

MONTEREY COUNTY RESOURCE MANAGEMENT AGENCY

Carl P. Holm, AICP, Director



LAND USE & COMMUNITY DEVELOPMENT | PUBLIC WORKS & FACILITIES | PARKS

1441 Schilling Place, South 2nd Floor
Salinas, California 93901-4527

(831)755-4800

www.co.monterey.ca.us/rma

MEMORANDUM

Date: July 28, 2017

To: California Transportation Commission
Susan Bransen, Executive Director

From: Brenda Villanueva, Assistant Engineer

Subject: MBSST – Moss Landing Segment Bicycle/Pedestrian Path & Bridge Project
Application Update

This memo is to notify the Commission that no significant changes were made to the original ATP Application for MBSST – Moss Landing Segment Bicycle/Pedestrian Path & Bridge Project. All funding remains the same and dates for project completion will still be met as outlined in the original application.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised April 2015)

General Instructions

<input checked="" type="checkbox"/> New Project					Date:	7/28/17
District	EA	Project ID	PPNO	MPO ID	TCRP No.	
05						
County	Route/Corridor	PM Bk	PM Ahd	Project Sponsor/Lead Agency		
MON				AMBAG		
				MPO	Element	
				AMBAG	Local Assistance	
Project Manager/Contact		Phone		E-mail Address		
Mohammad Qureshi		(831)796-3009		qureshiM@co.monterey.ca.us		
Project Title						
Las Lomas Bicycle Lane and Pedestrian Project						
Location, Project Limits, Description, Scope of Work						<input type="checkbox"/> See page 2
Located in the unincorporated community of Moss Landing. The project extends from the North Harbor on the northwest side of the existing State Highway 1 to Moss Landing Road. The project construction consists of the construction of a 10 ft pave bicycle path with 2 ft wide decomposed granit shoulders on either side of the path and a 12 ft wide, 386 ft long pedestrian/bicycle bridge for a distance of 4,466 ft (0.85 miles).						
<input checked="" type="checkbox"/> Includes ADA Improvements			<input checked="" type="checkbox"/> Includes Bike/Ped Improvements			
Component	Implementing Agency					
PA&ED	Monterey County Resource Management Agency					
PS&E	Monterey County Resource Management Agency					
Right of Way	Monterey County Resource Management Agency					
Construction	Monterey County Resource Management Agency					
Purpose and Need						<input type="checkbox"/> See page 2
Currently the existing route for pedestrians and bicycles forces them to walk or ride along the narrow shoulders of State Highway 1. State Highway 1 has a daily AADT of over 35,000 vehicles traveling at speeds of 55mph or greater. This project will provide a safe zone for the movement of pedestrians and bicyclists who are traveling for enjoyment or the view, commuting to work, research facilities, beaches, parks, dunes, wetlands, and bus stops.						
Project Benefits						<input type="checkbox"/> See page 2
The construction of a separate Class 1 paved bicycle path and pedestrian/bicycle bridge meets ATP program goals of addressing the safety and mobility of non-motorized users. It supports the sustainable community strategy by enhancing public safety, reducing greenhouse gases, and increasing biking and walking trips in the disadvantaged community of Moss Landing.						
<input checked="" type="checkbox"/> Supports Sustainable Communities Strategy (SCS) Goals			<input checked="" type="checkbox"/> Reduces Greenhouse Gas Emissions			
Project Milestone						Proposed
Project Study Report Approved						
Begin Environmental (PA&ED) Phase						09/24/08
Circulate Draft Environmental Document				Document Type	N/A	
Draft Project Report						
End Environmental Phase (PA&ED Milestone)						09/18/15
Begin Design (PS&E) Phase						08/31/09
End Design Phase (Ready to List for Advertisement Milestone)						07/20/17
Begin Right of Way Phase						10/01/14
End Right of Way Phase (Right of Way Certification Milestone)						03/19/17
Begin Construction Phase (Contract Award Milestone)						09/02/19
End Construction Phase (Construction Contract Acceptance Milestone)						11/19/21
Begin Closeout Phase						04/01/19
End Closeout Phase (Closeout Report)						04/01/22

ADA Notice

For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised April 2015)

Date: 7/28/17

District	County	Route	EA	Project ID	PPNO	TCRP No.
05	MON					
Project Title: Las Lomas Bicycle Lane and Pedestrian Project						

Proposed Total Project Cost (\$1,000s)									Notes
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	
E&P (PA&ED)	795							795	
PS&E	1,952							1,952	
R/W SUP (CT)	305							305	
CON SUP (CT)									
R/W									
CON					21,375			21,375	
TOTAL	3,052				21,375			24,427	

Fund No. 1:	ATP								Program Code
Proposed Funding (\$1,000s)									20.30.720
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)									CALTRANS
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON					7,587			7,587	
TOTAL					7,587			7,587	

Fund No. 2:	NON ATP								Program Code
Proposed Funding (\$1,000s)									NON ATP
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)	277							277	TAMC
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL	277							277	

Fund No. 3:	NON ATP								Program Code
Proposed Funding (\$1,000s)									NON ATP
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)	518							518	STATE
PS&E	1,952							1,952	
R/W SUP (CT)	305							305	
CON SUP (CT)									
R/W									
CON					12,288			12,288	
TOTAL	2,775				12,288			15,063	

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised April 2015)

Date: 7/28/17

District	County	Route	EA	Project ID	PPNO	TCRP No.
05	MON					
Project Title: Las Lomas Bicycle Lane and Pedestrian Project						

Fund No. 4:		Non ATP							Program Code	
		Proposed Funding (\$1,000s)							NON ATP	
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency	
E&P (PA&ED)									Pending Coastal Conservancy	
PS&E										
R/W SUP (CT)										
CON SUP (CT)										
R/W										
CON					1,500			1,500		
TOTAL					1,500			1,500		

Fund No. 5:									Program Code	
		Proposed Funding (\$1,000s)								
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency	
E&P (PA&ED)										
PS&E										
R/W SUP (CT)										
CON SUP (CT)										
R/W										
CON										
TOTAL										

Fund No. 6:									Program Code	
		Proposed Funding (\$1,000s)								
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency	
E&P (PA&ED)										
PS&E										
R/W SUP (CT)										
CON SUP (CT)										
R/W										
CON										
TOTAL										

Fund No. 7:									Program Code	
		Proposed Funding (\$1,000s)								
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency	
E&P (PA&ED)										
PS&E										
R/W SUP (CT)										
CON SUP (CT)										
R/W										
CON										
TOTAL										

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised April 2015)

Date: 7/28/17

District	County	Route	EA	Project ID	PPNO	TCRP No.
05	MON					
Project Title: Las Lomas Bicycle Lane and Pedestrian Project						

Fund No. 8:									Program Code
Proposed Funding (\$1,000s)									
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									

Fund No. 9:									Program Code
Proposed Funding (\$1,000s)									
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									

Fund No. 10:									Program Code
Proposed Funding (\$1,000s)									
Component	Prior	16/17	17/18	18/19	19/20	20/21	21/22+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									



ACTIVE TRANSPORTATION PROGRAM

IMPLEMENTING AGENCY: Monterey County Resource Management Agency Department of Public Works



PROJECT APPLICATION NO.: 5-Monterey County Resource Management Agency Department of Public Works-1

PROJECT NAME: MBSST- Moss Landing Segment Bicycle/Pedestrian Path & Bridge Project

PROJECT DESCRIPTION: The Project is approximately 4,466 ft (0.85 mile) consisting of the construction of a 10 foot paved bicycle path with 2 foot wide decomposed granite shoulders on either side of the path and a 12 foot wide 386 foot long pedestrian/bicycle bridge.

PROJECT LOCATION: North Monterey County in the community of Moss Landing extending from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road.

ATP FUNDED COMPONENTS							
Infrastructure				CON	Non-Infrastructure	Plan	
PA&ED	PS&E	R/W					
\$ -	\$ -	\$ -	\$ 7,587	\$ -	\$ -		
FY -	FY -	FY -	FY 19/20	FY -	FY -		

PROJECT FUNDING INFORMATION (1,000s)							
Total Project \$	Total ATP \$	Total Non-ATP \$	Past ATP \$	Leveraging \$	Matching \$	Non-Participating \$	Future Local \$
13,427	7,587	5,840	-	5,840	-	-	-

ADA Notice

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Application Part 1: Applicant Information

Implementing Agency: This agency must enter into a Master Agreement with Caltrans and will be financially and contractually responsible for the delivery of the project within all pertinent Federal and State funding requirements, including being responsible and accountable for the use and expenditure of program funds. This agency is responsible for the accuracy of the technical information provided in the application and is required to sign the application.

IMPLEMENTING AGENCY'S NAME:

Monterey County Resource Management Agency Department of Public Works

IMPLEMENTING AGENCY'S ADDRESS

168 W. Alisal Street, 2nd Floor

CITY

Salinas

ZIP CODE

CA 93901

IMPLEMENTING AGENCY'S CONTACT PERSON:

Ogarita Carranza

CONTACT PERSON'S TITLE:

Management Analyst II

CONTACT PERSON'S PHONE NUMBER:

831-755-5174

CONTACT PERSON'S EMAIL ADDRESS :

carranza@co.monterey.ca.us

Applicants have the opportunity to insert a project picture, agency seal, or other image on the cover page. If you would like to do this, attach the image (*.jpg, *.bmp, *.png, etc) by clicking in the box.



X

MASTER AGREEMENTS (MAs):

Does the Implementing Agency currently have a MA with Caltrans?

