County HCD Chief of Planning:

- 1) Finding that the project qualifies for a Class 3 Categorical Exemption pursuant to CEQA Guidelines section 15303, and there are no exceptions pursuant to section 15300.2; and
- 2) Approving a Coastal Administrative Permit to allow less than 100 cubic yards of grading on slopes in excess of 30 percent and a Design Approval to allow construction of 355 square foot storage structure and associated site improvements including an auto court and retaining walls.

[PLN220217, FOWLER JOHN F & ANNA M TRS,
46402 Pear Valley Road, Big Sur, Big Sur Coast
Land Use Plan (APN: 419-231-005-000)]

The FOWLER JOHN F & ANNA M TRS application (PLN220217) came on for an administrative decision before the Monterey County HCD Chief of Planning on June 5, 2024. Having considered all the written and documentary evidence, the administrative record, the staff report, oral testimony, and other evidence presented, the Monterey County HCD Chief of Planning finds and decides as follows:

FINDINGS

1. **FINDING:** **CONSISTENCY** – The Project, as conditioned, is consistent with the applicable plans and policies which designate this area as appropriate for development.
EVIDENCE: a) During the course of review of this application, the project has been reviewed for consistency with the text, policies, and regulations in:
 - the 1982 Monterey County General Plan;
 - Big Sur Coast Land Use Plan (BSC LUP);
 - Monterey County Coastal Implementation Plan, Part 3 (Big Sur CIP); and
 - Monterey County Zoning Ordinance (Title 20).No conflicts were found to exist. No communications were received during the course of review of the project indicating any inconsistencies with the text, policies, and regulations in these documents.
b) Allowed Use. The property is located at 46402 Pear Valley Road, Big Sur (Assessor's Parcel Number [APN]: 419-231-005-000), Big Sur Coast Land Use Plan. The parcel is zoned Watershed and Scenic Conservation, 40 acres per unit, Design Control overlay district, within

the Coastal Zone or “WSC/40-D(CZ)”, which allows for the construction of an accessory non-habitable structure, subject to the granting of a Coastal Administrative Permit and unless exempt under Title 20 section 20.70.120. Title 20 section 20.70.120.A, exempts the establishment of non-habitable accessory structures less than 1,000 square feet from the requirement of obtaining a Coastal Development Permit, or in this case, a Coastal Administrative Permit. However, a Design Approval is required for all development within the Design Control District. Additionally, the proposed project involves development on slopes in excess of 30 percent that meet the criteria of Title 20 section 20.64.230.C, and therefore also requires the granting of a Coastal administrative Permit. Accordingly, the project is an allowed land use for this site.

- c) Lot Legality. The subject property (9.95 acres) is identified in its current configuration as “Parcel A” in a January 1974 “Parcel Map” illustrating the “Division of the Southeast Quarter of the Northeast Quarter of Section 26, T.19S, R.1.E” (Minor Subdivision No. MS-73-372). Additionally, the subject property is developed with a single-family dwelling (HCD- Planning File No. PC06869). Therefore, the County recognizes the subject property as a legal lot of record
- d) Design/Neighborhood and Critical Viewshed. Pursuant to Title 20, Chapter 20.44, the project site and surrounding area are designated as a Design Control Zoning District (“D” zoning overlay), which is intended to regulate the location, size, configuration, materials, and colors of structures and fences to assure the protection of the public viewshed and neighborhood character. The 8.5 foot-high non-habitable storage structure will be partially set into the adjacent hillside and consist of a stone-clad exterior, steel door, and a vegetated roof. As stated, the proposed natural materials and vegetated roof will blend in with the surrounding hillside. The proposed project does not create any new visible structure within the Critical Viewshed (Big Sur Coast LUP Policy 3.2.2). Distance and topography effectively screen the proposed development from Highway 1. Additionally, the proposed development will not detract from the natural beauty of the undeveloped skylines, ridgelines, and shoreline (LUP Policy 3.2.4.A.1). The proposed exterior lighting (see Sheet A1.2 of the attached plans) is unobtrusive, down-lit, compatible with the local area, and will be constructed or located so that only intended area is illuminated and off-site glare is fully controlled. The project, as designed and sited, assures protection of the public viewshed, is consistent with neighborhood character, and assures visual integrity.
- e) Development Standards. Pursuant to Title 20 Section 20.17.060.C, accessory non-habitable structures shall have a front setback of 50 feet, and 6 feet for the side setbacks and 1 foot for rear setbacks. Further, 15 feet is the maximum allowed height for accessory non-habitable structures within the WSC zoning district. As proposed, the storage structure and connecting retaining walls will have setbacks of approximately 50 feet (front) and greater than 200 feet (sides and rear). The proposed storage structure will have an above-natural grade height of 8 feet 6 inches. All proposed retaining and site stone walls extending

from the storage structure range in height from approximately 6 feet to 8 feet 6 inches. All portions of the proposed retaining and site walls within the required front setback are less than 6 feet tall and therefore are not considered a “structure” (Title 20 section 20.06.1200) and are not subject to setback requirements.

The allowed maximum site coverage is 10 percent. The subject property is 9.95 acres or 433,422 square feet, which allows site coverage of 43,342 square feet. The existing site coverage, which is limited to the existing residence, decks over 24 inches tall, and eaves extending over 30 inches, is 2,628 square feet or 0.60 percent. The proposed 355-square-foot structure will result in a total site coverage of 2,983 square feet of 0.68 %. Therefore, the proposed project meets all development standards of the WSC zoning district.

- f) Cultural Resources. According to Monterey County Geographic Information System (GIS) records, the subject parcel has a high archaeological sensitivity but is not within 750 feet of a known archaeological resource. The archaeological report prepared for the construction of the single-family dwelling (HCD-Planning File No. PC06869) found that there was no surface evidence of archaeological resources or their indicators. Therefore, the potential for inadvertent impacts to cultural resources is limited and will be controlled by use of the County’s standard condition (Condition No. 3), which requires the contractor to stop work if previously unidentified resources are discovered during construction.
- g) Development on Slopes in Excess of 30 Percent. The subject parcel is also entirely covered by slopes in excess of 30 percent. Title 20 section 20.64.230 requires the granting of a Coastal Development Permit for all development on slopes in excess of 30 percent unless the development meets the criteria of subsection C.2, in which case the development would require the granting of a Coastal Administrative Permit. Criteria of Title 20 section 20.64.230.C.2, include the excavation and fill of less than 100 cubic yards, which does exceed 2 feet in depth and is not steeper than 66 percent. As illustrated on Sheets A1.1 and A4.0 of the attached plans, portions of the proposed structure, extending site walls, and auto court retaining walls are proposed on slopes in excess of 30 percent but less than 66 percent. Further, all cut and fill associated with these project components are less than 2 feet in depth, totaling approximately 2.6 cubic yards of cut and 7.1 cubic yards of fill.

The slope adjacent to the proposed structure was created with implementation of PC06869, which included grading to establish the driveway. Big Sur Coast LUP Policy 3.2.4.7 requires that adequate drainage and erosion control measures be implemented with the construction of new roads to prevent erosion. Due to the lack of existing drainage and erosion control measures, significant natural erosion has occurred along this slope. Construction of the proposed storage building includes the installation of downspouts, foundation drainage, and a gravel drywell to address this issue. Therefore, the proposed project

meets the requirements of 20.64.230.C.2, and the criteria to grant a Coastal Administrative Permit have been made in this case.

- h) Land Use Advisory Committee (LUAC) Review. The project was not referred to the Big Sur Land Use Advisory Committee (LUAC) for review because it does not involve a public hearing Design Approval, a Lot Line Adjustment, or preparation of an Initial Study.
- i) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

2. FINDING: SITE SUITABILITY – The site is physically suitable for the proposed development and/or use.

- EVIDENCE:**
- a) The project has been reviewed for site suitability by the following departments and agencies: HCD-Planning, HCD-Engineering Services, HCD-Environmental Services, Environmental Health Bureau, and CalFire. County staff reviewed the application materials and plans to verify that the project on the subject site conforms to the applicable plans and regulations, and there has been no indication from these departments/agencies that the site is not suitable for the development. Conditions recommended have been incorporated.
 - b) Staff identified no potential impacts to natural resources.
 - c) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

3. FINDING: HEALTH AND SAFETY – The establishment, maintenance, or operation of the project applied for will not under the circumstances of this particular case be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the County.

- EVIDENCE:**
- a) The project was reviewed by HCD-Planning, HCD- Engineering Services, HCD-Environmental Services, Environmental Health Bureau, and CaFire. The respective agencies have recommended conditions, where appropriate, to ensure that the project will not have an adverse effect on the health, safety, and welfare of persons either residing or working in the neighborhood.
 - b) Necessary public facilities are provided. The subject property's residence is served portable water by the Clear Ride Water Association (Water System ID: 2701898), while the on-site wastewater treatment system provides sewage treatment. The Environmental Health Bureau reviewed the project and had no comments or concerns.
 - c) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

4. FINDING: NO VIOLATIONS – The subject property is in compliance with all rules and regulations pertaining to zoning uses, subdivision, and any other applicable provisions of the County's zoning ordinance. No violations exist on the property.

- EVIDENCE:**
- a) Staff reviewed Monterey County HCD-Planning and HCD-Building Services records and is not aware of any violations existing on subject property.
 - b) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

5. **FINDING:** **CEQA (Exempt)** – The project is categorically exempt from environmental review and no unusual circumstances were identified to exist for the proposed project.

- EVIDENCE:**
- a) California Environmental Quality Act (CEQA) Guidelines section 15303 categorically exempts the construction of accessory structures.
 - b) As proposed, the project involves the construction of an accessory non-habitable storage structure and site improvements including retaining walls and an auto court. Therefore, the project meets the Class 3 Categorical Exemption requirements.
 - c) None of the exceptions under CEQA Guidelines Section 15300.2 apply to this project. There is no significant effect on the environment due to unusual circumstances. No trees are proposed for removal and the proposed development is not visible from any scenic corridor or scenic highway. There is no cumulative impact without any prior successive projects of the same type in the same place, over time and no new land use is proposed. The site is not included on any list compiled pursuant to Section 65962.5 of the Government Code to be considered on a hazardous waste site. No known historical or archaeological resources are present.
 - d) See supporting Finding Nos. 1 and 2. The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

6. **FINDING:** **PUBLIC ACCESS** – The project is in conformance with the public access and recreation policies of the Coastal Act (specifically Chapter 3 of the Coastal Act of 1976, commencing with Section 30200 of the Public Resources Code) and applicable Local Coastal Program, and does not interfere with any form of historic public use or trust rights.

- EVIDENCE:**
- a) No access is required as part of the project as no substantial adverse impact on access, either individually or cumulatively, as described in the Big Sur CIP Section 20.145.150 can be demonstrated.
 - b) No evidence or documentation has been submitted or found showing the existence of historic public use or trust rights over this property.
 - c) The subject property is not described as an area where the Local Coastal Program requires physical public access (Figure 3, Public Access, in the Big Sur LUP).
 - d) The subject project site is not identified as an area where the Local Coastal Program requires visual public access (Figure 3, Public Access, in the Big Sur LUP).
 - e) As sited, the development proposal will not interfere with visual access along Highway 1, Sycamore Canyon Road, or Pfeiffer State Beach because the property is not visible from any of these points due to topography. The proposed development is consistent with Big Sur Coast

LUP Policies 6.1.5.B.4 and 6.1.5.F, and will not block protected public views toward the ocean or along the mountain range and will not adversely impact the public viewshed or scenic character in the project vicinity. As proposed, the project is consistent with applicable visual resource and public access policies in the Big Sur Coast LUP.

- f) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning found in Project File PLN220217.

7. FINDING: **APPEALABILITY** – The decision on this project may be appealed to the Board of Supervisors and California Coastal Commission.

- EVIDENCE:**
- a) Board of Supervisors. Pursuant to Title 20, Section 20.86.030, an appeal may be made to the Board of Supervisors by any public agency or person aggrieved by a decision of an Appropriate Authority other than the Board of Supervisors.
 - b) Coastal Commission. Pursuant to Title 20, Section 20.86.080.A, the project is subject to appeal by/to the California Coastal Commission because it involves development between the sea and the first through public road paralleling the sea (i.e., State Route/Highway 1).

DECISION

NOW, THEREFORE, based on the above findings and evidence, the HCD Chief of Planning does hereby:

- 1) Find that the project qualifies for a Class 3 Categorical Exemption pursuant to CEQA Guidelines section 15303, and there are no exceptions pursuant to section 15300.2; and
- 2) Approve a Coastal Administrative Permit to allow less than 100 cubic yards of grading on slopes in excess of 30 percent and a Design Approval to allow construction of 355 square foot storage structure and associated site improvements including an auto court and retaining walls.

All of which are in general conformance with the attached sketch and subject to the attached conditions, all being attached hereto and incorporated herein by reference.

PASSED AND ADOPTED this 5th day of June 2024.

DocuSigned by:

F6C3AC03D78644E...

Melanie Beretti, AICP
Acting, HCD Chief of Planning

COPY OF THIS DECISION MAILED TO APPLICANT ON JUNE 13, 2024.

THIS APPLICATION IS APPEALABLE TO THE BOARD OF SUPERVISORS. IF ANYONE WISHES TO APPEAL THIS DECISION, AN APPEAL FORM MUST BE COMPLETED AND SUBMITTED TO THE CLERK TO THE BOARD ALONG WITH THE APPROPRIATE FILING FEE ON OR BEFORE JUNE 24, 2024.

THIS PROJECT IS LOCATED IN THE COASTAL ZONE AND IS APPEALABLE TO THE COASTAL COMMISSION. UPON RECEIPT OF NOTIFICATION OF THE FINAL LOCAL ACTION NOTICE (FLAN) STATING THE DECISION BY THE FINAL DECISION MAKING BODY, THE COMMISSION ESTABLISHES A 10 WORKING DAY APPEAL PERIOD. AN APPEAL FORM MUST BE FILED WITH THE COASTAL COMMISSION. FOR FURTHER INFORMATION, CONTACT THE COASTAL COMMISSION AT (831) 427-4863 OR AT 725 FRONT STREET, SUITE 300, SANTA CRUZ, CA.