Yes No

Implementing Agency's Federal Caltrans MA number

05-5944R

Implementing Agency's State Caltrans MA number

00518

* Implementing Agencies that do not currently have a MA with Caltrans, must be able to meet the requirements and enter into an MA with Caltrans prior to funds allocation. The MA approval process can take 6 to 12 months to complete and there is no guarantee the agency will meet the requirements necessary for the State to enter into a MA with the agency. Delays could also result in a failure to meeting the CTC Allocation timeline requirements and the loss of ATP funding.

Project Partnering Agency:

The "Project Partnering Agency" is defined as an agency, other than Implementing Agency, that will assume the responsibility for the ongoing operations and maintenance of the improved facility. The Implementing Agency must: 1) ensure the Partnering Agency agrees to assume responsibility for the ongoing operations and maintenance of the improved facility, 2) provide documentation of the agreement (e.g., letter of intent) as part of the project application, and 3) ensure a copy of the Memorandum of Understanding or Interagency Agreement between the parties is submitted with the first request for allocation. For these projects, the Project Partnering Agency's information shall be provided below.

Based on the definition above, does this project have a partnering agency?

Yes No



Application Part 2: General Project Information

PROJECT NAME: (Max of 10 Words) (To be used in the CTC project list)

Words Remaining:

MBSST- Moss Landing Segment Bicycle/Pedestrian Path & Bridge Project

SUMMARY OF PROJECT SCOPE: (Max of 200 Words)

(Summary of the Existing Condition, Project Scope, the Expected Benefits)

Words Remaining:

The project is intended to provide safe pedestrian access between the north and south ends of Moss Landing. Currently, the existing route is inadequate for bicyclists and pedestrians, forcing them to ride and walk along the narrow shoulders of State Highway 1. With a high annual average daily traffic (AADT) volume of over 35,000 traveling at speeds in excess of 55 miles per hour, SR-1 creates a dangerous situation for pedestrians traveling along the shoulder. The project will offer the residents, commuters, bicyclists and walkers continuous, convenient and safe access to employment centers, multi- purpose campuses, research facilities, scenic views, natural resources, beaches, parks, dunes, wetlands, and bus stops.

The Project consists of constructing a separate, Class 1 paved bicycle path and pedestrian/bicycle bridge connecting the disadvantaged community of Moss Landing meeting the ATP program goals of addressing the safety and mobility of non-motorized users, ensuring benefits to disadvantaged communities, reducing greenhouse gas, and increasing biking/walking trips.

In addition to these benefits, the project also promotes and improves public health and wellness. The trail will encourage people to ride their bicycles or walk between the various destinations between north and south Moss Landing.

PROJECT DESCRIPTION: (Max of 50 Words)

Words Remaining:

The Project is approximately 4,466 ft (0.85 mile) consisting of the construction of a 10 foot paved bicycle path with 2 foot wide decomposed granite shoulders on either side of the path and a 12 foot wide 386 foot long pedestrian/bicycle bridge.

PROJECT LOCATION: (Max of 50 Words)

Words Remaining:

North Monterey County in the community of Moss Landing extending from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road.

In addition to the Location Description provided, attach a location map to the application. The location needs to show the project boundaries in relation to the Implementing Agency's boundaries.

MBSST Location Map.pdf

Project Coordinates: (latitude/longitude in decimal format) Lat. N /long. W

Congressional District(s):

State Senate District(s):

State Assembly District(s):

Caltrans District:

County:

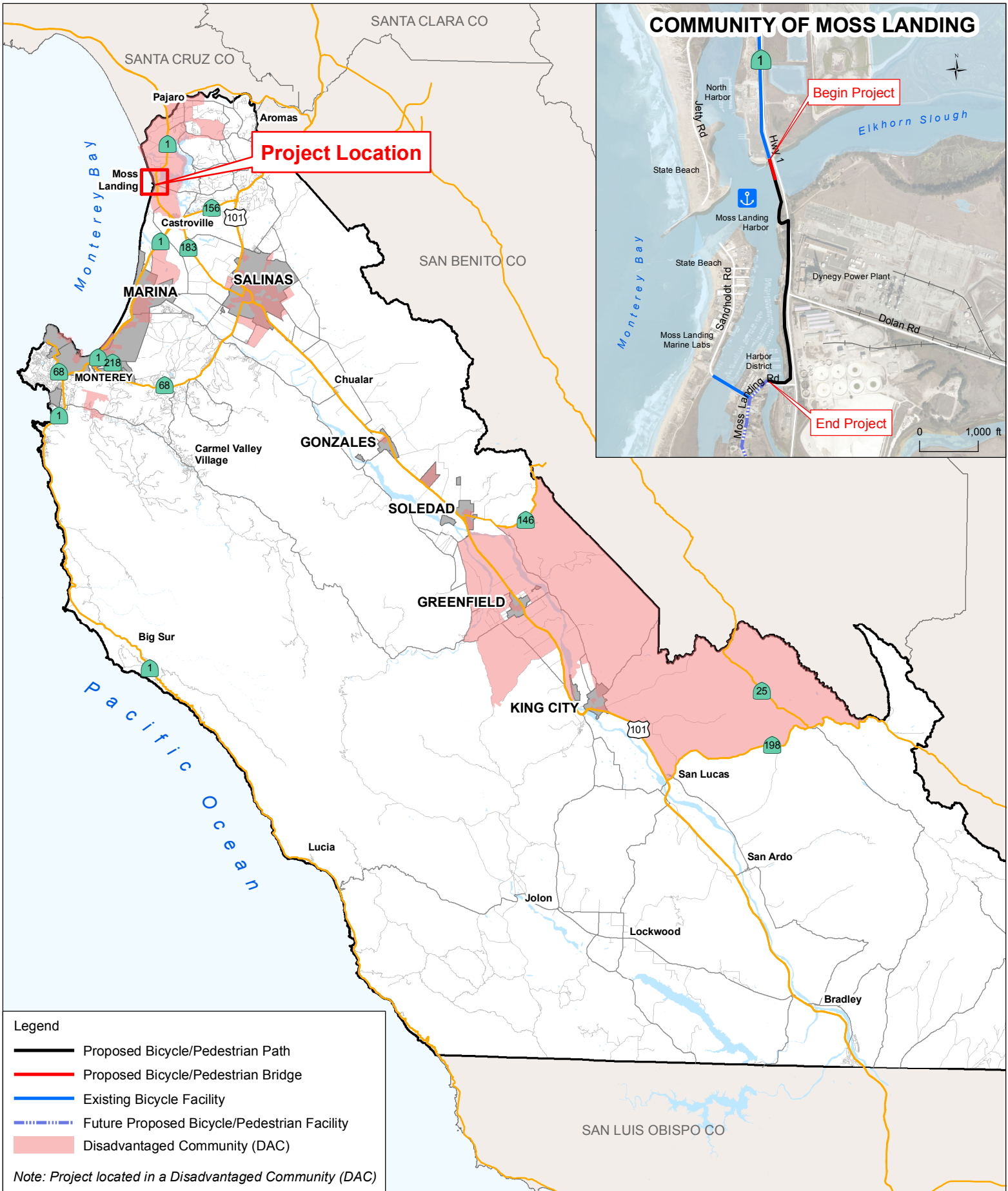
MPO:

RTPA:

Urbanized Zone Area (UZA) Population:

Past Projects: Within the last 10 years, has there been any previous State or Federal ATP, SRTS, SR2S, BTA or other ped/bike funding awards for a project(s) that are adjacent to or overlap the limits of project scope of this application?

Yes No



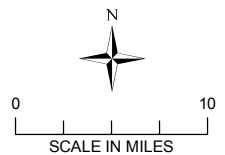
- Legend**
- Proposed Bicycle/Pedestrian Path
 - Proposed Bicycle/Pedestrian Bridge
 - Existing Bicycle Facility
 - Future Proposed Bicycle/Pedestrian Facility
 - Disadvantaged Community (DAC)

Note: Project located in a Disadvantaged Community (DAC)



County of Monterey
 Resource Management Agency
 Public Works Department
 April 5, 2016

ATTACHMENT D MBSST LOCATION MAP MONTEREY COUNTY, CA





Application Part 3: Project Type

PROJECT TYPE: (Use the drop down menu to select Combination (I/NI), Infrastructure (I), Non-Infrastructure (NI), or Plan.

Infrastructure (I)

Indicate any of the following plans that your agency currently has: (Check all that apply)

Bicycle Plan Pedestrian Plan Safe Routes to School Plan Active Transportation Plan

PROJECT SUB-TYPE (check all Project Sub-Types that apply):

- Bicycle Transportation** % of Project 90 %
- Pedestrian Transportation** % of Project 10 %
- Safe Routes to School** *(Also fill out Bicycle and Pedestrian Sub-Type information above)*

For a project to qualify for Safe Routes to School designation, the project must directly increase safety and convenience for public school students to walk and/or bike to school. Safe Routes to Schools infrastructure projects must be located within two miles of a public school or within the vicinity of a public school bus stop and the students must be the intended beneficiaries of the project. Other than traffic education and enforcement activities, non-infrastructure projects do not have a location restriction.

Projects with Safe Routes to School elements must fill out "School and Student Details" later in this application. As a condition of receiving funding, projects with Safe Routes to School Elements must commit to completing additional before and after student surveys as defined in the Caltrans Active Transportation Guidelines (LAPG Chapter 22).

- Trails (Multi-use and Recreational):** *(Also fill out Bicycle and Pedestrian Sub-Type information above)*

Trails Projects constructing multi-purpose trails are generally eligible in the Active Transportation Program. If the applicant believes all or part of their project meets the federal requirements of the Recreational Trails Program they are encouraged to seek a determination from the California Department of Parks and Recreation on the eligibility of their project to complete for this funding. This is optional but recommended because some trails projects may compete better under this funding program.

For all trails projects:

Do you feel a portion of your project is eligible for federal Recreational Trail funding? Yes No

If yes, estimate the total project costs that are eligible for the Recreational Trail funding: \$6,677,000

If yes, estimate the % of the total project costs that serve "transportation" uses? 100 %

Applicants intending to pursue "Recreational Trails Program funding" **must submit** the required information to the California Department of Parks and Recreation prior to the ATP application submissions deadline. (See the Application Instructions for details)

**Recreational Trail funding can only fund work outside of the roadway Right-of-way.*



Application Part 4: Project Details

INFRASTRUCTURE TYPE (Only Intended for Infrastructure Projects)

Note: When quantifying the amount of Active Transportation improvements proposed by the project, **do not double-count the improvements** that benefit both Bicyclists and Pedestrians (i.e. new RRFB/Signal should only show as a Pedestrian or Bicycle Improvement).

Bicycle Improvements

Pedestrian Improvements

Multi-use Trail Improvements

Class 1 Trails: New (8' or less wide): _____ Linear Feet New (over 8' wide): 4,466 Linear Feet

Widen/Reconstruct Existing: _____ Linear Feet

Non-Class 1 Trails: New: _____ Linear Feet Widen/Reconstruct Existing: _____ Linear Feet

Other Trail Improvements: #1: Pedestrian/Bicycle Bridge - 386 LF #: 1 #2: _____ #: _____

Vehicular-Roadway Traffic-Calming Improvements

ATP CYCLE 3 APPLICATION FORM

DLA-001 (NEW 4/2016)
v1.2



Right of Way (R/W) Impacts (Check all that apply)

- Project is 100% within the Implementing Agency's R/W (or within their control at the time of this application submittal).
- Project will likely require R/W and/or easements from private owners or will require utility relocations from 'non-public' utility companies.

The federal R/W process involving private property acquisitions and/or private utility relocations can often take 18 to 24 months. The project schedule in the application for R/W needs to reflect the necessary time to complete the federal R/W process.

- Project will likely require R/W, Easements, encroachment and/or approval involving Governmental, Environmental, or Railroad owner's property.

**See the application instructions for more details on the required coordination and documentation from these agencies.*

Attach a letter of support or neutrality from each separate agency. Combine all letters in one pdf attachment.

Co of Monterey Resource Mgmt Agency Ogarita Carranza 051716.pdf

DEPARTMENT OF TRANSPORTATION

50 HIGUERA STREET
SAN LUIS OBISPO, CA 93401-5415
PHONE (805) 549-3281
FAX (805) 549-3329
TTY 711
<http://www.dot.ca.gov/dist05/>



*Serious drought.
Serious drought
Help save water!*

May 17, 2016

Ms. Ogarita Carranza
Management Analyst II
County of Monterey Resource Management Agency
Department of Public Works
168 West Alisal Street 2nd Floor
Salinas, CA 93901-2438

Dear Ms. Carranza:

This letter is acknowledgement of the Active Transportation Program Cycle 3 (ATP) Grant Application process for the "Monterey Bay Sanctuary Scenic Trail (MBSST) - Moss Landing Segment Class I Bicycle/Pedestrian Path and Bridge Project" to construct multimodal facilities along Highway 1. We appreciate your efforts to improve connectivity for bicycles and pedestrians along this portion of the state highway system.

Based on preliminary reviews of the general project scope received by District 5 Traffic Operations, the improvement concept is acceptable. An Encroachment Permit will be required for all work within the state highway right of way. Please be aware that in addition to the subsequent reviews and the approval process required for work within state highway right of way, a Maintenance Agreement is required to be executed prior to the issuance of an Encroachment Permit.

Sincerely,

A handwritten signature in blue ink that reads "Sara Von Schwind".

SARA VON SCHWIND, P.E., PMP
District 5 Deputy District Director
Maintenance and Operations



Application Part 5: Project Schedule

- NOTES: 1) Per CTC Guidelines, all project applications must be submitted with the expectation of receiving federal funding and therefore the schedule below must account for the extra time needed for federal project delivery requirements and approvals, including a NEPA environmental clearance and for each CTC allocation there must also be a Notice to Proceed with Federally Reimbursable work.
- 2) Prior to estimating the durations of the project delivery tasks (below), applicants are highly encouraged to review the appropriate chapters of the Local Assistance Procedures Manual and work closely with District Local Assistance Staff.
- 3) The proposed CTC allocation dates must be between July 1, 2019 and June 30, 2021 to be consistent with the available ATP funds for Cycle 3.

INFRASTRUCTURE PROJECTS:

PA&ED Project Delivery Phase:

Will ATP funds be used in this phase of the project? Yes No

Expected or Past Start Date for PA&ED activities:

9/24/2008

Time to complete the separate CEQA & NEPA studies/approvals:

85	months	(See note #2, above)
----	--------	----------------------

Expected or Past Completion Date for the PA&ED Phase:

9/18/2015

* Applications showing the PA&ED phase as complete, must include/attach the signature pages for the CEQA and NEPA documents, which include project descriptions covering the full scope.

CEQA-NEPA.pdf

PS&E Project Delivery Phase:

Will ATP funds be used in this phase of the project? Yes No

Expected or Past Start Date for PS&E activities:

8/31/2009

Time to complete the final Plans, Specification & Estimate:

96	months
----	--------

Expected or Past Completion Date for the PS&E Phase:

7/20/2017

* Applications showing the PS&E phase as complete, must include/attach the signed & Stamped Title Sheet for the plans and approval page of the specifications.

MBSST Project Delivery.pdf

Right of Way Project Delivery Phase:

Will ATP funds be used in this phase of the project? Yes No

Expected or Past Start Date for R/W activities:

10/1/2014

Time to complete the R/W Engineering, Acquisition, and Utilities:

30	months
----	--------

Expected or Past Completion Date for the R/W Phase:

3/19/2017

* PS&E and Right of Way phases can be allocated at the same CTC meeting.

* Applications showing the R/W phase as complete, must include/attach the Caltrans approved R/W Certification.

MBSST Project Delivery.pdf

Construction Project Delivery Phase:

Will ATP funds be used in this phase of the project? Yes No

Proposed CTC "CON Allocation" Date:

7/1/2019

Notice to Proceed with Federally Reimbursable ATP Work:

8/30/2019

Expected Start Date for Construction activities:

9/2/2019

Time to complete the Construction activities:

27	months
----	--------

Expected or Past Completion Date for the CON Phase:

11/19/2021

Proposed Dates for "Before" and "After" Counts (As required by the CTC and Caltrans guidelines):

Expected Date for "Before" counts (Ideally, within 12 months of the beginning of the Construction Activities)

4/1/2019

Expected Date for "After" counts (Ideally, at least 6 months after the end of all Construction Activities)

4/1/2022



Monterey County

168 West Alisal Street,
1st Floor
Salinas, CA 93901
831.755.5066

Board Order

Upon motion of Supervisor Potter, seconded by Supervisor Armenta and carried by those members present, the Board of Supervisors hereby:

Adopted **Resolution 15-247** to:

- a. Adopt the final Mitigated Negative Declaration for the Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project, Project No. 8868, Federal Aid Project No. HP – 5944 (083, 084, 085, 089, 108, and 112) (SCH No. 2014121057);
- b. Adopt a Mitigation and Monitoring Reporting Program;
- c. Authorize the Resource Management Agency (RMA) – Director of Public Works or designee to proceed with the Monterey Bay Sanctuary Scenic Trail - Moss Landing Bicycle/Pedestrian Path and Bridge Project; and
- d. Authorize the RMA - Director of Public Works or designee to submit an application for a Coastal Development Permit to the Californian Coastal Commission.
[(APN 133-181-009, 133-173-005, 133-173-002)]

PASSED AND ADOPTED on this 15th day of September 2015, by the following vote, to wit:

AYES: Supervisors Armenta, Phillips, Salinas, Parker and Potter

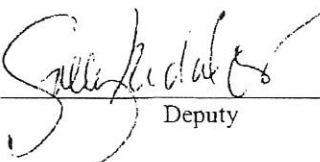
NOES: None

ABSENT: None

I, Gail T. Borkowski, Clerk of the Board of Supervisors of the County of Monterey, State of California, hereby certify that the foregoing is a true copy of an original order of said Board of Supervisors duly made and entered in the minutes thereof of Minute Book 78 for the meeting on September 15, 2015.

Dated: September 18, 2015
File ID: RES 15-088

Gail T. Borkowski, Clerk of the Board of Supervisors
County of Monterey, State of California

By 
Deputy

Before the Board of Supervisors in and for the County of Monterey, State of California

Resolution No. 15-247

- a. Adopt the final Mitigated Negative Declaration for the Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project, Project No. 8868, Federal Aid Project No. HP – 5944 (083, 084, 085, 089, 108, and 112) (SCH No. 2014121057);
b. Adopt a Mitigation and Monitoring Reporting Program;
c. Authorize the Resource Management Agency (RMA) – Director of Public Works or designee to proceed with the Monterey Bay Sanctuary Scenic Trail - Moss Landing Bicycle/Pedestrian Path and Bridge Project; and
d. Authorize the RMA - Director of Public Works or designee to submit an application for a Coastal Development Permit to the Californian Coastal Commission..... [(APN 133-181-009, 133-173-005, 133-173-002)]

The Final Mitigated Negative Declaration for the Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project came before the Monterey County Board of Supervisors on September 15, 2015. Having considered all the written and documentary evidence, the administrative record, the staff report, oral testimony, and other evidence presented, the Board of Supervisors finds and decides as follows:

- 1. FINDING: PROJECT DESCRIPTION - The proposed project is an approximately 4,466 foot (0.85 mile) bicycle/pedestrian path extending from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road. It includes the construction of a ten (10) foot paved bicycle/pedestrian path with two (2) foot wide decomposed granite shoulders on either side of the path and a 12 foot 386 foot long bicycle/pedestrian bridge over the Elkhorn Slough.
2. FINDING: CEQA (MND) - The County of Monterey has completed the Mitigated Negative Declaration (MND) in compliance with the California Environmental Quality Act (CEQA), and the Final MND reflects the County of Monterey’s independent judgment and analysis.