This decision, if this is the final administrative decision, is subject to judicial review pursuant to California Code of Civil Procedure Sections 1094.5 and 1094.6. Any Petition for Writ of Mandate must be filed with the Court no later than the 90th day following the date on which this decision becomes final.

NOTES

1. You will need a building permit and must comply with the Monterey County Building Ordinance in every respect.

Additionally, the Zoning Ordinance provides that no building permit shall be issued, nor any use conducted, otherwise than in accordance with the conditions and terms of the permit granted or until ten days after the mailing of notice of the granting of the permit by the appropriate authority, or after granting of the permit by the Board of Supervisors in the event of appeal.

Do not start any construction or occupy any building until you have obtained the necessary permits and use clearances from Monterey County HCD-Planning and HCD-Building Services Department office in Salinas.

2. This permit expires 3 years after the above date of granting thereof unless construction or use is started within this period.

Form Rev. 1-27-2021

County of Monterey HCD Planning

Conditions of Approval/Implementation Plan/Mitigation Monitoring and Reporting Plan

PLN220217

1. PD001 - SPECIFIC USES ONLY

Responsible Department: Planning

Condition/Mitigation Monitoring Measure: This Coastal Administrative Permit and Design Approval allow less than 100 cubic yards of grading on slopes and the construction of 355 square foot storage structure and associated site improvements including an auto court and retaining walls. The property is located at 46402 Pear Valley Road, Big Sur (Assessor's Parcel Number 419-231-005-000), Big Sur Coast Land Use Plan, Coastal Zone. This permit was approved in accordance with County ordinances and land use regulations subject to the terms and conditions described in the project file. Neither the uses nor the construction allowed by this permit shall commence unless and until all of the conditions of this permit are met to the satisfaction of the Director of HCD - Planning. Any use or construction not in substantial conformance with the terms and conditions of this permit is a violation of County regulations and may result in modification or revocation of this permit and subsequent legal action. No use or construction other than that specified by this permit is allowed unless additional permits are approved by the appropriate authorities. To the extent that the County has delegated any condition compliance or mitigation monitoring to the Monterey County Water Resources Agency, the Water Resources Agency shall provide all information requested by the County and the County shall bear ultimate responsibility to ensure that conditions and mitigation measures are properly fulfilled. (HCD - Planning)

Compliance or Monitoring Action to be Performed: The Owner/Applicant shall adhere to conditions and uses specified in the permit on an ongoing basis unless otherwise stated.

2. PD002 - NOTICE PERMIT APPROVAL

Responsible Department: Planning

Condition/Mitigation Monitoring Measure: The applicant shall record a Permit Approval Notice. This notice shall state:
"A Coastal Administrative Permit and Design Approval (Resolution Number 24-024) was approved by the HCD Chief of Planning for Assessor's Parcel Number 419-231-005-000 on June 5, 2024. The permit was granted subject to 4 conditions of approval which run with the land. A copy of the permit is on file with Monterey County HCD - Planning."

Proof of recordation of this notice shall be furnished to the Director of HCD - Planning prior to issuance of grading and building permits, Certificates of Compliance, or commencement of use, whichever occurs first and as applicable. (HCD - Planning)

Compliance or Monitoring Action to be Performed: Prior to the issuance of grading and building permits, certificates of compliance, or commencement of use, whichever occurs first and as applicable, the Owner/Applicant shall provide proof of recordation of this notice to the HCD - Planning.

3. PD003(A) - CULTURAL RESOURCES NEGATIVE ARCHAEOLOGICAL REPORT

Responsible Department: Planning

Condition/Mitigation Monitoring Measure: If, during the course of construction, cultural, archaeological, historical or paleontological resources are uncovered at the site (surface or subsurface resources) work shall be halted immediately within 50 meters (165 feet) of the find until a qualified professional archaeologist can evaluate it. Monterey County HCD - Planning and a qualified archaeologist (i.e., an archaeologist registered with the Register of Professional Archaeologists) shall be immediately contacted by the responsible individual present on-site. When contacted, the project planner and the archaeologist shall immediately visit the site to determine the extent of the resources and to develop proper mitigation measures required for recovery.
(HCD - Planning)

Compliance or Monitoring Action to be Performed: The Owner/Applicant shall adhere to this condition on an on-going basis.

Prior to the issuance of grading or building permits and/or prior to the recordation of the final/parcel map, whichever occurs first, the Owner/Applicant shall include requirements of this condition as a note on all grading and building plans. The note shall state "Stop work within 50 meters (165 feet) of uncovered resource and contact Monterey County HCD - Planning and a qualified archaeologist immediately if cultural, archaeological, historical or paleontological resources are uncovered."

When contacted, the project planner and the archaeologist shall immediately visit the site to determine the extent of the resources and to develop proper mitigation measures required for the discovery.

4. PD011 - TREE AND ROOT PROTECTION

Responsible Department: Planning

Condition/Mitigation Monitoring Measure: Trees which are located close to construction site(s) shall be protected from inadvertent damage from construction equipment by fencing off the canopy driplines and/or critical root zones (whichever is greater) with protective materials, wrapping trunks with protective materials, avoiding fill of any type against the base of the trunks and avoiding an increase in soil depth at the feeding zone or drip-line of the retained trees. Said protection, approved by certified arborist, shall be demonstrated prior to issuance of building permits subject to the approval of HCD - Director of Planning. If there is any potential for damage, all work must stop in the area and a report, with mitigation measures, shall be submitted by certified arborist. Should any additional trees not included in this permit be harmed, during grading or construction activities, in such a way where removal is required, the owner/applicant shall obtain required permits. (HCD - Planning)

Compliance or Monitoring Action to be Performed: Prior to issuance of grading and/or building permits, the Owner/Applicant shall submit evidence of tree protection to HCD - Planning for review and approval.

During construction, the Owner/Applicant/Arborist shall submit on-going evidence that tree protection measures are in place through out grading and construction phases. If damage is possible, submit an interim report prepared by a certified arborist.

Prior to final inspection, the Owner/Applicant shall submit photos of the trees on the property to HCD-Planning after construction to document that tree protection has been successful or if follow-up remediation or additional permits are required.

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.

PROPOSED EXTERIOR MATERIALS



STONE CLAD CURVED WALLS & PLANTED ROOF



STEEL SLIDING DOOR



FLAGSTONE WALKWAYS & GRAVEL DRIVEWAY



1 RENDERING OF (P) STORAGE BUILDING
SCALE: N.T.S.

EXISTING PROPERTY PHOTOS



FRONT OF (E) HOUSE & DRIVEWAY, LOOKING SOUTHWEST



FRONT OF (E) HOUSE & DRIVEWAY, LOOKING WEST

SHEET INDEX

G1.0	PROJECT INFORMATION
G1.1	CONSTRUCTION NOTES
G1.2	CONSTRUCTION MANAGEMENT, GRADING, EROSION PLAN
G1.3	BEST MANAGEMENT PRACTICES
G1.4	CALGREEN RESIDENTIAL CHECKLIST
G1.5	CALGREEN RESIDENTIAL CHECKLIST
A1.0	EXISTING SITE PLAN
A1.1	PROPOSED SITE PLAN
A1.2	PROPOSED SITE LIGHTING
A2.0	PROPOSED FLOOR & ROOF PLAN
A3.0	BUILDING ELEVATIONS
A4.0	BUILDING SECTIONS

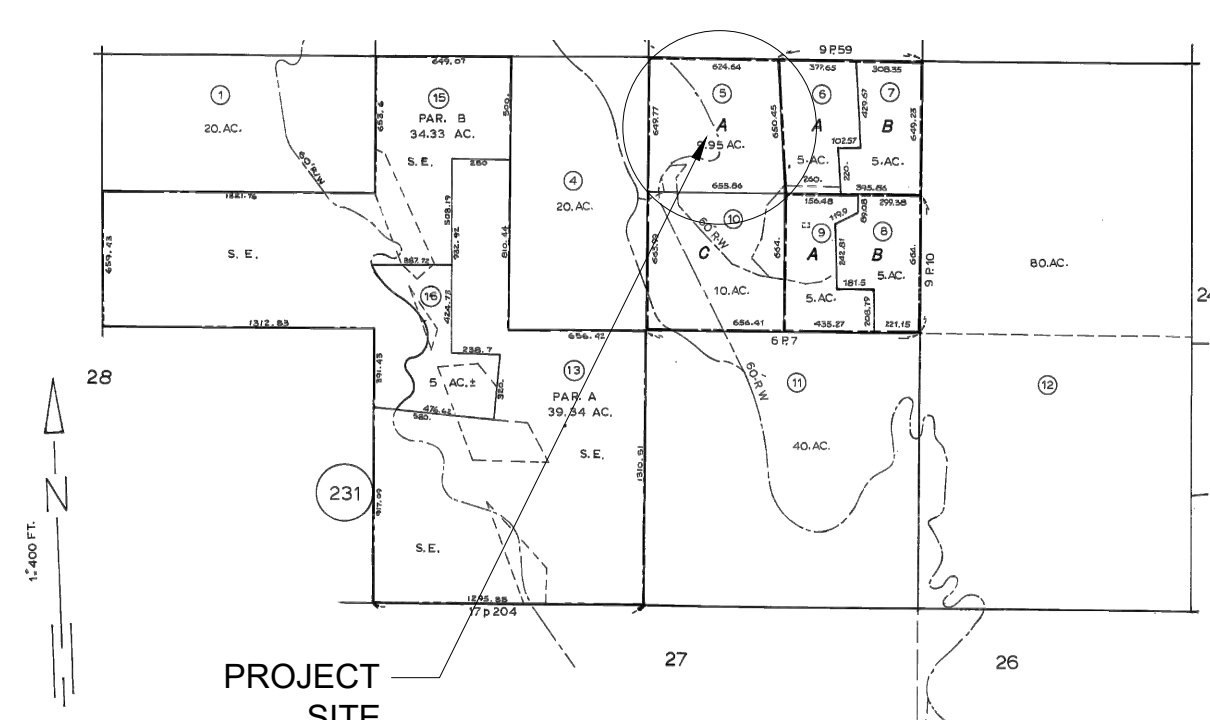
SCOPE OF WORK

CONSTRUCTION OF A NEW DETACHED STORAGE BUILDING, 355 SF IN SIZE. STORAGE BUILDING TO BE CLAD IN NATURAL STONE VENEER AND HAVE CURVED WALLS TO BLEND INTO (E) HILLSIDE. STORAGE BUILDING WILL BE ACCESSED BY NEW 6'W STEEL DOOR. SCOPE OF WORK ALSO INCLUDES REPLACING (E) DIRT DRIVEWAY & PARKING AREA WITH GRAVEL, AND INSTALLING NEW STONE CLAD GARDEN WALLS & FLAGSTONE WALKWAYS.

VICINITY MAP



PARCEL MAP



GENERAL NOTES

- CONTRACTOR TO FIELD VERIFY SURVEY AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND SHALL NOTIFY THE ARCHITECT IN WRITING OF ANY DISCREPANCIES.
- CONTRACTOR SHALL PROTECT EXISTING TREES AND ROOT SYSTEM. ALL EXCAVATION AROUND EXISTING TREES SHALL BE MADE BY HAND.
- CULTURAL, ARCHAEOLOGICAL, HISTORICAL, OR PALEONTOLOGICAL RESOURCES NOTIFICATION: "STOP WORK WITHIN 50 METERS (165 FEET) OF UNCOVERED RESOURCE AND CONTACT MONTEREY COUNTY RMA-PLANNING AND A QUALIFIED ARCHAEOLOGIST IMMEDIATELY IF CULTURAL, ARCHAEOLOGICAL, HISTORICAL, OR PALEONTOLOGICAL RESOURCES ARE UNCOVERED".
- SMOKE DETECTORS IN THE MAIN DWELLING SHALL BE INSTALLED AND FIELD VERIFIED IN EACH BEDROOM, IN THE HALLWAY LEADING TO THE BEDROOMS AND ON EACH FLOOR PER CRC R314.2. CARBON MONOXIDE ALARMS SHLL BE INSTALLED AND FIELD VERIFIED ON EACH FLOOR PER CRC R315.2.
- THE ISSUANCE OF A PERMIT SHALL NOT PREVENT THE BUILDING OFFICIAL FROM REQUIRING THE CORRECTION OF ERRORS ON THESE PLANS OR FROM PREVENTING ANY VIOLATION OF THE CODES ADOPTED BY THE COUNTY, RELEVANT LAWS, ORDINANCES, RULES AND/OR REGULATIONS.
- CONTRACTOR TO OBTAIN AN 8-1-1/DIG ALERT TICKET PRIOR TO PERMIT ISSUANCE AND TO MAINTAIN THE TICKET IN ACTIVE STATUS AND ON SITE FOR INSPECTION THROUGHOUT THE PROJECT.