- EVIDENCE: a) CEQA requires preparation of a mitigated negative declaration if there is no substantial evidence in light of the whole record that the project as revised may have a significant effect on the environment.
b) In September 2008, in accordance with Section 15063 of CEQA Guidelines, the County, acting as the lead agency, as well as the applicant or project proponent, prepared an Initial Study evaluating the

- proposed project. The Initial Study is on file with the Resource Management Agency – Public Works (RMA-PW).
- c) The Initial Study (IS) identified potentially significant effects but revisions to the project have been made or agreed to by the project proponent which mitigate the effects to a point where clearly no significant effects would occur to aesthetics, biological resources, greenhouse gas emissions, cultural resources, hazards and hazardous materials, air quality, biological resources, geology and soils, hydrology and water quality, noise, recreation, and mandatory findings of significance. Therefore, an MND was prepared (State Clearing House No. 2014121057).
 - d) The *IS/Draft Mitigated Negative Declaration SCH#:2014121057* was prepared in accordance with CEQA and circulated for public review from December 22, 2014 through February 4, 2014, and re-circulated from March 6, 2015 through April 6, 2015.
 - e) SUMMARY OF IMPACTS
Issues that were analyzed in the MND include aesthetics, biological resources, greenhouse gas emissions, cultural resources, hazards and hazardous materials, air quality, biological resources, geology and soils, hydrology and water quality, noise, recreation, and mandatory findings of significance.
 - f) The County prepared the *Final Mitigated Negative Declaration* (State Clearinghouse # 2014121057) (“Final MND”). The Final MND responds to comments on the Draft MND. The Final MND was released to the public on or about September 15, 2015 and responds to all significant environmental issues raised by persons and organizations that commented on the Draft MND. The County has considered the Draft MND, and the information contained in the Final MND including responses to the comments received. The Final MND together with the Draft MND constitutes the complete Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project MND.
 - g) All project changes required to avoid significant effects on the environment have been incorporated into the project. A Condition Compliance & Mitigation Monitoring and/or Reporting Program (MMRP) has been prepared in accordance with CEQA and the County’s Conditions of Approval and Mitigation Monitoring and Reporting Program. The County is adopting the MMRP as part of project approval (See Exhibit A incorporated by this reference). The MMRP includes mitigation measures and conditions of approval and obligates the County to implement the mitigation measures and monitor that implementation.
 - h) Evidence that has been received and considered includes technical studies and reports, and staff reports that reflect the County’s independent judgment. These documents are on file with the Resource Management Agency (RMA) – Public Works (RMA-PW) and the Clerk of the Board of Supervisors, and are incorporated herein by this reference.
 - i) Staff analysis contained in the MND and the record as a whole indicate the project could result in changes to the resources listed in Section 753.5(d) of

the Department of Fish and Wildlife (DFW) regulations. All land development projects that are subject to environmental review are subject to a State filing fee plus the County recording fee, unless the Department of Fish and Wildlife determines that the project will have no effect on fish and wildlife resources. There are biological resources that are present on the site. For purposes of the Fish and Game Code, the project impact on the fish and wildlife will be reduced to less than significant through implementation of mitigation measures. Therefore, the County will be required to pay the State fee plus a fee payable to the Monterey County Clerk/Recorder for processing said fee and posting the Notice of Determination (NOD).

- j) Monterey County RMA-PW, located at 168 W. Alisal, 2nd Floor, Salinas, California, 93901, is the custodian of documents and other materials that constitute the record of proceedings upon which the decision to adopt the MND is based. The Final MND is also on file in the office of the Clerk of the Board of Supervisors.

3. FINDING: MND-ENVIRONMENTAL IMPACTS MITIGATED TO LESS THAN SIGNIFICANT - The Board of Supervisors finds that potentially significant environmental impacts of the project will be mitigated to a less than significant level due to incorporation of mitigation measures from the MND as conditions of project approval. Changes or alterations have been required in, or incorporated into, the project which avoid or substantially lessen the significant environmental effects as identified in the final MND.

- EVIDENCE:**
- a) Potentially significant impacts on biological resources have been mitigated to a less than significant level with mitigation incorporated through the following mitigation measures; require avoidance measures and/or pre-construction surveys to ensure development activities will not disrupt nesting activities, retaining a qualified biologist for construction monitoring, develop a Marine Mammal Protection Plan approved by authorized agencies prior to construction, limit construction activities to daylight hours (8:00 a.m. to 5:00 pm), acoustic monitoring of pile driving to ensure that sound levels from pile driving are limited to approved values, fish monitoring during pile driving activities to minimize fish exposure to adverse level of underwater sound, and obtain Incidental Harassment Authorization (IHA) prior to initiation of in-water construction.
 - b) Potentially significant impacts on archaeological resources have been mitigated to a less than significant level through mitigation measures which require implementation of the County's standard requirements for accidental discovery of cultural, archaeological, historical or paleontological resources and implementation of the County's requirements for accidental discovery of human remains.
 - c) Potentially significant impacts on hazards and hazardous material have been mitigated to a less than significant level through mitigation

measures which require soil/fill contaminant testing and aerially deposited lead testing prior to construction.

- d) Potentially significant impacts on hydrology and water quality have been mitigated to a less than significant level through mitigation measures which require State Water Resources Control Board Construction General Permits to reduce and eliminate pollutants associated with the construction site, implement source control, site design and best management practices to target pollutants of concern during project construction, and preparation of an erosion control plan consistent with the Monterey County Code.
- e) Potentially significant impacts on noise have been mitigated to a less than significant level through mitigation measures which require restrictions in the project plans and specifications to mitigate construction noise.

4. FINDING: MND-NO SIGNIFICANT UNAVOIDABLE ENVIRONMENTAL IMPACTS – The MND identified no significant unavoidable impacts that would result from the project.

EVIDENCE: a) The MND for Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project did not identify any significant unavoidable impacts that would result from the project.

5. FINDING: RECIRCULATION OF THE DRAFT MND

EVIDENCE: a) The *IS/Draft Mitigated Negative Declaration SCH#:2014121057* was prepared in accordance with CEQA and circulated for public review from December 22, 2014 through February 4, 2014, and re-circulated from March 6, 2015 through April 6, 2015. The re-circulated draft IS/MND provided an additional 30 day public comment review period in order to clarify a few details about the project and the project site. The clarifications provided in the Re-Circulated Draft IS/MND did not result in any new, unavoidable, significant effects or require any new or revised mitigation measures.

6. FINDING: PROJECT APPROVAL – The project is funded by a \$3.8 Million Federal Grant and \$276,000 local fund. Staff is seeking an additional Federal Grant Fund for the unfunded amount of \$6.1 Million. The Monterey County Planning Commission considered the project as part of the Capital Improvement Program (CIP) and found it to be consistent with the Monterey County General Plan. The project lies within the jurisdictional boundaries of both the County and the California Coastal Commission and thus will require a Coastal Development Permit. The California Coastal Commission will consider the County's application for the Coastal Development Permit and take jurisdiction over the entire permit. Board of Supervisors adoption of the MMRP obligates the County to perform the mitigation measures and to monitor and document compliance with the mitigation measures.

NOW, THEREFORE, BE IT RESOLVED THAT, the Board of Supervisors of the County of Monterey does hereby:

- a. Adopt the final Mitigated Negative Declaration for the Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle/Pedestrian Path and Bridge Project, Project No. 8868, Federal Aid Project No. HP – 5944 (083, 084, 085, 089, 108, and 112) (SCH No. 2014121057);
- b. Adopt a Mitigation and Monitoring Reporting Program;
- c. Authorize the Resource Management Agency (RMA) – Director of Public Works or designee to proceed with the Monterey Bay Sanctuary Scenic Trail - Moss Landing Bicycle/Pedestrian Path and Bridge Project; and
- d. Authorize the RMA - Director of Public Works or designee to submit an application for a Coastal Development Permit to the Californian Coastal Commission.
[(APN 133-181-009, 133-173-005, 133-173-002)]


PASSED AND ADOPTED upon motion of Supervisor Potter, seconded by Supervisor Armenta carried this 15th day of September 2015, by the following vote, to wit:

AYES: Supervisors Armenta, Phillips, Salinas, Parker and Potter
NOES: None
ABSENT: None

I, Gail T. Borkowski, Clerk of the Board of Supervisors of the County of Monterey, State of California, hereby certify that the foregoing is a true copy of an original order of said Board of Supervisors duly made and entered in the minutes thereof of Minute Book 78 for the meeting on September 15, 2015.

Dated: October 5, 2015
File Number: RES 15-088

Gail T. Borkowski, Clerk of the Board of Supervisors
County of Monterey, State of California

By  _____
Deputy

CATEGORICAL EXEMPTION/CATEGORICAL EXCLUSION DETERMINATION FORM

05-MON-0-CR	05-1300-0040	DEM10L-5944 (112)
Dist.-Co.-Rte. (or Local Agency) P.M./P.M.	E.A/Project No.	Federal-Aid Project No. (Local Project)/Project No.

PROJECT DESCRIPTION: (Briefly describe project including need, purpose, location, limits, right-of-way requirements, and activities involved in this box. Use Continuation Sheet, if necessary.)

Monterey County, with assistance from the California Department of Transportation and the Federal Highway Administration, proposes to construct a bicycle and pedestrian trail near the community of Moss Landing.

The proposed project is a segment of the Monterey Bay Sanctuary Scenic Trail (MBSST) and will be approximately 12-feet wide, and extend 0.80-mile. In addition, the trail includes the construction of a new bridge across Elkhorn Slough and several low retaining walls. This trail will run parallel to the western edge of State Route 1 from Moss Landing Road, across Elkhorn Slough, and connect to an existing Class I trail at the Harbor Facilities of the Moss Landing Harbor District. Easements are required from the State of California (Caltrans), and from adjacent property owners in order to construct the trail.

(project description continued on next page)

CEQA COMPLIANCE (for State Projects only)

Based on an examination of this proposal and supporting information, the following statements are true and exceptions do not apply (See 14 CCR 15300 et seq.):

- If this project falls within exempt class 3, 4, 5, 6 or 11, it does not impact an environmental resource of hazardous or critical concern where designated, precisely mapped and officially adopted pursuant to law.
- There will not be a significant cumulative effect by this project and successive projects of the same type in the same place, over time.
- There is not a reasonable possibility that the project will have a significant effect on the environment due to unusual circumstances.
- This project does not damage a scenic resource within an officially designated state scenic highway.
- This project is not located on a site included on any list compiled pursuant to Govt. Code § 65962.5 ("Cortese List").
- This project does not cause a substantial adverse change in the significance of a historical resource.

CALTRANS CEQA DETERMINATION (Check one)

Exempt by Statute. (PRC 21080[b]; 14 CCR 15260 et seq.)

Based on an examination of this proposal, supporting information, and the above statements, the project is:

Categorically Exempt. Class . (PRC 21084; 14 CCR 15300 et seq.)

Categorically Exempt. General Rule exemption. [This project does not fall within an exempt class, but it can be seen with certainty that there is no possibility that the activity may have a significant effect on the environment (CCR 15061[b][3].)]

Print Name: Environmental Branch Chief	Print Name: Project Manager/DLA Engineer
Signature _____	Signature _____
Date _____	Date _____

NEPA COMPLIANCE

In accordance with 23 CFR 771.117, and based on an examination of this proposal and supporting information, the State has determined that this project:

- does not individually or cumulatively have a significant impact on the environment as defined by NEPA and is excluded from the requirements to prepare an Environmental Assessment (EA) or Environmental Impact Statement (EIS), and
- has considered unusual circumstances pursuant to 23 CFR 771.117(b).

CALTRANS NEPA DETERMINATION (Check one)

23 USC 326: The State has determined that this project has no significant impacts on the environment as defined by NEPA, and that there are no unusual circumstances as described in 23 CFR 771.117(b). As such, the project is categorically excluded from the requirements to prepare an environmental assessment or environmental impact statement under the National Environmental Policy Act. The State has been assigned, and hereby certifies that it has carried out the responsibility to make this determination pursuant to Chapter 3 of Title 23, United States Code, Section 326 and a Memorandum of Understanding dated June 07, 2013, executed between the FHWA and the State. The State has determined that the project is a Categorical Exclusion under:

- 23 CFR 771.117(c): activity (c)(3)**
- 23 CFR 771.117(d): activity (d)()**
- Activity ___ listed in Appendix A of the MOU between FHWA and the State**

23 USC 327: Based on an examination of this proposal and supporting information, the State has determined that the project is a CE under 23 USC 327.

Randy LaVack	Garin Schneider
Print Name: Environmental Branch Chief	Print Name: Project Manager/DLA Engineer
	
Signature _____	Signature _____
Date <u>1-16-15</u>	Date <u>1-16-15</u>

Date of Categorical Exclusion Checklist completion: _____ Date of ECR or equivalent : _____

Briefly list environmental commitments on continuation sheet. Reference additional information, as appropriate (e.g., CE checklist, additional studies and design conditions).



Application Part 6: Project Funding

(1,000s)

Project Phase	Total Project Costs	Total ATP Funding	ATP Allocation Year *	Total Non-ATP Funding **	Non-Participating Funding	"Prior" ATP Funding	Leveraging Funding	Matching Funding *** (for federal \$)	Future Local Identified Funding
PA&ED	795	-		795	-	-	795	-	-
PS&E	1,952	-		1,952	-	-	1,952	-	-
R/W	305	-		305	-	-	305	-	-
CON	10,375	7,587	19/20	2,788	-	-	2,788	-	-
NI-CON	-	-		-	-	-	-	-	-
TOTAL	13,427	7,587		5,840	-	-	5,840	-	-

* The CTC Allocation-Year is calculated based on the information entered into the "Project Schedule" section.

** Applicants must ensure that the "Total Non-ATP Funding" values show in this table match the overall Non-ATP Funding values they enter into Page 2 of the PPR (later in this form)

*** For programming purposes, applicants, are asked to identify the portion of the Leveraging Funding that meets the requirements to be used as match for new Federal ATP funding.

ATP FUNDING TYPE REQUESTED:

Per the CTC Guidelines, all ATP projects must be eligible to receive federal funding. Most ATP projects will receive federal funding; however, it is the intent of the Commission to consolidate the allocation of federal funds to as few projects as practicable. Therefore, the smallest projects may be granted State Funding from the State Highway Account (SHA) for all or part of the project. Agencies with projects under \$1M, especially ones being implemented by agencies who are not familiar with the federal funding process, are encouraged to request State funding.

Do you believe your project warrants receiving state-only funding? Yes No

ATP PROJECT PROGRAMMING REQUEST (PPR):

Using the Project Schedule, Project Funding, and General Project information provided, this electronic form has automatically prepared the following PPR pages. Applicants must review the information in the PPR to confirm it matches their expectations.



Application Part 7: Application Questions

Screening Criteria

The following Screening Criteria are requirements for applications to be considered for ATP funding. Failure to demonstrate a project meets these criteria will result in the disqualification of the application.

1. Demonstrated fiscal needs of the applicant:

- **Is all or part of the project currently (or has it ever been) formally programmed in an RTPA, MPO and/or Caltrans funding program?** Yes No

If "Yes", explain why the project is not considered "fully funded". (Max of 200 Words)

Words Remaining: **113**

The current cost estimate for this Project is \$13.4 million. The County has secured \$4.023 million State/Federal grant funds, and \$.277 thousand in local funds, \$1.5 million is currently pending the results of a Coastal Conservancy grant application. The County is requesting \$7.6 million in ATP grant funds to fully fund the Project. The proposed Project does not contain any elements that are directly or indirectly related to past or future environmental mitigation resulting from a separate development or capital improvement Project.

- **Are any elements of the proposed project directly or indirectly related to the intended improvements of a past or future development or capital improvement project?** Yes No
- **Are adjacent properties undeveloped or under-developed where standard "conditions of development" could be placed on future adjacent redevelopment to construct the proposed project improvements?** Yes No

2. Consistency with an adopted regional transportation plan:

- **Is the project consistent with the relevant adopted regional transportation plan that has been developed and updated pursuant to Government Code Section 65080?** Yes No

If "Yes", the applicant must provide that portion of Regional Transportation Plan showing that the proposed project is consistent. Attach a copy of ONLY the following elements of the plan: cover page and pages linking the proposed project to the plan. Highlighted and/or mark the attachment to clearly identify the connection.

2014-Monterey-County-RTP_I_ScreeningCrit.pdf

Note: Projects not providing proof will be disqualified and not be evaluated.

2014

ROADWAYS



MONTEREY COUNTY REGIONAL TRANSPORTATION PLAN

MASS TRANSIT



CYCLING

COMPLETE STREETS



Active Transportation: Bicycle and Pedestrian Investments

Bike and pedestrian facilities are integral components of Monterey County's multi-modal transportation system. The region's mild climate and relatively flat topology make biking and walking a viable mode of travel for many living in the county. The close proximity between housing and jobs in the older communities of Monterey County also support the use of bicycles and walking as a transportation alternative, although key gaps in the network currently exist.



The Regional Transportation Plan includes policies for maximizing the transportation system to promote walking and bicycle travel, including development of bicycle and pedestrian facilities, safety programs and promotional events, improved access and safety provisions, and improved linkages to bikeways and recreational trail system.

Bicycle Facilities

Monterey County's regional bikeways system, and the Agency's regional bicycle planning activities are described in more detail below.

Bikeways and Planning in Monterey County – Existing Conditions

Monterey County has approximately 246 miles of maintained bikeways on state, county and local roads. Bikeways in the county are classified as Class I, II, and III. These classifications generally follow design standards established by Caltrans. Classifications are described as follows:

- **Class I: Bicycle Paths** are bikeways separated from vehicle traffic.
- **Class II: Bicycle Lanes** provide cyclists a marked area of the roadway that is part of the roadway also used by motor vehicles. Bicycle lanes have identification signage, pavement stencils, striping, and minimum width requirements.
- **Class III: Bicycle Routes** are recommended roadways that bicycles share with motor vehicles without a marked bike lane. Bicycle Route signs are placed periodically along the route and at changes of direction.

The majority of bikeways in Monterey County are Class II and III, however a large Class I facility exists along the Monterey Peninsula coastline. The Monterey Bay Coastal Trail extends from Lovers Point in Pacific Grove to Del Monte Boulevard, north of Marina, providing a scenic and highly traveled recreational opportunity as well as an important bicycle and pedestrian commuter link in the Monterey peninsula. It is anticipated that the Monterey Bay Coastal Trail

Americans with Disabilities Act

The Americans with Disabilities Act (ADA), passed in 1990, is a comprehensive law prohibiting discrimination against people with disabilities. ADA requires access to public transportation systems for people with disabilities equal to the service available to the able-bodied. Problems commonly associated with sidewalks and pathways for the disabled are driveway cuts, lack of curb cuts, sign posts, benches, and rough and severely cracked sidewalk surfaces.