PROJECT TEAM

OWNER	JOHN & AMY FOWLER 46402 CLEAR RIDGE ROAD BIG SUR, CA 93920
ARCHITECT	SAMUEL PITNICK ARCHITECTS, INC. PO BOX 22412, CARMEL, CA 93922 PHONE: (831) 241-1895 SAMUEL@PITNICK@GMAIL.COM
SURVEYOR	RASMUSSEN LAND SURVEYING, INC. 2150 GARDEN ROAD, SUITE A-3 MONTEREY, CA 93942 PHONE: (831) 375-7240
CONTRACTOR	FRANK LLOYD JENKINS CONSTRUCTION LICENSE #875218 PO BOX 1079, CARMEL, CA 93921 PHONE: (831) 402-0247

PROJECT INFORMATION

PROPERTY ADDRESS	46402 CLEAR RIDGE ROAD BIG SUR, CA 93920
APN	419-231-005-000
ZONING	WSC/40-D(CZ)
TYPE OF CONSTRUCTION	TYPE V-B
OCCUPANCY GROUP	R-3 / SINGLE FAMILY RESIDENCE U / GARAGE, STORAGE
YEAR BUILT	1991

BUILDING INFORMATION & LOT COVERAGE

LOT SIZE	433,422 SF (9.95 ACRES)
ALLOWABLE SITE COVERAGE	10% OR 43,342 SF
(E) LOT COVERAGE:	
(E) STRUCTURES	2,016 SF
(E) DECKS/PATIOS > 24"	588 SF
(E) EAVES > 30"	24 SF
TOTAL	2,628 SF OR 0.6%
(P) LOT COVERAGE:	
(E) STRUCTURES	2,016 SF
(P) STRUCTURES	355 SF
(E) DECKS/PATIOS > 24"	588 SF
(E) EAVES > 30"	24 SF
TOTAL	2,983 SF OR 0.6%
(E) FLOOR AREA:	
(E) MAIN HOUSE	2,337 SF
(E) GUEST HOUSE	441 SF
TOTAL	2,778 SF
(P) FLOOR AREA:	
(E) MAIN HOUSE	2,337 SF
(E) GUEST HOUSE	441 SF
(P) STORAGE	355 SF
TOTAL	3,133 SF

MISCELLANEOUS

WATER SOURCE	MUTUAL WELL SYSTEM
WASTE DISPOSAL SYSTEM	SEPTIC
TREES TO BE REMOVED	NONE
GRADING ESTIMATES	SEE SHEET G1.2
(E) PARKING	2 SPACES (UNCOVERED)
(E) BUILDING SPRINKLERED	NO
(P) BUILDING SPRINKLERED	NO

BUILDING CODE INFO

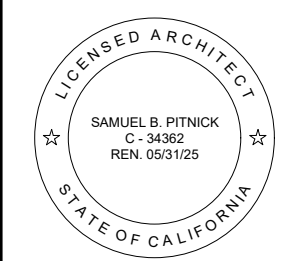
THIS PROJECT SHALL COMPLY WITH THE FOLLOWING:
2022 CALIFORNIA RESIDENTIAL CODE
2022 CALIFORNIA MECHANICAL CODE
2022 CALIFORNIA PLUMBING CODE
2022 CALIFORNIA ELECTRICAL CODE
2022 CALIFORNIA ENERGY CODE
2022 CALIFORNIA FIRE CODE
2022 CALIFORNIA GREEN BUILDING STANDARDS CODE

ALL MATERIALS & CONSTRUCTION TO COMPLY WITH CHAPTER 7A OF THE 2022 CBC, AND CHAPTER 3, SECTION 337R OF THE 2022 CRC.

DEFERRED SUBMITTALS

- PHOTOVOLTAIC SYSTEM DESIGN, DETAILS, SPECIFICATIONS

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK@GMAIL.COM

REVISIONS	DATE

ARCHITECTURAL
PROJECT
INFORMATION

Scale: SEE DWG.
Drawn By: SBP
Job: -

G1.0

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.

DEMOLITION NOTES

1. MATERIAL SALVAGE: COORDINATE WITH OWNER PRIOR TO DEMOLITION. REMOVE, PROTECT AND STORE MATERIAL SCHEDULED FOR REUSE AND SALVAGE.
2. SEE STRUCTURAL DRAWINGS FOR DEMOLITION SPECIFIC TO ANY NEW STRUCTURAL WORK.
3. THE ARCHITECT HAS NO KNOWLEDGE OF AND SHALL NOT BE LIABLE FOR ANY ASBESTOS OR OTHER HAZARDOUS MATERIALS ON THE JOB SITE. IF ASBESTOS OR OTHER HAZARDOUS MATERIALS ARE DISCOVERED DURING CONSTRUCTION, THE CONTRACTOR SHALL ISOLATE THE AFFECTED AREA AND CONTACT THE ARCHITECT FOR FURTHER INSTRUCTIONS BEFORE PROCEEDING.
4. REPAIR ALL DEMOLITION PERFORMED, IN EXCESS OF THAT REQUIRED, AT NO ADDITIONAL COST TO THE OWNER.
5. REMOVE FROM SITE AND LEGALLY DISPOSE OF DAILY ALL REFUSE, DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM DEMOLITION OPERATIONS. BURNING OF DEBRIS ON SITE SHALL NOT BE PERMITTED.
6. REMOVE TOOLS AND EQUIPMENT FROM SITE UPON COMPLETION OF WORK. LEAVE CONTRACT AREAS AND SITE CLEAN, ORDERLY AND IN A CONDITION ACCEPTABLE FOR NEW OR OTHER CONSTRUCTION.

LIGHTING NOTES

1. ALL INTERIOR LIGHTS SHALL BE DIMMABLE UNLESS OTHERWISE NOTED.
2. ALL BATHROOM FANS SHALL HAVE TIMER OPTIONS.
3. ALL BATHROOM LIGHTS SHALL HAVE OCCUPANCY SENSORS.
4. OUTLET BOXES INSTALLED FOR LUMINAIRES OR LIGHTING SHALL BE PERMITTED TO SUPPORT 50 POUNDS OR LESS. LUMINAIRES WEIGHING MORE THAN 50 POUNDS MUST BE LISTED AND MARKED FOR THE MAXIMUM WEIGHT.
5. ALL INSTALLED LUMINAIRES SHALL BE HIGH EFFICACY; EITHER LISTED BY SOURCE TYPE OR BY BEING JA8-2019 CERTIFIED LABELED.
6. SCREW BASED LUMINAIRES SHALL MEET ALL OF THE FOLLOWING:
 - 6.1. SHALL NOT BE RECESSED DOWNLIGHT LUMINAIRES IN CEILINGS; AND
 - 6.2. SHALL CONTAIN LAMPS THAT COMPLY WITH CEC REFERENCE JOINT APPENDIX JA8; AND
 - 6.3. THE INSTALLED LAMPS SHALL BE MARKED WITH JA8-2019 OR JA8-2019-E.
7. RECESSED LIGHT FIXTURES IN INSULATED CEILINGS SHALL BE APPROVED, LISTED ZERO-CLEARANCE INSULATION COVER TYPE, CERTIFIED AIR TIGHT (ASTM E283) AND SEALED WITH A GASKET OR CAULKED BETWEEN HOUSING AND CEILING, AND SHALL BE CERTIFIED TO COMPLY WITH SECTION 110.9 AND ALLOW BALLAST MAINTENANCE AND REPLACEMENT TO BE READILY ACCESSIBLE TO BUILDING OCCUPANTS FROM BELOW. [CENC 150.0(K)1C].
8. LUMINAIRES INSTALLED IN CLOSETS SHALL BE 12" FROM EDGE OF STORAGE SHELF FOR INCANDESCENT OR LED SURFACE MOUNTED. SURFACE MOUNTED OR RECESSED FLUORESCENT, RECESSED INCANDESCENT OR LED, 6".
9. SURFACE MOUNTED FLUORESCENT OR LED PERMITTED IN STORAGE AREA IF LISTED FOR USE.
10. RESIDENTIAL OUTDOOR LIGHTING PERMANENTLY MOUNTED TO THE DWELLING OR TO OTHER BUILDINGS ON THE SAME LOT SHALL BE CONTROLLED BY A MANUAL ON AND OFF SWITCH AND CONTROLLED BY A PHOTOCELL AND MOTION SENSOR OR BY PHOTOCONTROL AND AUTOMATIC TIME SWITCH CONTROL OR BY ASTRONOMICAL TIME CLOCK CONTROL THAT AUTOMATICALLY TURNS THE OUTDOOR LIGHTING OFF DURING DAYLIGHT HOURS OR BY ENERGY MANAGEMENT CONTROL SYSTEM.
11. AT LEAST ONE LUMINAIRE IN EACH BATHROOM, LAUNDRY ROOM, UTILITY ROOM AND GARAGE SHALL BE CONTROLLED BY A VACANCY OR OCCUPANT SENSOR.

CONSTRUCTION NOTES FOR VERY HIGH
FIRE HAZARD SEVERITY ZONES

1. NEW BUILDINGS AND STRUCTURES LOCATED IN "HIGH" FIRE HAZARD SEVERITY ZONES SHALL COMPLY WITH THE REQUIREMENTS OF CBC AND, CHAPTER 7A SECTION 701A.1. & CRC, CHAPTER 3, SECTION 337R.
2. ROOF GUTTERS SHALL BE PROVIDED WITH THE MEANS TO PREVENT THE ACCUMULATION OF LEAVES AND DEBRIS IN THE GUTTER. [§R327.5.4]
3. ROOF AND ATTIC VENTS SHALL RESIST THE INTRUSION OF FLAME AND EMBERS INTO THE ATTIC AREA OF THE STRUCTURE, OR SHALL BE PROTECTED BY CORROSION-RESISTANT, NONCOMBUSTIBLE WIRE MESH WITH OPENINGS A MINIMUM OF 1/16-INCH AND SHALL NOT EXCEED 1/8-INCH. [§R327.6.2]
4. VENTS SHALL NOT BE INSTALLED ON THE UNDERSIDE OF EAVES AND CORNICES, UNLESS THE VENTS ARE APPROVED TO RESIST THE INTRUSION OF FLAME AND EMBERS, THE ATTIC SPACE IS SPRINKLERED IN ACCORDANCE WITH CBC SEC. 903.3.1.1, OR IF THE EXTERIOR WALL AND UNDERSIDE OF THE EAVE ARE OF IGNITION RESISTANT MATERIALS AND THE VENTS ARE LOCATED MORE THAN 12 FEET FROM THE GROUND OR WALKING SURFACE. [§R327.6.3]
5. EXTERIOR WALL COVERING OR WALL ASSEMBLY SHALL BE APPROVED NONCOMBUSTIBLE MATERIAL, IGNITION-RESISTANT MATERIAL, HEAVY TIMBER, LOG WALL CONSTRUCTION, OR SHALL MEET THE PERFORMANCE CRITERIA OF STANDARD SFM 12-7A-1 FOR 10-MINUTE DIRECT FLAME CONTACT EXPOSURE TEST. [§R327.7.3] SEE EXCEPTIONS TO THIS SECTION FOR OTHER ALTERNATIVES.
6. EXTERIOR WALL COVERINGS SHALL EXTEND FROM THE TOP OF THE FOUNDATION TO THE ROOF, AND TERMINATE AT 2-INCH NOMINAL SOLID WOOD BLOCKING BETWEEN RAFTERS AND EAVES AT ALL ROOF OVERHANGS, OR IN THE CASE OF ENCLOSED EAVES, TERMINATE AT THE ENCLOSURE. [§R327.7.3.1]
7. THE EXPOSED ROOF DECK ON THE UNDERSIDE OF UNENCLOSED EAVES SHALL BE APPROVED NONCOMBUSTIBLE MATERIAL, IGNITION-RESISTANT MATERIAL, ONE LAYER OF 5/8" TYPE X GYPSUM BOARD, OR EXTERIOR PORTION OF AN APPROVED ONE HOUR WALL ASSEMBLY. [§R327.7.4] SEE EXCEPTIONS TO THESE SECTIONS FOR OTHER ALTERNATIVES.
8. EXTERIOR WINDOWS AND EXTERIOR GLAZED DOORS SHALL BE MULTIPANE GLAZING WITH A MINIMUM OF ONE TEMPERED PANE, GLASS BLOCK UNITS, HAVE A FIRE RESISTANCE RATING OF 20 MINUTES WHEN TESTED IN ACCORDANCE WITH NFPA 257, OR MEET THE REQUIREMENTS OF SFM 12-7A-2. [§R327.8.2.1]
9. EXTERIOR DOORS SHALL BE OF APPROVED NONCOMBUSTIBLE CONSTRUCTION OR IGNITIONRESISTANT MATERIAL, SOLID CORE WOOD HAVING STILES AND RAILS NOT LESS THAN 1-3/8 INCHES THICK WITH INTERIOR FIELD PANEL THICKNESS NO LESS THAN 1-1/4 INCHES THICK, SHALL HAVE A FIRE-RESISTANCE RATING OF NOT LESS THAN 20 MINUTES WHEN TESTED ACCORDING TO NFPA 252, OR MEET THE REQUIREMENTS OF SFM-7A-1. [§R327.8.3]

POLLUTANT CONTROL MEASURES

- IN ACCORDANCE WITH SECTION 4.504, THE FOLLOWING POLLUTANT CONTROL MEASURES SHALL BE IMPLEMENTED.
- 1.1. PAINT, STAINS AND OTHER COATINGS SHALL BE COMPLIANT WITH VOC LIMITS.
 - 1.2. AEROSOL PAINTS AND COATINGS SHALL BE COMPLIANT WITH PRODUCT WEIGHTED MIR LIMITS FOR ROG AND OTHER TOXIC COMPOUNDS. DOCUMENTATION SHALL BE PROVIDED TO VERIFY COMPLIANT VOC LIMIT FINISH MATERIALS HAVE BEEN USED.
 - 1.3. CARPET AND CARPET SYSTEMS SHALL BE COMPLIANT WITH VOC LIMITS.
 - 1.4. 50% OF THE FLOOR AREA RECEIVING RESILIENT FLOORINGS SHALL COMPLY WITH THE VOC EMISSION LIMITS DEFINED IN THE COLLABORATIVE FOR HIGH PERFORMANCE SCHOOLS (CHPS) LOW-EMITTING MATERIALS LIST OR BE CERTIFIED UNDER THE RESILIENT FLOOR COVERING INSTITUTE (RFCI) FLOORSCORE PROGRAM.
 - 1.5. PARTICLEBOARD, MEDIUM DENSITY FIBERBOARD (MDF) AND HARDWOOD PLYWOOD USED IN INTERIOR FINISH SYSTEMS SHALL COMPLY WITH LOW FORMALDEHYDE EMISSION STANDARDS. RESPONSE