Future Needs: The Monterey County Bicycle & Pedestrian Facilities Master Plan

The Transportation Agency has worked closely with its Bicycle and Pedestrian Facilities Advisory Committee and 13 member jurisdictions to identify gaps in the countywide road and highway network where bicycle and pedestrian improvements are needed. In 2011, the Agency adopted the *Monterey County Bicycle & Pedestrian Facilities Master Plan* to provide a basis for the allocation of state and federal funds for bicycle and pedestrian projects.

The Plan serves to accomplish two main purposes. First, the plan lists all existing and proposed projects and facilities of jurisdictions within Monterey County and satisfies the General Bikeways Plan requirements set by the California Department of Transportation (California Streets and Highways Code Section 891.2). Local jurisdictions may choose to adopt the plan or submit an equally qualified plan to ensure eligibility for state and federal bicycle-funding sources.

Second, the plan establishes a countywide list of projects. This list assists the Agency in the allocation of various funds for regional bicycle and pedestrian projects. The plan identifies over 500 bicycle and pedestrian projects to accommodate non-motorized travel, which are reflected in the active transportation costs included in the Regional Transportation Plan. The plan also assigns rankings to projects in the plan to serve as a guide for funding and implementation. The top ranked projects identified in the Master Plan are identified in **Table 4-1** below.

Table 4-1: Bicycle & Pedestrian Facilities Master Plan – Top Ranked Projects

Top Ranked Bikeways				
Rank	Title	Description	Cost	Jurisdiction
1	Imjin Parkway Bike Lanes	Stripe bike lanes on Imjin Parkway in addition to Class I bike path	\$2,200,000	Marina
2	Canyon del Rey Blvd	Stripe Class II Bike lanes on east side of Canyon Del Rey Blvd and fillgaps on Westside; Stripe/Restripe bike lanes to the left of right-turn lanes.	\$32,500	Del Rey Oaks
3	Castroville Bicycle Path and Railroad Crossing	Install a Class I bike/ped path and bridge over railroad crossing	\$5,995,000	County
4	Blanco Rd	Install Class II Bikeway from Research Rd to Luther Way	\$221,880	County
5	Davis Rd	Install Class II Bikeway from Blanco Rd to Rossi St	\$3,411,000	County
6	Blanco Rd	Install Class II Bikeway from Luther Way to Abbott St	\$107,300	County
7	Broadway	Install Class II Bikeway from Del Monte Blvd to Mescal St	\$67,900	Seaside

Rank	Title	Description	Cost	Jurisdiction
8	Hwy 68 Segment	Install Class II Bikeway from Joselyn Canyon Rd to San Benancio Rd	\$351,300	Caltrans
9	Sanctuary Scenic Trail Seg. 15	Construct Class I Bikeway from Moss Landing Rd to Hwy 1 & new Elkhorn Slough Bridge	\$5,082,000	County
10	San Juan Grade Rd	Install Class II Bikeway from Russell Rd to Boronda Rd	\$39,200	Salinas
10	San Juan Grade Rd	Install Class II Bikeway from Herbert Rd to Rogge Rd	\$88,300	County
10	San Juan Grade Rd	Install Class III Bike route from Russell Rd to Rogge Rd	\$1,200	County
11	Gabilan Creek	Install Class I Bikeway from Danbury St to Constitution Blvd	\$569,300	Salinas
Priority Pedestrian Improvements				
	Title	Description	Cost	Jurisdiction
	Castroville Bicycle Path and Railroad Crossing	Install a Class I bike/ped path and bridge over railroad crossing	\$5,995,000	County
	Sanctuary Scenic Trail Segment 15	Construct Class I Bikeway from Moss Landing Rd to Hwy 1 & new Elkhorn Slough Bridge	\$5,082,000	County
	Gabilan Creek	Install Class I Bikeway from Danbury St to Constitution Blvd	\$569,300	Salinas
	Hatton Canyon Path	Install Class I path from Carmel Valley Road to Hwy 1	\$1,689,600	County
	Sanctuary Scenic Trail Segment 4B	Install Class I path from Tioga Ave to the Coastal Trail	\$292,600	Sand City

Detailed information on planned bicycle and pedestrian facilities in Monterey County can be found in the *Bicycle & Pedestrian Facilities Master Plan*.

Monterey Bay Sanctuary Scenic Trail

One of the most important planned regional bicycle facilities in Monterey County is the Monterey Bay Sanctuary Scenic Trail. The Monterey Bay Sanctuary Scenic Trail is a collaborative effort among public agencies, non-profit organizations and the public to construct a trail that would span Monterey Bay from the city of Pacific Grove to Santa Cruz County line. The primary purpose of the Trail is to enhance appreciation and protection of the Monterey Bay National Marine Sanctuary as well as provide a safe, accessible scenic trail for pedestrians, bicyclists, and other users free of automobile traffic. The Sanctuary Scenic Trail originally was a project of the Santa Cruz County Inter-Agency Task Force, a Santa Cruz Committee that formed in 1993.

The Transportation Agency completed the Monterey County portion of the Sanctuary Scenic Trail Master Plan in 2008. That plan lists the various components needed to complete the trail. There are 17 planned trail segments with a total length of 33 miles in Monterey County. With





Part B: Narrative Questions

Detailed Instructions for Question #1

QUESTION #1

DISADVANTAGED COMMUNITIES (0-10 POINTS)

This project does not qualify as a Disadvantaged Community.

A. Map of Project Boundaries, Access and Destination (0 points): Required

Provide a scaled map showing the boundaries of the proposed project/program/plan, the geographic boundaries of the disadvantaged community, and disadvantaged community access point(s) and destinations that the project/program/plan is benefiting.

MBSST Destinations Map w DAC.pdf

B. Identification of Disadvantaged Community: (0 points)

Select one of the following 4 options. Must provide information for all Census Tract/Block Group/Place # that the project affects.

- **Median Household Income**
- **CalEnviroScreen**
- **Free or Reduced Priced School Meals** - Applications using this measure must demonstrate how the project benefits the school students in the project area.
- **Other**

Select Option: Median Household Income

The Median Household Income (Table ID B19013) is less than 80% of the statewide median based on the most current Census Tract (ID 140) level data from the 2010-2014 American Community Survey (ACS) (<\$49,191). Communities with a population less than 15,000 may use data at the Census Block Group (ID 150) level. Unincorporated communities may use data at the Census Place (ID 160) level. Data is available at: <http://factfinder.census.gov/faces/nav/jsf/pages/index.xhtml>

Census Tract/Block Group/Place #	Population	MHI
Census Tract 146.01/Block Group 2/Place #160	204	30,500

Lowest median household income from above (autofill): \$ 30,500 (to be used for qualifying as benefiting a DAC only)

Median household income by census tract for the community(ies) benefited by the project: \$ 30,500
(to be used for severity calculation only)

Must attach a copy of FactFinder ACS page for each census tract listed above. Attach all pages as one pdf.

FactFinder ACS MossLanding.pdf

C. Direct Benefit: (0 - 4 points)

1. Explain how the project/program/plan closes a gap, provides connections to, or addresses a deficiency in an active transportation network or meets an important community need. (Max of 50 Words)

Words Remaining:

The only path for bicycle and pedestrian traffic in this area is along the narrow shoulders of SR-1. With a high annual average daily traffic (AADT) volume of 35,000+ traveling at speeds over 55 miles per hour, SR-1 creates a dangerous situation for any pedestrian or bicyclists.

2. Explain how the disadvantaged community residents will have physical access to the project/program/plan. (Max of 50 Words)

Words Remaining:

The project will provide safe access between north and south Moss Landing by offering a paved bicycle path and bicycle/pedestrian bridge over the Elkhorn Slough. The Project is fully located within the DAC providing direct physical access.

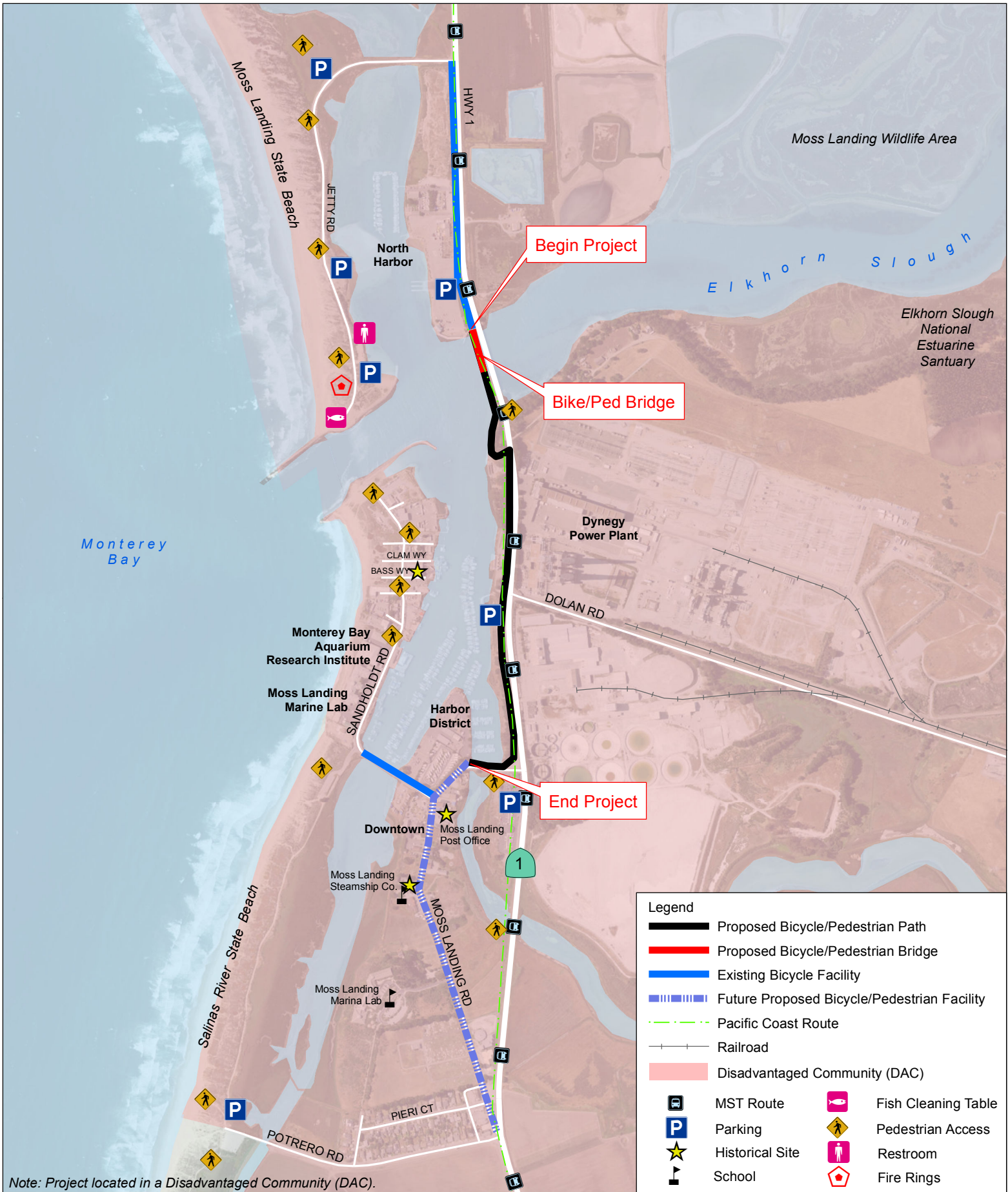
3. Illustrate how the project was requested or supported by the disadvantaged community residents. (Max of 50 Words)