FOWLER
RESIDENCE

46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

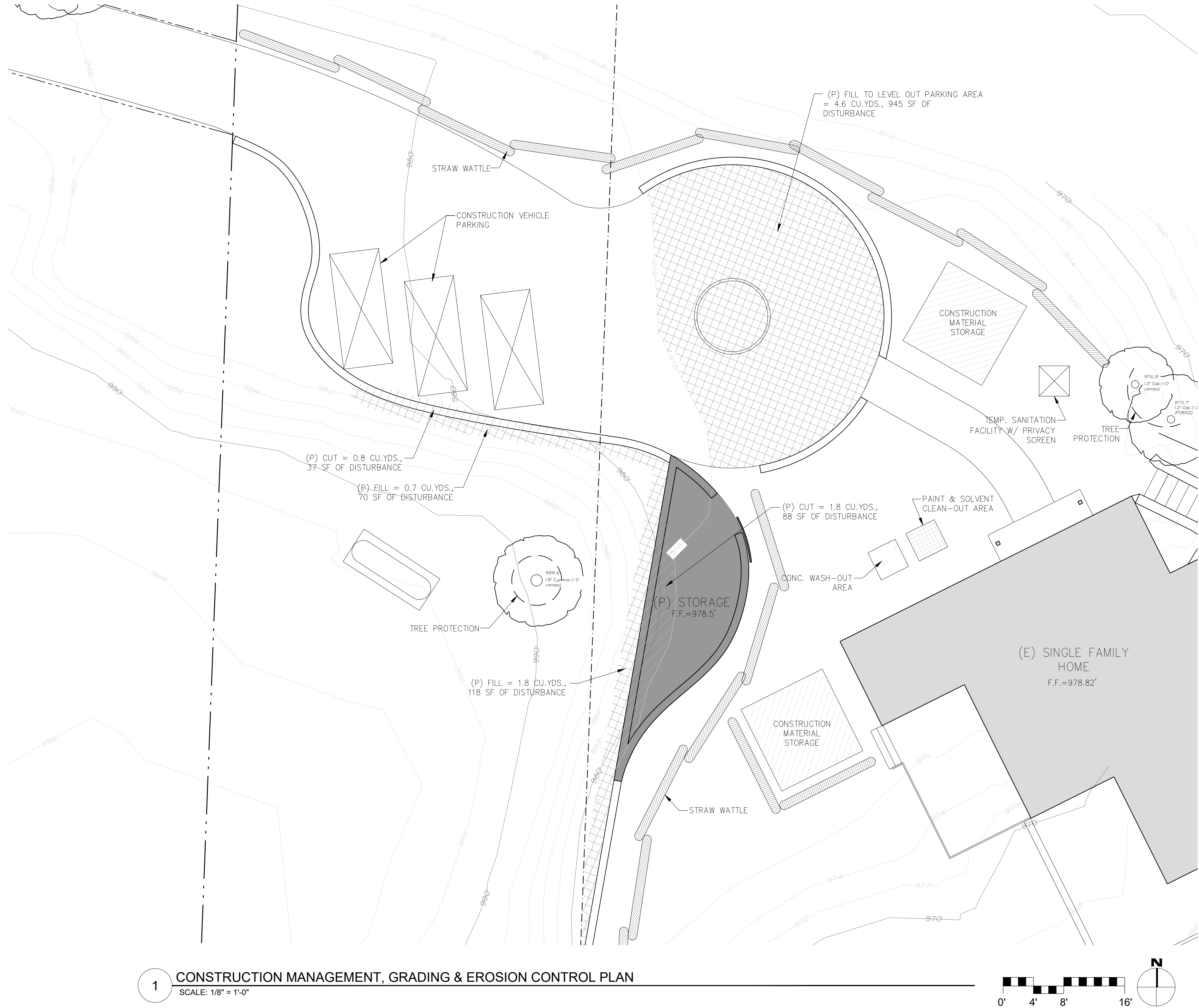
ARCHITECTURAL
CONSTRUCTION
NOTES

Scale: SEE DWG.
Drawn By: SBP
Job: -

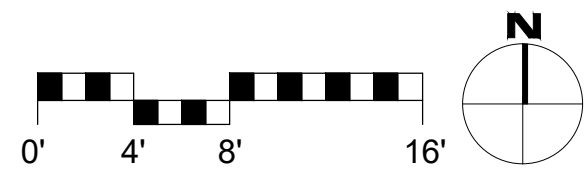
G1.1

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



1 CONSTRUCTION MANAGEMENT, GRADING & EROSION CONTROL PLAN
SCALE: 1/8" = 1'-0"



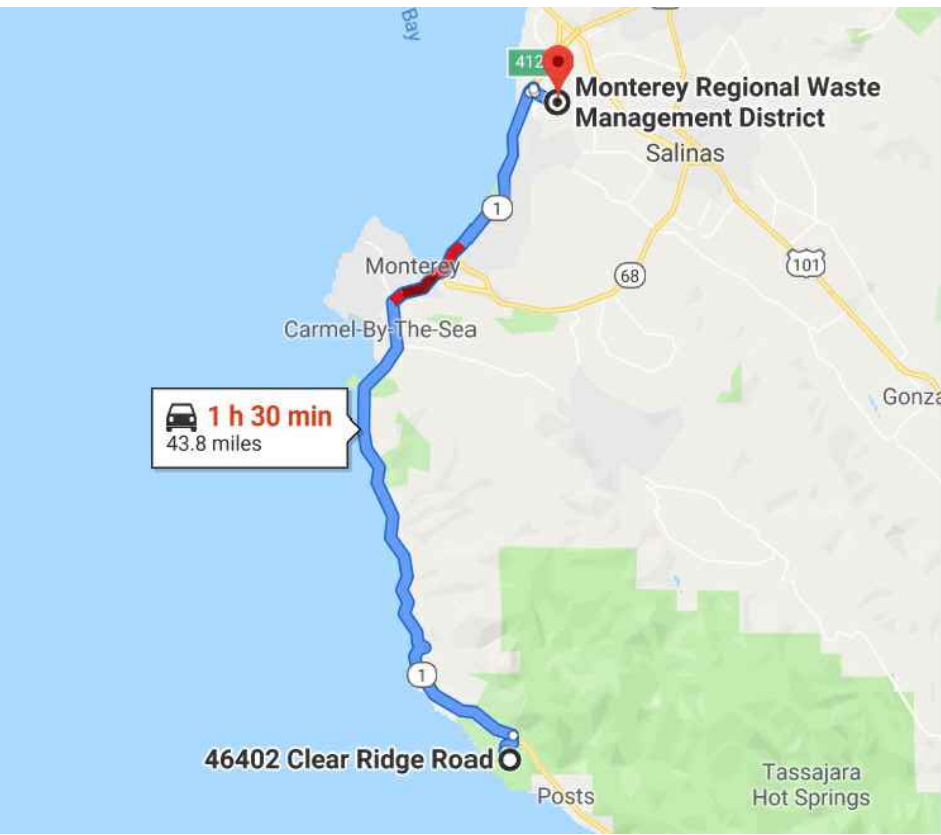
GENERAL NOTES

1. TREE PROTECTION SHALL BE IN PLACE PRIOR TO ANY PERMITS BEING ISSUED.
2. ALSO SEE BEST MANAGEMENT PRACTICES SHEET.
3. CULTURAL, ARCHAEOLOGICAL, HISTORICAL, OR PALEONTOLOGICAL RESOURCES NOTIFICATION: "STOP WORK WITHIN 50 METERS (165 FEET) OF UNCOVERED RESOURCE AND CONTACT MONTEREY COUNTY RMA-PLANNING AND A QUALIFIED ARCHAEOLOGIST IMMEDIATELY IF CULTURAL, ARCHAEOLOGICAL, HISTORICAL, OR PALEONTOLOGICAL RESOURCES ARE UNCOVERED".

CONSTRUCTION MANAGEMENT NOTES

1. DURATION OF CONSTRUCTION IS APPROX. 4-6 MONTHS STARTING FROM THE DATE PERMITS ARE ISSUED.
2. WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 8 AM AND 5 PM. WORK PERFORMED BEFORE 8AM SHALL BE NON-CONSTRUCTION ACTIVITY (QUIET HOUR)
3. NO TRUCK TRIPS ARE ESTIMATED TO BE NECESSARY FOR THE GRADING PHASE OF THE PROJECT.
4. TRUCKS WILL BE ROUTED TO AND FROM THE SITE USING THE CLEAR RIDGE ROAD EXIT OFF OF HIGHWAY ONE. TRUCKS WILL TAKE HIGHWAY ONE NORTH AND EXIT DEL MONTE BLVD. TO TRANSPORT WASTE AND DEBRIS TO MONTEREY REGIONAL WASTE MANAGEMENT DISTRICT. REFER TO MAP ON THIS SHEET.
5. THE NUMBER OF WORKERS WILL VARY THROUGH OUT THE CONSTRUCTION. WORKERS ONSITE WILL RANGE FROM 2 TO 12.
6. EROSION CONTROL PROTECTION TO BE INSTALLED PER THE PERMITTED PLANS.
7. STERILE STRAW WATTLES SHALL BE PLACED BEFORE AND DURING RAIN STORM EVENTS TO CONTAIN STORM WATER AND EROSION DURING CONSTRUCTION.
8. ALL ON AND OFF-ROAD DIESEL EQUIPMENT SHALL NOT IDLE FOR MORE THAN 5 MINUTES.
9. SUBSTITUTE GASOLINE-POWERED IN PLACE OF DIESEL-POWERED EQUIPMENT, WHERE FEASIBLE.
10. USE ALTERNATIVELY FUELED CONSTRUCTION EQUIPMENT ON-SITE WHERE FEASIBLE, SUCH AS COMPRESSED NATURAL GAS (CNG), LIQUEFIED NATURAL GAS (LNG), PROPANE OR BIODIESEL.
11. CONSTRUCTION TRUCK TRIPS WILL BE SCHEDULED DURING NON-PEAK HOURS TO REDUCE PEAK HOUR EMISSIONS.
12. DUST CONTROL MEASURES WILL BE IMPLEMENTED INCLUDING THE USE WATER TRUCKS OR SPRINKLER SYSTEMS IN SUFFICIENT QUANTITIES TO PREVENT AIRBORNE DUST FROM LEAVING THE SITE. WATERING FREQUENCY SHALL BE INCREASED WHENEVER WIND SPEEDS EXCEED 15 MPH. RECLAIMED (NON-POTABLE) WILL BE USED WHENEVER POSSIBLE.
13. ALL TRUCKS HAULING DIRT, SAND, SOIL, OR OTHER LOOSE MATERIALS ARE TO BE COVERED OR SHOULD MAINTAIN AT LEAST TWO FEET OF FREEBOARD (MINIMUM VERTICAL DISTANCE BETWEEN TOP OF LOAD AND TOP OF TRAILER) IN ACCORDANCE WITH CVC SECTION 23114.
14. THE PROPOSED AMOUNT OF GRADING PER DAY WILL BE SIGNIFICANTLY LESS THAN THE POTENTIAL THRESHOLD FOR SIGNIFICANT IMPACTS OF 2.2 ACRES/DAY.CEQA TABLE 5-2.
15. TRUCK STAGING - ALL TRUCK STAGING REQUIRED FOR THE CONSTRUCTION OF THIS PROJECT SHALL BE STAGED ON PRIVATE PROPERTY.

TRUCK ROUTING FROM SITE TO DUMP



CONSTRUCTION MANAGEMENT LEGEND:

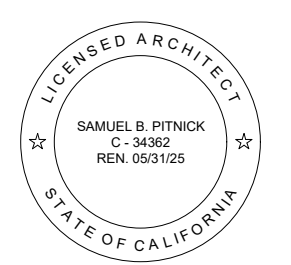
- | | |
|--------------------------------|-----------------|
| CONSTRUCTION MATERIAL STORAGE | STRAW WATTLE |
| PAINT & SOLVENT CLEAN-OUT AREA | TREE PROTECTION |
| CONCRETE CONTAINMENT WASHOUT | SILT FENCING |

GRADING LEGEND:

- | |
|---------------|
| CUT MATERIAL |
| FILL MATERIAL |

GRADING ESTIMATES:
(P) CUT = 2.6 CU.YDS.
(P) FILL = 7.1 CU.YDS.
(P) NET FILL = 4.5 CU.YDS.
AREA OF DISTURBANCE = 1,258 SF

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

ARCHITECTURAL
CONSTRUCTION
MANAGEMENT
PLAN

Scale: SEE DWG.
Drawn By: SBP
Job: -

G1.2

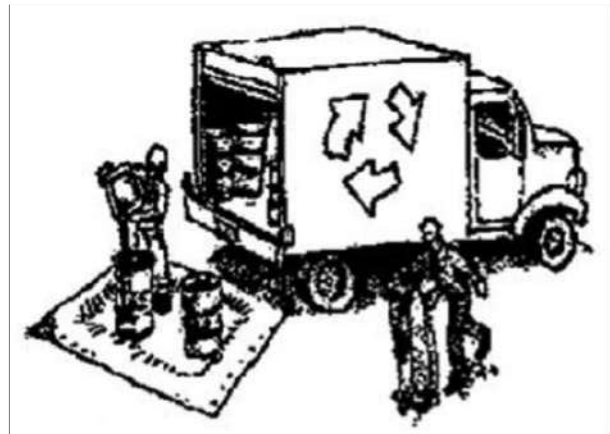
09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



CONSTRUCTION BEST MANAGEMENT PRACTICES (BMPs)

Construction Projects Are Required to Implement the Stormwater Best Management Practices (BMPs)
on this Page, as they Apply to Your Project, All Year Long.



MATERIALS & WASTE MANAGEMENT

Non-Hazardous Materials

- ❑ Berm and securely cover stockpiles of sand, dirt, or other construction materials with tarps when rain is forecast or if stockpiles are not actively being used. For best results, this should be done at the end of the work day throughout construction when feasible.
- ❑ Use (but don't overuse) reclaimed water for dust control.

Hazardous Materials

- ❑ Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
- ❑ Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
- ❑ Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals outdoors when rain is forecast within 24 hours.
- ❑ Arrange for appropriate disposal of all hazardous wastes.

Construction Entrances and Perimeter

- ❑ Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharges from site and tracking off site.

- ❑ Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking. Never hose down streets to clean up tracking.

Waste Management

- ❑ The California Green Building Code requires all permitted residential and non-residential construction, demolition and additions/alterations projects to recycle or salvage a minimum 65% of nonhazardous construction materials from the project.
- ❑ Cover waste disposal containers securely with tarps at the end of every work day and during wet weather.
- ❑ Clean or replace portable toilets, and inspect them frequently for leaks and spills. Incorporate secondary containment and locate them away from storm drain inlets.
- ❑ Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste (the Monterey Regional Waste Management District offers a Household Hazardous Waste Facility that accepts these items).