Words Remaining:

Public participation was received at two separate community meetings. Input was received on overall trail construction, schedule, aesthetics, interpretive/educational signage, and site management. The community had positive feedback expressing that the improvements would address their safety concerns of currently walking/biking on the shoulders of State Highway 1.

D. Project Location: (0 - 2 points)

1. Is your project located within a disadvantaged community? Fully



Note: Project located in a Disadvantaged Community (DAC).

TIGERweb
Geography Division
CT 101.01

Layers Legend Task Results

Search: moss landing, ca

Select Vintage: Current

- Labels
- Transportation (Roads and Railroads)
- PUMAs, UGAs, and ZCTAs
- Tribal Census Tracts and Block Groups
- Census Tracts and Blocks
- Census Tracts
- Census Block Groups
- 2010 Census Blocks
- Military and Other Special Land Use Areas
- School Districts
- Places and County Subdivisions
- American Indian, Alaska Native, and Native Hawaiian Areas
- Legislative Areas
- Census Regions and Divisions
- Urban Areas
- Metropolitan and Micropolitan Statistical Areas and Related Statistical Areas
- Hydrography
- States and Counties

CT 146.01 MONTEREY CALIFORNIA

CT 9900

CT 103.06

Zoom: 15
1:18,056
0.4km
0.2mi

Current Extent (degs):
[-121.7956, 36.8012] [-121.7506, 36.8317]

Geocoding Complete.

TIGERweb
Geography Division

Search: moss landing, ca

Select Vintage: Current

- Labels
- Transportation (Roads and Railroads)
- PUMAs, UGAs, and ZCTAs
- Tribal Census Tracts and Block Groups
- Census Tracts and Blocks
- Census Tracts
- Census Block Groups
- 2010 Census Blocks
- Military and Other Special Land Use Areas
- School Districts
- Places and County Subdivisions
- American Indian, Alaska Native, and Native Hawaiian Areas
- Legislative Areas
- Census Regions and Divisions
- Urban Areas
- Metropolitan and Micropolitan Statistical Areas and Related Statistical Areas
- Hydrography
- States and Counties

MONTEREY CALIFORNIA

BG 0 BG 1 BG 2

Zoom: 15
1:18,056
0.4km
0.2mi

Current Extent (degs):
[-121.7956, 36.8012] [-121.7506, 36.8317]

Geocoding Complete.



Community Facts - Find popular facts (population, income, etc.) and frequently requested data about your community.

Enter a state, county, city, town, or zip code: [GO](#)

- [Population](#)
- [Age](#)
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- [Race and Hispanic Origin](#)
- [Veterans](#)
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Moss Landing CDP, California

Population [Bookmark/Save](#) | [Print](#)

Census 2010 Total Population ▼

204

Source: 2010 Demographic Profile

Popular tables for this geography:

2010 Census

- [General Population and Housing Characteristics \(Population, Age, Sex, Race, Households and Housing, ...\)](#)
- [Race and Hispanic or Latino Origin](#)
- [Hispanic or Latino by Type \(Mexican, Puerto Rican, ...\)](#)
- [Households and Families \(Relationships, Children, Household Size, ...\)](#)

2014 American Community Survey

- [Demographic and Housing Estimates \(Age, Sex, Race, Households and Housing, ...\)](#)

2014 Population Estimates Program

- [Annual Population Estimates](#)

Census 2000

- [General Demographic Characteristics \(Population, Age, Sex, Race, Households and Housing, ...\)](#)

• [Want more?](#) Need help? Use [Guided Search](#) or visit [Census.gov's Quick Facts](#).

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S1903

MEDIAN INCOME IN THE PAST 12 MONTHS (IN 2014 INFLATION-ADJUSTED DOLLARS)
 2010-2014 American Community Survey 5-Year Estimates

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the [Data and Documentation](#) section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the [Methodology](#) section.

Versions of this table are available for the following years:
2014
 2013
 2012
 2011
 2010
 2009

Subject	Moss Landing CDP, California			
	Total		Median income (dollars)	
	Estimate	Margin of Error	Estimate	Margin of Error
Households	70	+/-45	30,500	+/-47,873
One race--				
White	62.9%	+/-38.6	85,455	+/-96,040
Black or African American	0.0%	+/-36.7	-	**
American Indian and Alaska Native	0.0%	+/-36.7	-	**
Asian	0.0%	+/-36.7	-	**
Native Hawaiian and Other Pacific Islander	0.0%	+/-36.7	-	**
Some other race	37.1%	+/-38.6	-	**
Two or more races	0.0%	+/-36.7	-	**
Hispanic or Latino origin (of any race)	75.7%	+/-26.0	31,250	+/-107,128
White alone, not Hispanic or Latino	24.3%	+/-26.0	28,750	+/-18,500
HOUSEHOLD INCOME BY AGE OF HOUSEHOLDER				
15 to 24 years	0.0%	+/-36.7	-	**
25 to 44 years	24.3%	+/-27.2	26,964	+/-20,195
45 to 64 years	75.7%	+/-27.2	31,250	+/-111,794
65 years and over	0.0%	+/-36.7	-	**
FAMILIES				
Families	60	+/-43	37,857	+/-74,317
With own children under 18 years	5.0%	+/-11.3	-	**
With no own children under 18 years	95.0%	+/-11.3	38,393	+/-82,516
Married-couple families	25.0%	+/-29.3	32,250	+/-12,928
Female householder, no husband present	75.0%	+/-29.3	85,341	+/-138,190
Male householder, no wife present	0.0%	+/-39.7	-	**
NONFAMILY HOUSEHOLDS				
Nonfamily households	10	+/-14	-	**
Female householder	100.0%	+/-97.3	-	**
Living alone	100.0%	+/-97.3	-	**
Not living alone	0.0%	+/-97.3	-	**
Male householder	0.0%	+/-97.3	-	**
Living alone	0.0%	+/-97.3	-	**
Not living alone	0.0%	+/-97.3	-	**
PERCENT IMPUTED				
Household income in the past 12 months	64.3%	(X)	(X)	(X)
Family income in the past 12 months	75.0%	(X)	(X)	(X)
Nonfamily income in the past 12 months	0.0%	(X)	(X)	(X)

Source: U.S. Census Bureau, 2010-2014 American Community Survey 5-Year Estimates

Explanation of Symbols:

- An "" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
- An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
- An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.
- An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.
- An "" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
- An "" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
- An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
- An "(X)" means that the estimate is not applicable or not available.

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see [Accuracy of the Data](#)). The effect of nonsampling error is not represented in these tables.

While the 2010-2014 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.



Search - Use the options on the left (topics, geographies, ...) to narrow your search results

Your Selections

Search using...

Block (2010)
22 selected

[clear all selections and start a new search](#)

[load search](#) | [save search](#)

Search using the options below:

Topics
(age, income, year, dataset, ...)

Geographies
(states, counties, places, ...)

Race and Ethnic Groups
(race, ancestry, tribe)

Industry Codes
(NAICS industry, ...)

EEO Occupation Codes
(executives, analysts, ...)