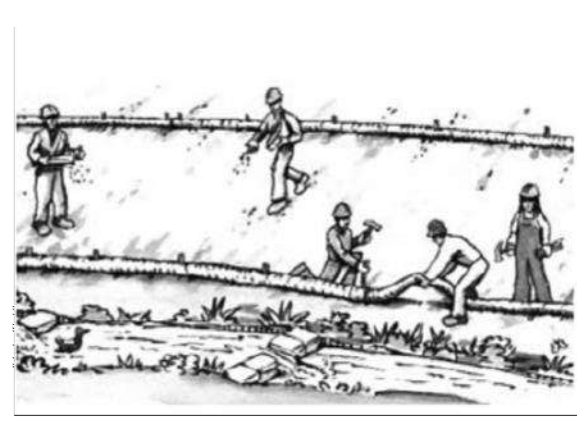
EQUIPMENT MANAGEMENT & SPILL CONTROL

Maintenance and Parking

- ❑ Designate an area, fitted with appropriate BMPs, for vehicle and equipment parking and storage.
- ❑ Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
- ❑ If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
- ❑ If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
- ❑ Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, steam cleaning equipment, etc.
- ❑ Inlet protection is the last line of spill defense. Drains/inlets that receive storm water must be covered or otherwise protected from receiving sediment/dirt/mud, other debris, or illicit discharges, and include gutter controls and filtration where applicable in a manner not impeding traffic or safety.

Spill Prevention and Control

- ❑ Keep spill cleanup materials (rags, absorbents, etc.) available at the construction site at all times.
- ❑ Inspect vehicles and equipment frequently for and repair leaks promptly. Use drip pans to catch leaks until repairs are made.
- ❑ Clean up spills or leaks immediately and dispose of cleanup materials properly (see the Monterey Regional Waste Management District's guidelines for accepting hazardous waste materials).
- ❑ Do not hose down surfaces where fluids have spilled. Use dry cleanup methods (absorbent materials, cat litter, and/or rags).
- ❑ Sweep up spilled dry materials immediately. Do not try to wash them away with water, or bury them.
- ❑ Clean up spills on dirt areas by digging up and properly disposing of contaminated soil (see the Monterey Regional Waste Management District's Contaminated Soil Acceptance Criteria).
- ❑ Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill: Dial 911.



EARTHWORK & CONTAMINATED SOILS

Erosion Control

- ❑ Schedule grading and excavation work for dry weather only.
- ❑ Stabilize all denuded areas, install and maintain temporary erosion controls (such as erosion control fabric or bonded fiber matrix) until vegetation is established.
- ❑ Seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.

Sediment Control

- ❑ Protect storm drain inlets, gutters, ditches, and drainage courses with appropriate BMPs, such as gravel bags, inlet filler, berms, etc.
- ❑ Prevent sediment from migrating offsite by installing and maintaining sediment controls, such as fiber rolls, silt fences, or sediment basins.
- ❑ Keep excavated soil on the site where it will not collect into the street.
- ❑ Transfer excavated materials to dump trucks on the site, not in the street.
- ❑ If any of the following conditions are observed, test for contamination and contact the Monterey County Environmental Health Department, Regional Water Quality Control Board, and local municipal inspector:
 - Unusual soil conditions, discoloration, or odor
 - Abandoned underground tanks
 - Abandoned wells
 - Buried barrels, debris, or trash.



PAVING/ASPHALT WORK

- ❑ Avoid paving and seal coating in wet weather, or when rain is forecast before fresh pavement will have time to cure.
- ❑ Cover storm drain inlets and manholes when applying seal coat, tack coat, slurry seal, fog seal, etc.
- ❑ Collect and recycle or appropriately dispose of excess abrasive gravel or sand. Do NOT sweep or wash it into gutters.
- ❑ Do not use water to wash down fresh asphalt or concrete pavement.

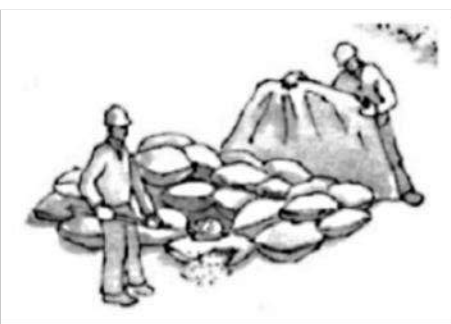
Sawcutting & Asphalt/Concrete Removal

- ❑ Completely cover or barricade storm drain inlets when saw cutting. Use filter fabric, catch basin inlet filters, or gravel bags to keep slurry out of the storm drain system.
- ❑ Protect storm drain inlets, gutters, ditches, and drainage courses with appropriate BMPs, such as gravel bags, inlet filters, berms, etc.
- ❑ Shovel, absorb, or vacuum saw-cut slurry and dispose of all waste as soon as you are finished in one location or at the end of each work day (whichever is sooner!).
- ❑ If sawcut slurry enters a catch basin, clean it up immediately.



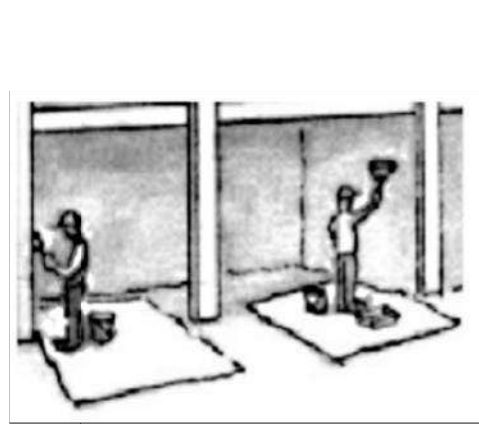
CONCRETE, GROUT & MORTAR APPLICATION

- ❑ Store concrete, grout and mortar under cover, on pallets and away from drainage areas. These materials must never reach a storm drain.
- ❑ Wash out concrete equipment/trucks offsite or in a contained area, so there is no discharge into the underlying soil or onto surrounding areas. Let concrete harden and dispose of as garbage.
- ❑ Collect the wash water from washing exposed aggregate concrete and remove it for appropriate disposal offsite.



LANDSCAPE MATERIALS

- ❑ Contain stockpiled landscaping materials by storing them under tarps when they are not actively being used.
- ❑ Stack erodible landscape material on pallets. Cover or store these materials when they are not actively being used or applied.
- ❑ Discontinue application of any erodible landscape material within 2 days before a forecast rain event or during wet weather.



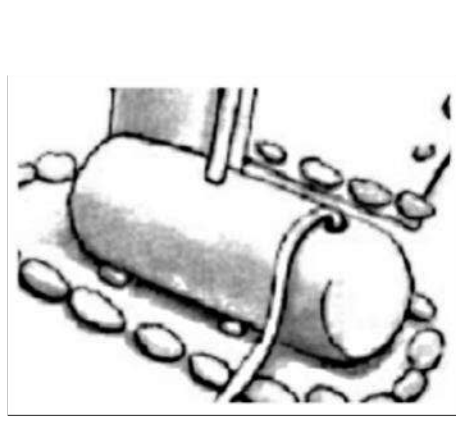
PAINTING & PAINT REMOVAL

Painting cleanup

- ❑ Never clean brushes or rinse paint containers into a street, gutter, storm drain, or surface waters.
- ❑ For water-based paints, paint out brushes to the extent possible. Rinse to the sanitary sewer once you have gained permission from the local wastewater treatment authority. Never pour paint down a drain.
- ❑ For oil-based paints, paint out brushes to the extent possible and clean with thinner or solvent in a proper container. Filter and reuse thinners and solvents. Dispose of residue and unusable thinner/solvents as hazardous waste.

Paint Removal

- ❑ Chemical paint stripping residue and chips and dust from marine paints or paints containing lead or tributyltin must be disposed of as hazardous waste.
- ❑ Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic drop cloths and disposed of as trash.



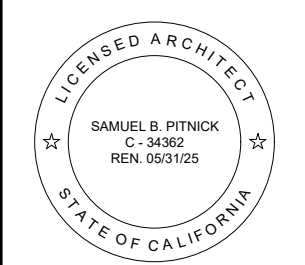
DEWATERING

- ❑ Effectively manage all run-on, all runoff within the site, and all runoff that discharges from the site.
- ❑ Divert run-on water from offsite away from all disturbed areas or otherwise ensure protection of its water quality for compliance.
- ❑ When dewatering, notify and obtain approval from the local municipality before discharging water to a street gutter or storm drain. Filtration or diversion through a basin, tank, or sediment trap, and/or disposal in sanitary sewer may be required.
- ❑ In areas of known contamination, testing is required prior to reuse or discharge of groundwater. Consult with the Engineer and municipal staff to determine whether testing is required and how to interpret results. Contaminated groundwater must be treated or hauled off-site for proper disposal.

STORM DRAIN POLLUTERS MAY BE LIABLE FOR FINES OF UP TO \$10,000 PER DAY!

* Adapted with permission from the San Mateo Countywide Water Pollution Prevention Program

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1885
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

ARCHITECTURAL

BEST
MANAGEMENT
PRACTICES

Scale: SEE DWG.
Drawn By: SBP
Job: -

G1.3

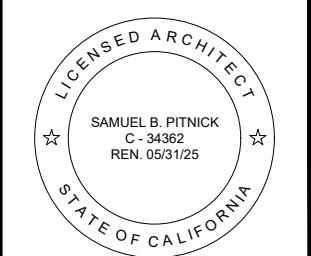
09/08/2023



2022 CALIFORNIA GREEN BUILDING STANDARDS CODE

RESIDENTIAL MANDATORY MEASURES, SHEET 1 (January 2023)

FOWLER RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICKARCH.COM

Y	N/A	RESPON. PARTY	CHAPTER 3 GREEN BUILDING SECTION 301 GENERAL	Y	N/A	RESPON. PARTY	4.106.4.2.1 Multifamily development projects with less than 20 dwelling units; and hotels and motels with less than 20 sleeping units or guest rooms.	Y	N/A	RESPON. PARTY	4.106.4.2.2 Multifamily development projects with 20 or more dwelling units, hotels and motels with 20 or more sleeping units or guest rooms.	Y	N/A	RESPON. PARTY	4.106.4.2.3 Electric vehicle charging for additions and alterations of parking facilities serving existing multifamily buildings.	Y	N/A	RESPON. PARTY	4.304 OUTDOOR WATER USE 4.304.1 OUTDOOR POTABLE WATER USE IN LANDSCAPE AREAS. Residential developments shall comply with a local water efficient landscape ordinance or the current California Department of Water Resources' Model Water Efficient Landscape Ordinance (MWELO), whichever is more stringent.
			301.1 SCOPE. Buildings shall be designed to include the green building measures specified as mandatory in the application checklists contained in this code. Voluntary green building measures are also included in the application checklists and may be included in the design and construction of structures covered by this code, but are not required unless adopted by a city, county, or city and county as specified in Section 101.7.				1.EV Capable. Ten (10) percent of the total number of parking spaces on a building site, provided for all types of parking facilities, shall be electric vehicle charging spaces (EV spaces) capable of supporting future Level 2 EVSE. Electrical load calculations shall demonstrate that the electrical panel service capacity and electrical system, including any on-site distribution transformer(s), have sufficient capacity to simultaneously charge all EVs at all required EV spaces at a minimum of 40 amperes.				1.EV Capable. Ten (10) percent of the total number of parking spaces on a building site, provided for all types of parking facilities, shall be electric vehicle charging spaces (EV spaces) capable of supporting future Level 2 EVSE. Electrical load calculations shall demonstrate that the electrical panel service capacity and electrical system, including any on-site distribution transformer(s), have sufficient capacity to simultaneously charge all EVs at all required EV spaces at a minimum of 40 amperes.				1.EV Capable. Ten (10) percent of the total number of parking spaces on a building site, provided for all types of parking facilities, shall be electric vehicle charging spaces (EV spaces) capable of supporting future Level 2 EVSE. Electrical load calculations shall demonstrate that the electrical panel service capacity and electrical system, including any on-site distribution transformer(s), have sufficient capacity to simultaneously charge all EVs at all required EV spaces at a minimum of 40 amperes.				Notes: 1. The Model Water Efficient Landscape Ordinance (MWELO) is located in the California Code Regulations, Title 23, Chapter 2.7, Division 2. MWELO and supporting documents, including water budget calculator, are available at: https://www.water.ca.gov/
			301.1.1 Additions and alterations. [HCD] The mandatory provisions of Chapter 4 shall be applied to additions or alterations of existing residential buildings where the addition or alteration increases the building's conditioned area, volume, or size. The requirements shall apply only to and/or within the specific area of the addition or alteration.				Exception: Areas of parking facilities served by parking lifts.				Exception: Areas of parking facilities served by parking lifts.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until EV chargers are installed for use.				
			Note: Repairs including, but not limited to, resurfacing, restriping and repairing or maintaining existing lighting fixtures are not considered alterations for the purpose of this section.				Exceptions: 1. When EV chargers (Level 2 EVSE) are installed in a number equal to or greater than the required number of EV capable spaces. 2. When EV chargers (Level 2 EVSE) are installed in a number less than the required number of EV capable spaces, the number of EV capable spaces required may be reduced by a number equal to the number of EV chargers installed.				Exceptions: 1. When EV chargers (Level 2 EVSE) are installed in a number equal to or greater than the required number of EV capable spaces. 2. When EV chargers (Level 2 EVSE) are installed in a number less than the required number of EV capable spaces, the number of EV capable spaces required may be reduced by a number equal to the number of EV chargers installed.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until EV chargers are installed for use.				
			Note: On and after January 1, 2014, residential buildings undergoing permitted alterations, additions, or improvements shall replace noncompliant plumbing fixtures with water-conserving plumbing fixtures. Plumbing fixture replacement is required prior to issuance of a certificate of final completion, certificate of occupancy or final permit approval by the local building department. See Civil Code Section 1101.1, et seq., for the definition of a noncompliant plumbing fixture, types of residential buildings affected and other important enactment dates.				301.2 LOW-RISE AND HIGH-RISE RESIDENTIAL BUILDINGS. [HCD] The provisions of individual sections of CALGreen may apply to either low-rise residential buildings high-rise residential buildings, or both. Individual sections will be designated by banners to indicate where the section applies specifically to low-rise only (LR) or high-rise only (HR). When the section applies to both low-rise and high-rise buildings, no banner will be used.				301.2 LOW-RISE AND HIGH-RISE RESIDENTIAL BUILDINGS. [HCD] The provisions of individual sections of CALGreen may apply to either low-rise residential buildings high-rise residential buildings, or both. Individual sections will be designated by banners to indicate where the section applies specifically to low-rise only (LR) or high-rise only (HR). When the section applies to both low-rise and high-rise buildings, no banner will be used.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			SECTION 302 MIXED OCCUPANCY BUILDINGS 302.1 MIXED OCCUPANCY BUILDINGS. In mixed occupancy buildings, each portion of a building shall comply with the specific green building measures applicable to each specific occupancy. Exceptions: 1. [HCD] Accessory structures and accessory occupancies serving residential buildings shall comply with Chapter 4 and Appendix A4, as applicable. 2. [HCD] For purposes of CALGreen, live/work units, complying with Section 419 of the California Building Code, shall not be considered mixed occupancies. Live/Work units shall comply with Chapter 4 and Appendix A4, as applicable.				4.106.4.2.2 Multifamily development projects with 20 or more dwelling units, hotels and motels with 20 or more sleeping units or guest rooms. The number of dwelling units, sleeping units or guest rooms shall be based on all buildings on a project site subject to this section.				4.106.4.2.2 Multifamily development projects with 20 or more dwelling units, hotels and motels with 20 or more sleeping units or guest rooms. The number of dwelling units, sleeping units or guest rooms shall be based on all buildings on a project site subject to this section.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			DIVISION 4.1 PLANNING AND DESIGN ABBREVIATION DEFINITIONS: HCD Department of Housing and Community Development BSC California Building Standards Commission DSA-SS Division of the State Architect, Structural Safety OSHPD Office of Statewide Health Planning and Development LR Low Rise HR High Rise AA Additions and Alterations N New				1.EV Capable. Ten (10) percent of the total number of parking spaces on a building site, provided for all types of parking facilities, shall be electric vehicle charging spaces (EV spaces) capable of supporting future Level 2 EVSE. Electrical load calculations shall demonstrate that the electrical panel service capacity and electrical system, including any on-site distribution transformer(s), have sufficient capacity to simultaneously charge all EVs at all required EV spaces at a minimum of 40 amperes.				1.EV Capable. Ten (10) percent of the total number of parking spaces on a building site, provided for all types of parking facilities, shall be electric vehicle charging spaces (EV spaces) capable of supporting future Level 2 EVSE. Electrical load calculations shall demonstrate that the electrical panel service capacity and electrical system, including any on-site distribution transformer(s), have sufficient capacity to simultaneously charge all EVs at all required EV spaces at a minimum of 40 amperes.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			CHAPTER 4 RESIDENTIAL MANDATORY MEASURES				Exception: When EV chargers (Level 2 EVSE) are installed in a number greater than five (5) percent of parking spaces required by Section 4.106.4.2.2, Item 3, the number of EV capable spaces required may be reduced by a number equal to the number of EV chargers installed over the five (5) percent required.				Exception: When EV chargers (Level 2 EVSE) are installed in a number greater than five (5) percent of parking spaces required by Section 4.106.4.2.2, Item 3, the number of EV capable spaces required may be reduced by a number equal to the number of EV chargers installed over the five (5) percent required.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			SECTION 4.102 DEFINITIONS 4.102.1 DEFINITIONS The following terms are defined in Chapter 2 (and are included here for reference) FRENCH DRAIN. A trench, hole or other depressed area loosely filled with rock, gravel, fragments of brick or similar pervious material used to collect or channel drainage or runoff water. WATTLES. Wattles are used to reduce sediment in runoff. Wattles are often constructed of natural plant materials such as hay, straw or similar material shaped in the form of tubes and placed on a downflow slope. Wattles are also used for perimeter and inlet controls.				2.EV Ready. Twenty-five (25) percent of the total number of parking spaces shall be equipped with low power Level 2 EV charging receptacles. For multifamily parking facilities, no more than one receptacle is required per dwelling unit when more than one parking space is provided for use by a single dwelling unit. Exception: Areas of parking facilities served by parking lifts.				2.EV Ready. Twenty-five (25) percent of the total number of parking spaces shall be equipped with low power Level 2 EV charging receptacles. For multifamily parking facilities, no more than one receptacle is required per dwelling unit when more than one parking space is provided for use by a single dwelling unit. Exception: Areas of parking facilities served by parking lifts.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			4.106 SITE DEVELOPMENT 4.106.1 GENERAL. Preservation and use of available natural resources shall be accomplished through evaluation and careful planning to minimize negative effects on the site and adjacent areas. Preservation of slopes, management of storm water drainage and erosion controls shall comply with this section. 4.106.2 STORM WATER DRAINAGE AND RETENTION DURING CONSTRUCTION. Projects which disturb less than one acre of soil and are not part of a larger common plan of development which in total disturbs one acre or more, shall manage storm water drainage during construction. In order to manage storm water drainage during construction, one or more of the following measures shall be implemented to prevent flooding of adjacent property, prevent erosion and retain soil runoff on the site. <ol style="list-style-type: none">Retention basins of sufficient size shall be utilized to retain storm water on the site.Where storm water is conveyed to a public drainage system, collection point, gutter or similar disposal method, water shall be filtered by use of a barrier system, wattle or other method approved by the enforcing agency.Compliance with a lawfully enacted storm water management ordinance. Note: Refer to the State Water Resources Control Board for projects which disturb one acre or more of soil, or are part of a larger common plan of development which in total disturbs one acre or more of soil. (Website: https://www.waterboards.ca.gov/water_issues/programs/stormwater/construction.htm)				3.EV Chargers. Five (5) percent of the total number of parking spaces shall be equipped with Level 2 EVSE. Where common use parking is provided, at least one EV charger shall be located in the common use parking area and shall be available for use by all residents or guests. When low power Level 2 EV charging receptacles or Level 2 EVSE are installed beyond the minimum required, an automatic load management system (ALMS) may be used to reduce the maximum required electrical capacity to each space served by the ALMS. The electrical system and any on-site distribution transformers shall have sufficient capacity to deliver at least 3.3 kW simultaneously to each EV charging station (EVCS) served by the ALMS. The branch circuit shall have a minimum capacity of 40 amperes, and installed EVSE shall have a capacity of not less than 30 amperes. ALMS shall not be used to reduce the minimum required electrical capacity to the required EV capable spaces.				3.EV Chargers. Five (5) percent of the total number of parking spaces shall be equipped with Level 2 EVSE. Where common use parking is provided, at least one EV charger shall be located in the common use parking area and shall be available for use by all residents or guests. When low power Level 2 EV charging receptacles or Level 2 EVSE are installed beyond the minimum required, an automatic load management system (ALMS) may be used to reduce the maximum required electrical capacity to each space served by the ALMS. The electrical system and any on-site distribution transformers shall have sufficient capacity to deliver at least 3.3 kW simultaneously to each EV charging station (EVCS) served by the ALMS. The branch circuit shall have a minimum capacity of 40 amperes, and installed EVSE shall have a capacity of not less than 30 amperes. ALMS shall not be used to reduce the minimum required electrical capacity to the required EV capable spaces.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			4.106.3 GRADING AND PAVING. Construction plans shall indicate how the site grading or drainage system will manage all surface water flows to keep water from entering buildings. Examples of methods to manage surface water include, but are not limited to, the following: <ol style="list-style-type: none">SwalesWater collection and disposal systemsFrench drainsWater retention gardensOther water measures which keep surface water away from buildings and aid in groundwater recharge. Exception: Additions and alterations not altering the drainage path.				4.106.4.2.2.1 Location. EVCS shall comply with at least one of the following options: 1. The charging space shall be located adjacent to an accessible parking space meeting the requirements of the California Building Code, Chapter 11A, to allow use of the EV charger from the accessible parking space. 2. The charging space shall be located on an accessible route, as defined in the California Building Code, Chapter 2, to the building. Exception: Electric vehicle charging stations designed and constructed in compliance with the California Building Code, Chapter 11B, are not required to comply with Section 4.106.4.2.2.1.1 and Section 4.106.4.2.2.1.2, Item 3.				4.106.4.2.2.1 Location. EVCS shall comply with at least one of the following options: 1. The charging space shall be located adjacent to an accessible parking space meeting the requirements of the California Building Code, Chapter 11A, to allow use of the EV charger from the accessible parking space. 2. The charging space shall be located on an accessible route, as defined in the California Building Code, Chapter 2, to the building. Exception: Electric vehicle charging stations designed and constructed in compliance with the California Building Code, Chapter 11B, are not required to comply with Section 4.106.4.2.2.1.1 and Section 4.106.4.2.2.1.2, Item 3.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			4.106.4 Electric vehicle (EV) charging for new construction. New construction shall comply with Sections 4.106.4.1 or 4.106.4.2 to facilitate future installation and use of EV chargers. Electric vehicle supply equipment (EVSE) shall be installed in accordance with the California Electrical Code, Article 625. Exceptions: <ol style="list-style-type: none">On a case-by-case basis, where the local enforcing agency has determined EV charging and infrastructure are not feasible based upon one or more of the following conditions:<ol style="list-style-type: none">Where there is no local utility power supply or the local utility is unable to supply adequate power.Where there is evidence suitable to the local enforcing agency substantiating that additional local utility infrastructure design requirements, directly related to the implementation of Section 4.106.4, may adversely impact the construction cost of the project.Accessory Dwelling Units (ADU) and Junior Accessory Dwelling Units (JADU) without additional parking facilities. 4.106.4.1 New one- and two-family dwellings and townhouses with attached private garages. For each dwelling unit, install a listed raceway to accommodate a dedicated 208/240-volt branch circuit. The raceway shall not be less than trade size 1 (nominal 1-inch inside diameter). The raceway shall originate at the main service or subpanel and shall terminate into a listed cabinet, box or other enclosure in close proximity to the proposed location of an EV charger. Raceways are required to be continuous at enclosed, inaccessible or concealed areas and spaces. The service panel and/or subpanel shall provide capacity to install a 40-ampere 208/240-volt minimum dedicated branch circuit and space(s) reserved to permit installation of a branch circuit overcurrent protective device. Exception: A raceway is not required if a minimum 40-ampere 208/240-volt dedicated EV branch circuit is installed in close proximity to the proposed location of an EV charger at the time of original construction in accordance with the California Electrical Code.				1. The minimum length of each EV space shall be 18 feet (5486 mm). 2. The minimum width of each EV space shall be 9 feet (2743 mm). 3. One in every 25 charging spaces, but not less than one, shall also have an 8-foot (2438 mm) wide minimum aisle. A 5-foot (1524 mm) wide minimum aisle shall be permitted provided the minimum width of the EV space is 12 feet (3658 mm). a. Surface slope for this EV space and the aisle shall not exceed 1 unit vertical in 48 units horizontal (2.083 percent slope) in any direction.				1. The minimum length of each EV space shall be 18 feet (5486 mm). 2. The minimum width of each EV space shall be 9 feet (2743 mm). 3. One in every 25 charging spaces, but not less than one, shall also have an 8-foot (2438 mm) wide minimum aisle. A 5-foot (1524 mm) wide minimum aisle shall be permitted provided the minimum width of the EV space is 12 feet (3658 mm). a. Surface slope for this EV space and the aisle shall not exceed 1 unit vertical in 48 units horizontal (2.083 percent slope) in any direction.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
			4.106.4.1.1 Identification. The service panel or subpanel circuit directory shall identify the overcurrent protective device space(s) reserved for future EV charging as "EV CAPABLE". The raceway termination location shall be permanently and visibly marked as "EV CAPABLE".				4.106.4.2.3 EV space requirements. 1. Single EV space required. Install a listed raceway capable of accommodating a 208/240-volt dedicated branch circuit. The raceway shall not be less than trade size 1 (nominal 1-inch inside diameter). The raceway shall originate at the main service or subpanel and shall terminate into a listed cabinet, box or enclosure in close proximity to the proposed location of an EV charger. Raceways are required to be continuous at enclosed, inaccessible or concealed areas and spaces. The service panel and/or subpanel shall provide capacity to install a 40-ampere 208/240-volt minimum dedicated branch circuit and space(s) reserved to permit installation of a branch circuit overcurrent protective device. Exception: A raceway is not required if a minimum 40-ampere 208/240-volt dedicated EV branch circuit is installed in close proximity to the location or the proposed location of the EV space, at the time of original construction in accordance with the California Electrical Code.				4.106.4.2.3 EV space requirements. 1. Single EV space required. Install a listed raceway capable of accommodating a 208/240-volt dedicated branch circuit. The raceway shall not be less than trade size 1 (nominal 1-inch inside diameter). The raceway shall originate at the main service or subpanel and shall terminate into a listed cabinet, box or enclosure in close proximity to the location or the proposed location of an EV charger. Raceways are required to be continuous at enclosed, inaccessible or concealed areas and spaces. The service panel and/or subpanel shall provide capacity to install a 40-ampere 208/240-volt minimum dedicated branch circuit and space(s) reserved to permit installation of a branch circuit overcurrent protective device. Exception: A raceway is not required if a minimum 40-ampere 208/240-volt dedicated EV branch circuit is installed in close proximity to the location or the proposed location of the EV space, at the time of original construction in accordance with the California Electrical Code.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				
							2. Multiple EV spaces required. Construction documents shall indicate the raceway termination point and the location of installed or future EV spaces, receptacles or EV chargers. Construction documents shall also provide information on ampacity of installed or future receptacles or EVSE, raceway method(s), wiring schematics and electrical load calculations. Plan design shall be based upon a 40-ampere minimum branch circuit. Required raceways and related components that are planned to be installed underground, enclosed, inaccessible or in concealed areas and spaces shall be installed at the time of original construction.				2. Multiple EV spaces required. Construction documents shall indicate the raceway termination point and the location of installed or future EV spaces, receptacles or EV chargers. Construction documents shall also provide information on ampacity of installed or future receptacles or EVSE, raceway method(s), wiring schematics and electrical load calculations. Plan design shall be based upon a 40-ampere minimum branch circuit. Required raceways and related components that are planned to be installed underground, enclosed, inaccessible or in concealed areas and spaces shall be installed at the time of original construction.				Notes: 1. Construction documents are intended to demonstrate the project's capability and capacity for facilitating future EV charging. 2. There is no requirement for EV spaces to be constructed or available until receptacles for EV charging or EV chargers are installed for use.				



P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUELPTNICK@GMAIL.COM

REVISIONS	DATE

ARCHITECTURAL

**CALGREEN
RESIDENTIAL
CHECKLIST**

Scale: SEE DWG.

Drawn By: SBP

Job: -

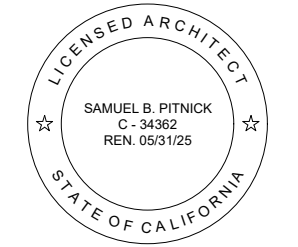
09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



1 EXISTING SITE PLAN
SCALE: 1/8" = 1'-0"

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

ARCHITECTURAL
EXISTING
SITE PLAN

Scale: SEE DWG.
Drawn By: SBP
Job: -

A1.0

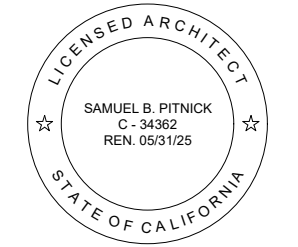
09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



- SITE PLAN LEGEND
- SLOPES 30% OR GREATER
 - EXTERIOR LED PATH LIGHT - BY 'SPJ LIGHTING - SQ100-1'
 - LED RECESSED LOW WALL / STEP LIGHT - BY 'HINKLEY LIGHTING - LUNA'
 - *SEE SHEET A1.2 FOR LIGHT FIXTURE CUT SHEETS

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK
ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

ARCHITECTURAL
PROPOSED
SITE PLAN

Scale: SEE DWG.
Drawn By: SBP
Job: -

A1.1

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.

Lightology

12V Luna Horizontal Step Light
ITEM NUMBER HIN360681



BRAND Hinkley Lighting

DESCRIPTION
Hinkley's 12V Luna Horizontal Step Light features a Bronze, Satin Black, Matte Bronze, Satin White, or Titanium finish. Made from die cast zinc aluminum alloy. Fits in standard junction box. Accessories included: Wiring kit. One 3.8 watt 12 volt LED module is included. 4.5 inch width x 3 inch height x 1.5 inch depth. UL listed for wet locations.



Shown in: Matte Bronze

SHADE COLOR	N/A
BODY FINISH	Matte Bronze
WATTAGE	3.8W
DIMMER	Low Voltage Magnetic
DIMENSIONS	4.5"W x 3"H x 1.5"D
LAMP	1 x LED/3.8W/12V LED
Technical Information	
LUMINOUS FLUX	200 lumens
LUMENS/WATT	52.63
LAMP COLOR	2700 K
COLOR RENDERING	90 CRI
ITEM NUMBER	HIN360681

1 PROPOSED EXTERIOR LOW WALL / STEP LIGHT
SCALE: N.T.S.

Lightology

SQ100-1 Outdoor Path Light 12V
ITEM NUMBER SPJ358094



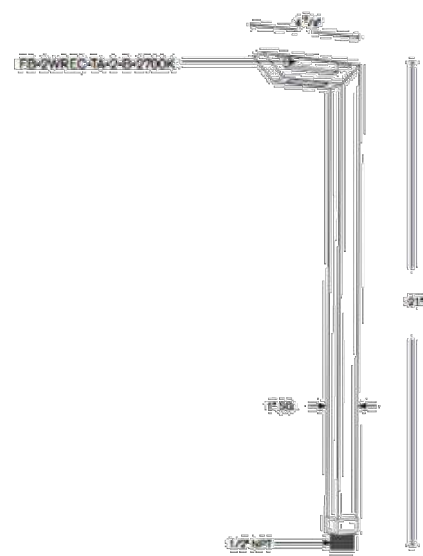
BRAND SPJ Lighting

DESCRIPTION
SQ100-1 Outdoor Path Light is made of solid brass with etched finishes in Verde, Rusty, Matte Bronze, Moss, Black, or Aged Brass. Includes one 2 watt, 12 volt Nichia Forever Bright LED, 2700K color temperature, 125 lumens. .5 inch NPT. Dual fin spike included. ETL listed. Suitable for wet locations. Made in USA. 20 year warranty. Dimensions: 4.9 inch width x 21 inch height. Requires magnetic transformer, sold separately.



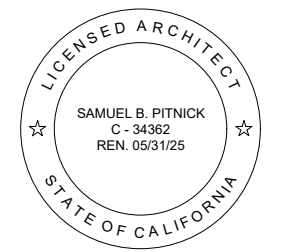
Shown in: Matte Bronze

SHADE COLOR	N/A
BODY FINISH	Matte Bronze
WATTAGE	2W
DIMMER	Low Voltage Magnetic
DIMENSIONS	4.9"W x 21"H
LED MODULE INCLUDED	
LAMP	1 x LED/2W/12V LED
Technical Information	
LUMINOUS FLUX	125 lumens
LUMENS/WATT	62.50
LAMP COLOR	2700 K
ITEM NUMBER	SPJ358094



2 PROPOSED EXTERIOR PATH LIGHT
SCALE: N.T.S.

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

ARCHITECTURAL

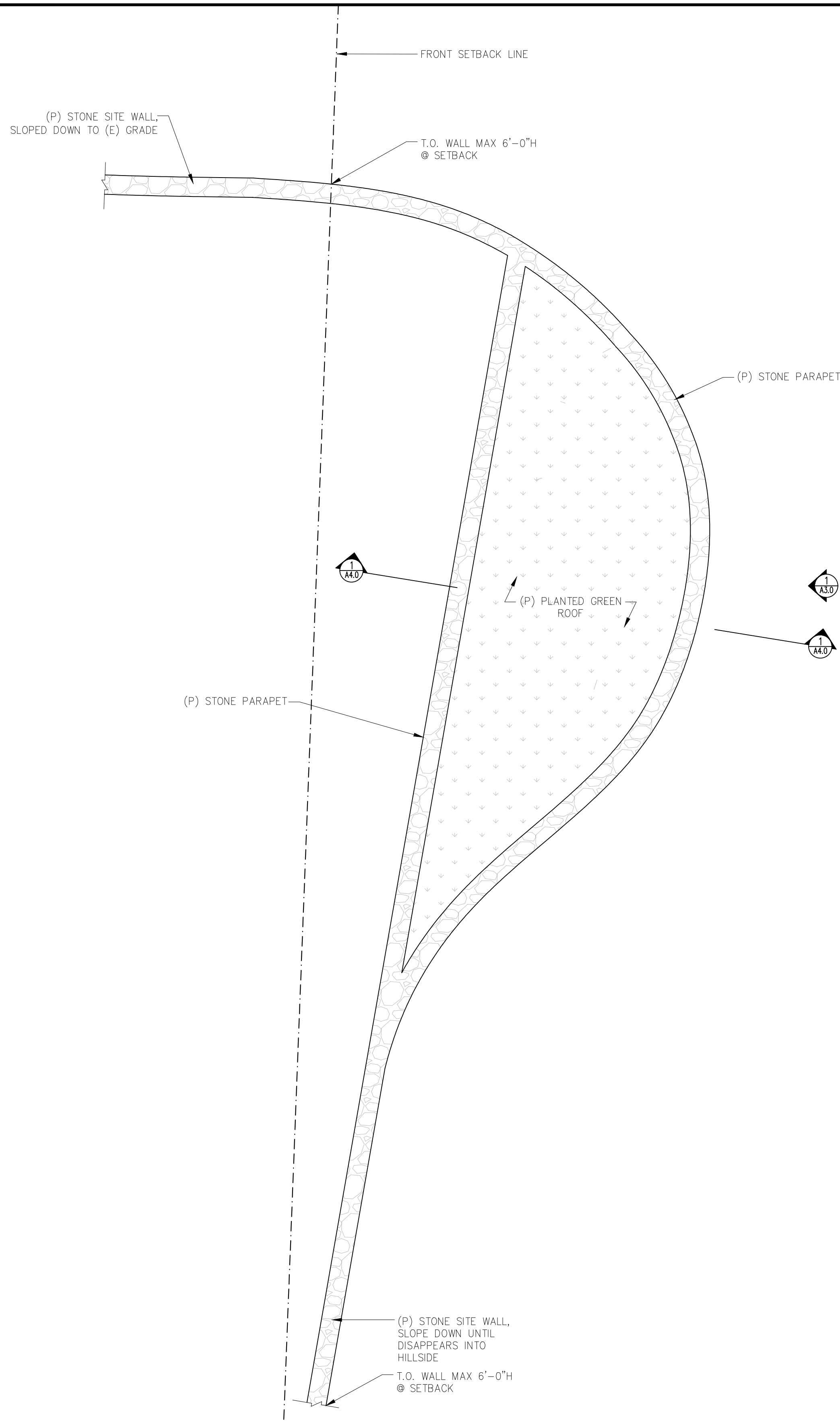
PROPOSED
SITE
LIGHTING

Scale: SEE DWG.
Drawn By: SBP
Job: -

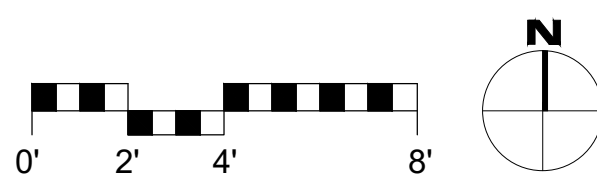
A1.2

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.

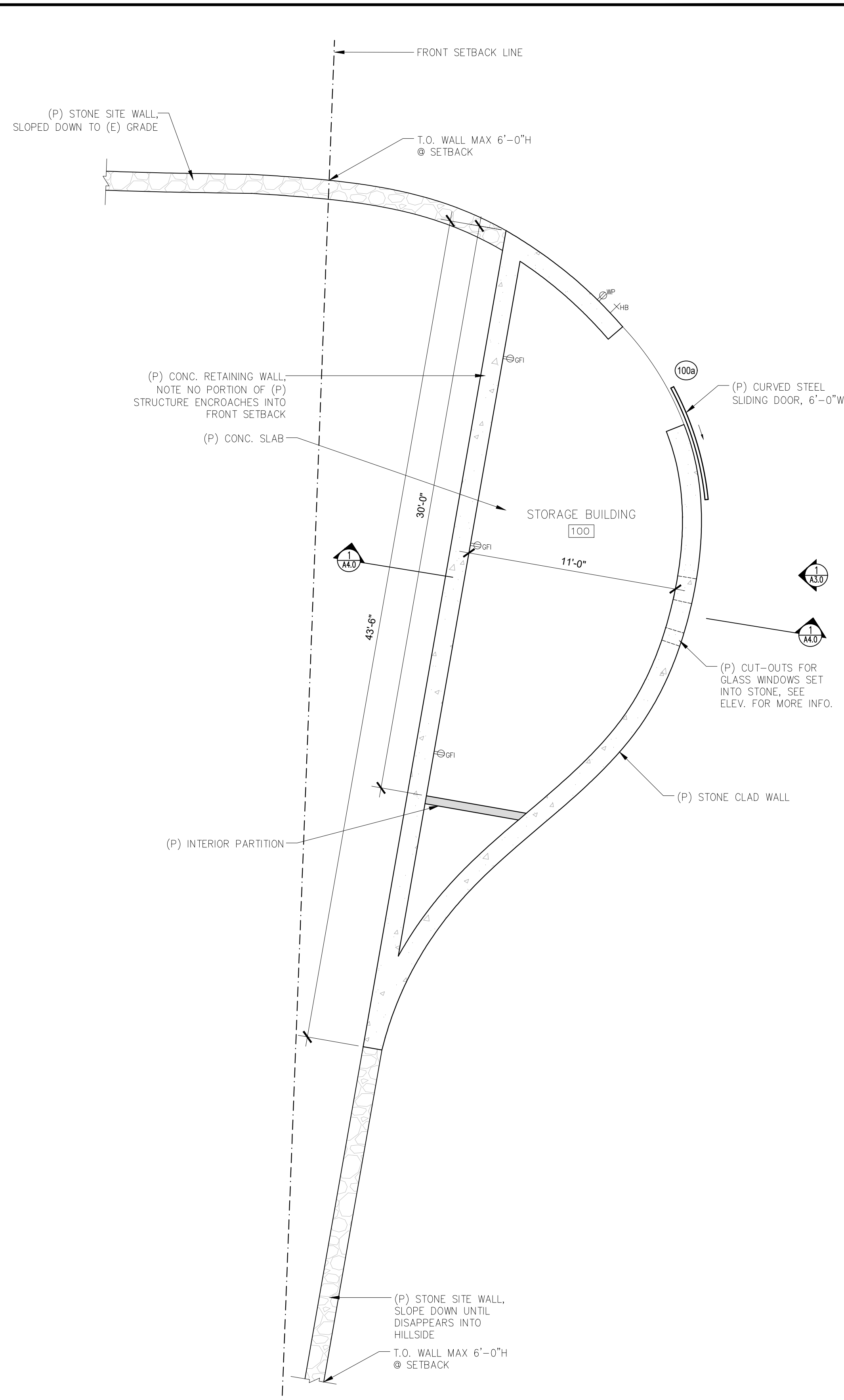


2 PROPOSED ROOF PLAN
SCALE: 1/4" = 1'-0"

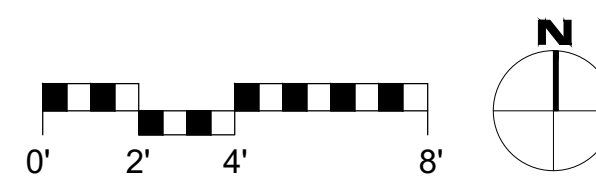


WALL TYPE LEGEND:
— (E) WALL TO REMAIN
- - (E) WALL TO REMOVE
— (P) WALL

FLOOR PLAN SYMBOLS LEGEND	
	GROUND FAULT CIRCUIT INTERRUPT OUTLET
	WATER PROOF EXTERIOR GFCI OUTLET / TAMPER RESISTANT, RECESSED INTO WALL
	HOSE BIBB W/ ANTI-SIPHON DEVICE, RECESSED INTO WALL

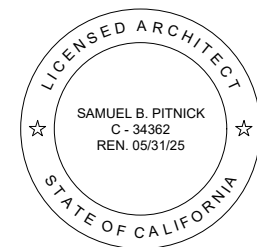


1 PROPOSED FLOOR PLAN
SCALE: 1/4" = 1'-0"



EXTERIOR DOOR SCHEDULE							
TAG	WIDTH	HEIGHT	THICK	DOOR MTL.	FRAME MTL.	TYPE	COMMENTS
100a	6'-0"	6'-8"	-	STEEL	STEEL	SLIDING / BARN	CUSTOM STEEL DOOR ON TRACK

FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

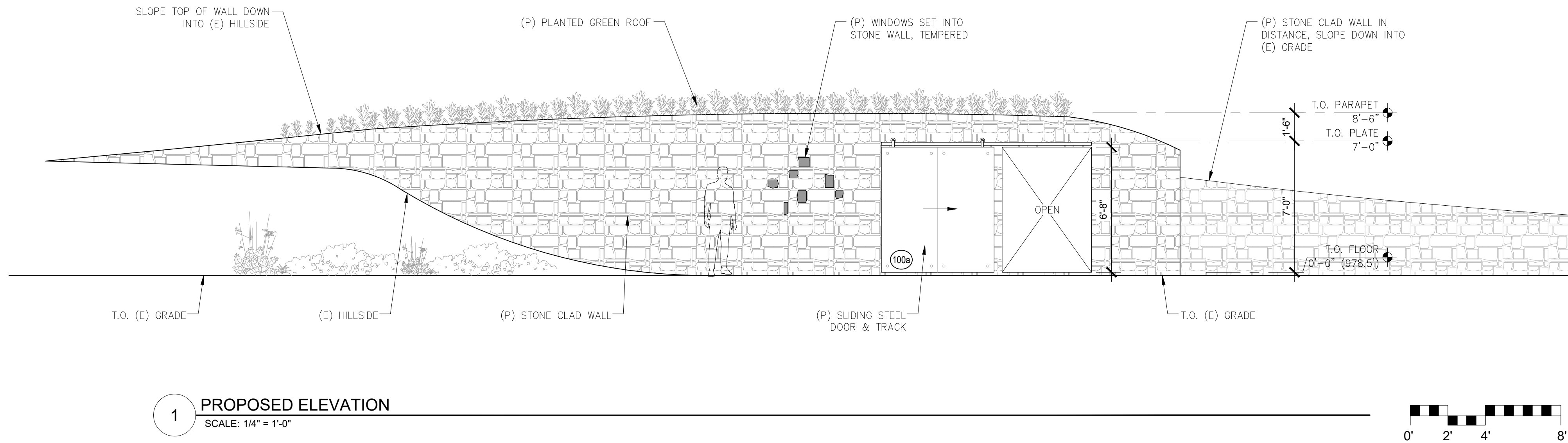
REVISIONS	DATE

ARCHITECTURAL
PROPOSED
FLOOR &
ROOF PLAN
Scale: SEE DWG.
Drawn By: SBP
Job: -

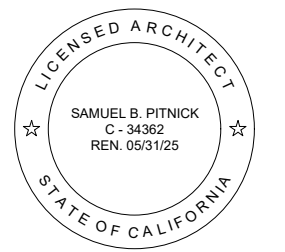
A2.0

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS	DATE

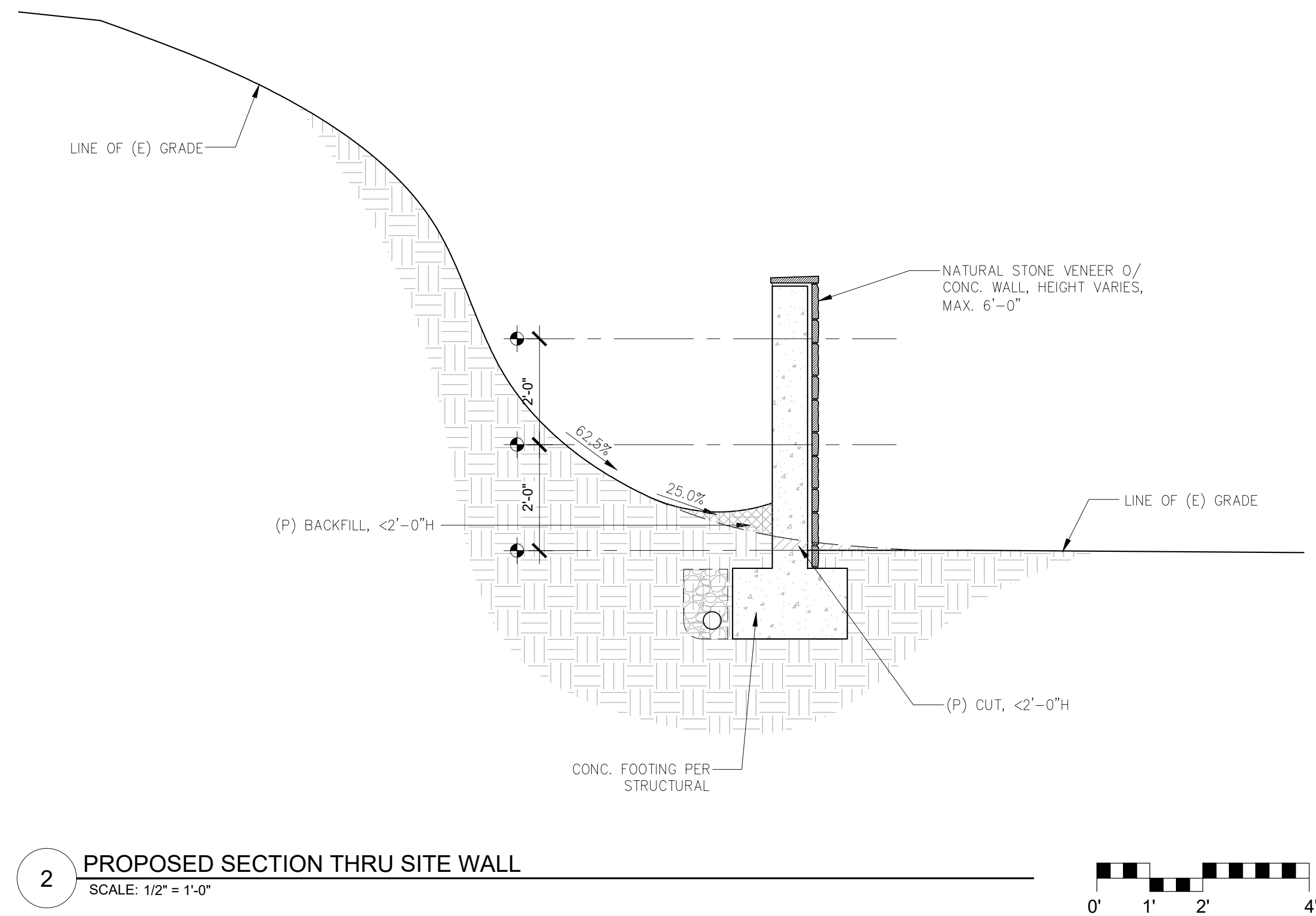
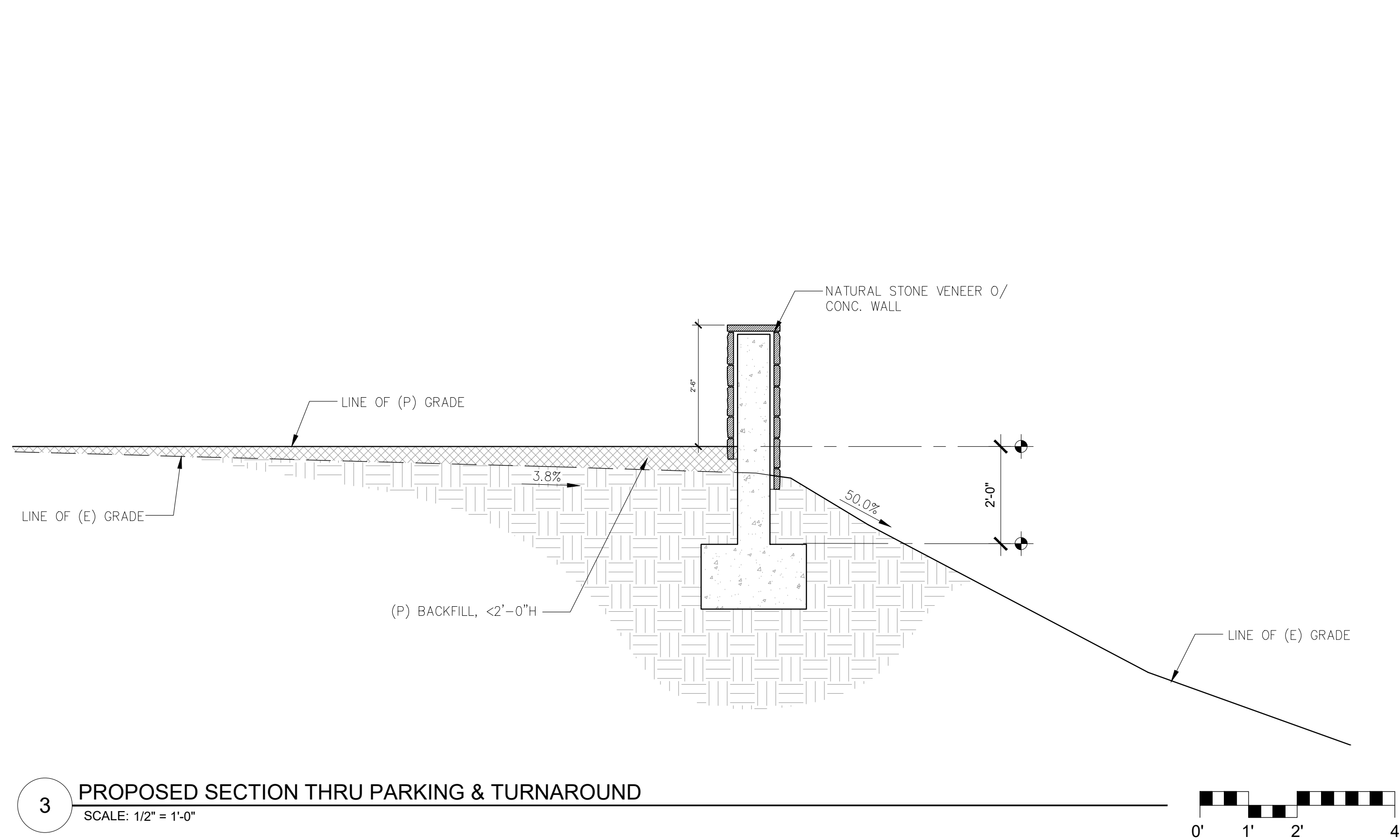
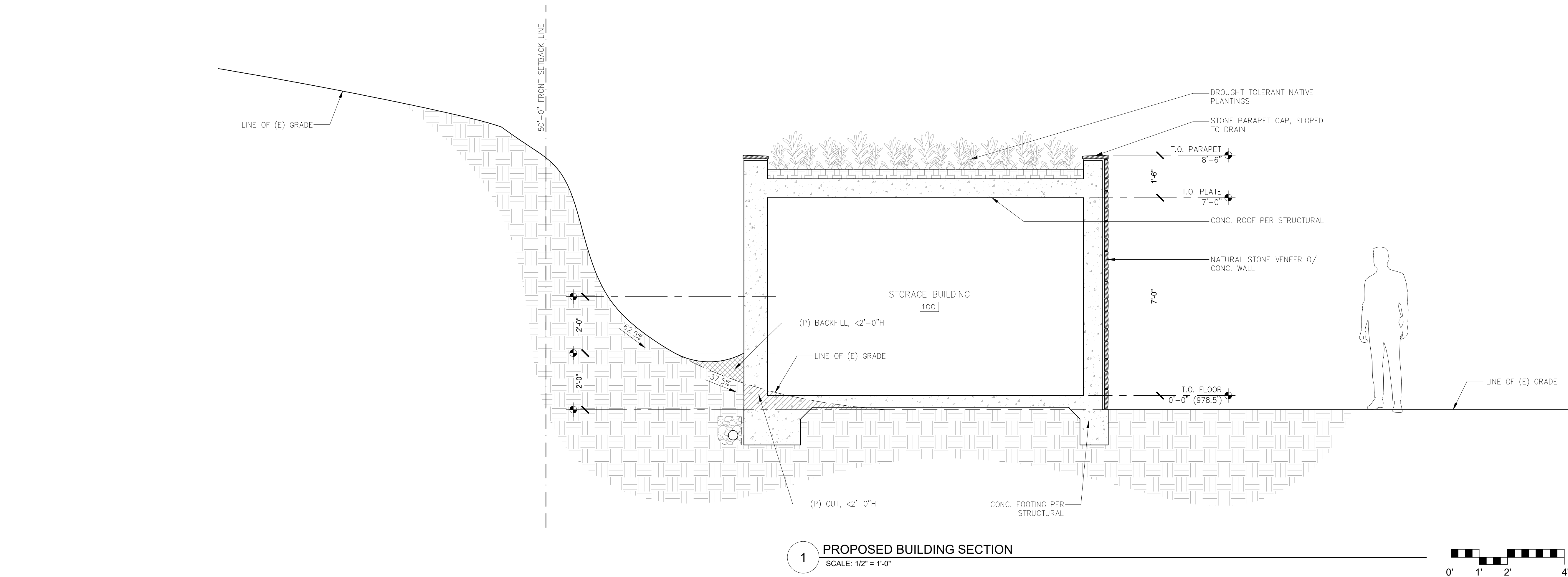
ARCHITECTURAL
BUILDING
ELEVATIONS

Scale: SEE DWG.
Drawn By: SBP
Job: -

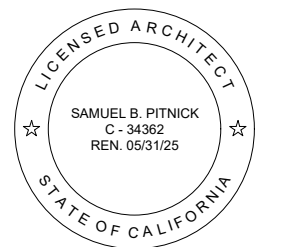
A3.0

09/08/2023

Use of these plans and specifications shall be restricted to the original site for which they were prepared and publication thereof is expressly limited to such use. Reproduction or publication by any method, in whole or in part, is prohibited. Title to the plans and specifications remains with the architect without prejudice. Visual contact with these plans and specifications shall constitute prima facie evidence of the acceptance of these restrictions.



FOWLER
RESIDENCE
46402 CLEAR RIDGE RD.
BIG SUR, CA
93920



SAMUEL PITNICK ARCHITECTS
P.O. BOX 22412, CARMEL, CA 93922
PHONE: (831) 241-1895
EMAIL: SAMUEL@PITNICK.COM

REVISIONS DATE

ARCHITECTURAL

PROPOSED
BUILDING
SECTIONS

Scale: SEE DWG.
Drawn By: SBP
Job: -

A4.0

09/08/2023