Search Results: 1-25 of 249 tables and other products match 'Your Selections'

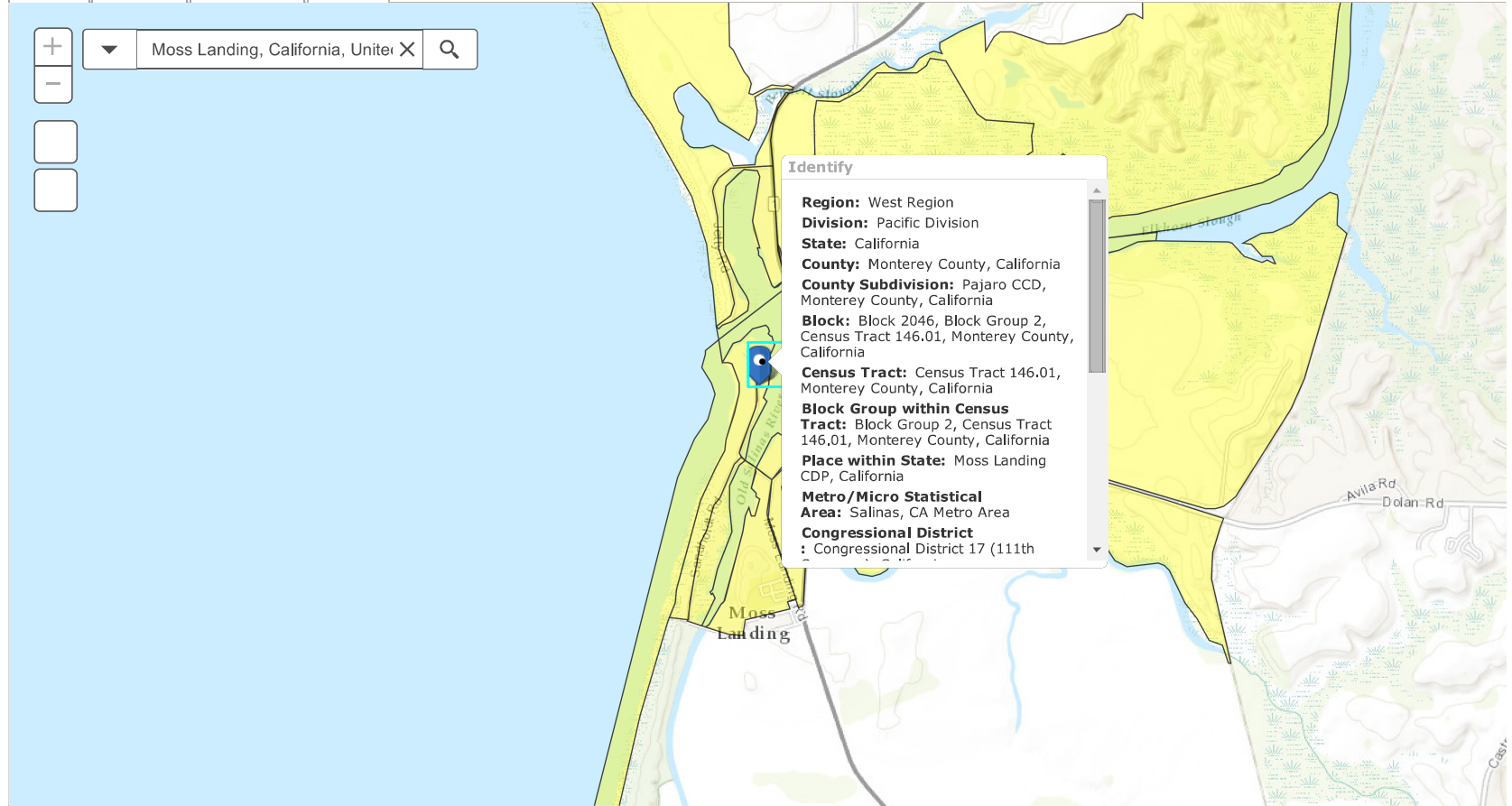
per page: 25

Select Geographies

List | Name | Address | Map

+ Moss Landing, California, United States

-



Selected: View | Download | Compare | Clear All | Reset Sort

1 2 3 4 5



ATP CYCLE 3 APPLICATION FORM

DLA-001 (NEW 4/2016)
v1.2



E. Severity: (0 - 4 points)

- a. Auto calculated



Part B: Narrative Questions

Question #2

QUESTION #2

POTENTIAL FOR INCREASED WALKING AND BICYCLING, ESPECIALLY AMONG STUDENTS, INCLUDING THE IDENTIFICATION OF WALKING AND BICYCLING ROUTES TO AND FROM SCHOOLS, TRANSIT FACILITIES, COMMUNITY CENTERS, EMPLOYMENT CENTERS, AND OTHER DESTINATIONS; AND INCLUDING INCREASING AND IMPROVING CONNECTIVITY AND MOBILITY OF NON-MOTORIZED USERS. (0-35 POINTS)

Please provide the following information: (This must be completed to be considered for funding for infrastructure projects)

# of Users	Pedestrian	Bicycle	Date of Counts	Mark here if N/A to project
Current	18	22	3/3/2016	<input type="checkbox"/>
Projected (1 year after completion)	110	110	11/18/2022	<input type="checkbox"/>

Safe Routes to School projects and programs: The following information related to the Safe Routes to School Projects data was already entered in part 3 of the application.

School	Total Student Enrollment	Approx. # of Students Living Along School Route Proposed	# of Students Currently Walking/Biking to School	Projected # of Students that will walk/bike after project	Net projected Change in Students walking/biking
					0
Total	0	0	0	0	0

Document the methodologies used to establish the **current** count data. (Max of 200 Words)

Words Remaining: 45

Manual counts were taken on Tuesday, March 3, 2016 and Thursday, March 8, 2016 by the County's RMA PW - Traffic Section. The counts were conducted during twelve-hour periods from 6AM to 6PM to capture bicycle and pedestrian activities in the area. Other factors taken into consideration were access to area employment centers, school facilities (university), bus stops, recreational locations, and the community.

The methodology used for projected 1-year after completion was the inputs of a twelve-hour count total for bicyclists and pedestrians from the table above and the Annual Average Daily Traffic (AADT) for Moss Landing Road of 1,800 (Source: Monterey County RMA PW - Traffic Section. To calculate a user rate 1-year after construction it is assumed that 5% of current ADT was grown to the anticipated construction year of 2022 at 2% per year. According to this methodology, the 1-year estimate of daily users will be 110.

A. Describe the specific active transportation need that the proposed project/plan/program will address. (0-15 points) (Max of 500 Words)

Words Remaining: 233

The Project addresses the need to provide safe and adequate walking/bicycling routes, restore connectivity, and improve mobility over the entire disadvantaged community of Moss Landing.

Currently, the only path for bicycle and pedestrian traffic in this area is along the narrow shoulders of SR-1. With a high annual average daily traffic (AADT) volume of over 35,000 traveling at speeds in excess of 55 miles per hour, SR-1 creates a dangerous situation for any pedestrian traveling along the shoulder. The Project will create operational and physical improvements that will reduce hazards from speeding vehicles making a safer environment for pedestrians and bicyclists.

Additionally the Project will promote and improve public health and wellness by providing a safe, convenient, and continuous bicycle/pedestrian facility. This new facility will encourage more people, who would have otherwise driven their cars, to ride bicycles or walk to and from the various destinations and activities spread between north and south Moss Landing.

The project is also a missing link in the Monterey Bay Sanctuary Scenic Trail (MBSST), a larger chain of trail segments connecting Santa Cruz and Monterey counties as part of the California Coastal Trail. The California Coastal Trail is an environmental project adopted by the California Coastal Conservancy, designed to create a continuous interconnected public trail system along the California coastline, which upon completion will span 1,200 miles from Oregon to Mexico.

Lastly, the project is consistent with the goals and objective of the 2014 Regional Transportation Plan (RTP) prepared by the Transportation Agency for Monterey County (TAMC), which is Monterey County's regional transportation agency.

**B. Describe how the proposed project/plan/program will address the active transportation need: (0-20 points)****1. Close a gap?** Yes NoNo. of gaps: 1 Total length of gap(s) (feet): 4,466

Gap closure = Construction of a missing segment of an existing facility in order to make that facility continuous.

- a. Must provide a map of each gap closure identifying gap and connections.

MBSST Destinations Map051916.pdf

- b. Describe how the project links or connects, or encourages use of existing routes to transportation-related and community identified destinations where an increase in active transportation modes can be realized, including but not limited to: schools, school facilities, transit facilities, community, social service or medical centers, employment centers, high density or affordable housing, regional, State or national trail system, recreational and visitor destinations or other community identified destinations. Specific destination must be identified. (Max of 100 Words)

Words Remaining:

The Project will close a gap in the community of Moss Landing between the North Harbor (northwest side of the existing State Highway 1 Bridge) and Moss Landing Road. The Project will connect to existing Class II bicycle lanes along Moss Landing Road, employment centers, transit facilities, schools, two state beaches, and other traveler destinations in the community and eventually connecting to the larger trail network of the Monterey Bay Sanctuary Scenic Trail once all the phases are completed.

2. Creation of new routes? Yes No

New route = Construction of a new facility that did not previously exist for non-motorized users that provides a course or way to get from one place to another.

- a. Must provide a map of the new route location.

MBSST Location Map.pdf

- b. Describe the existing route(s) that currently connect the affected transportation related and community identified destinations and why the route(s) are not adequate. (Max of 100 Words)

Words Remaining:

Currently, the only path for bicycle and pedestrian traffic between south and north Moss Landing is along the narrow shoulders of SR-1. With a high annual average daily traffic (AADT) volume of over 35,000 traveling at speeds in excess of 55 miles per hour, SR-1 creates a dangerous situation for any pedestrian traveling along the shoulder.

- c. Describe how the project links or connects, or encourages use of existing routes to transportation-related and community identified destinations where an increase in active transportation modes can be realized, including but not limited to: schools, school facilities, transit facilities, community, social service or medical centers, employment centers, high density or affordable housing, regional, State or national trail system, recreational and visitor destinations or other community identified destinations. Specific destination must be identified. (Max of 100 Words)

Words Remaining:

Once completed, the Project will connect existing trail facilities that run from Lovers Point in Pacific Grove through the City of Monterey, Sand City, Seaside to Marina and Santa Cruz County. The Project will provide new and enhanced access to scenic views, natural resources, beaches, parks, dunes, wetlands, employment centers, multi-purpose campus and research facilities, and bus stops. Construction of the Moss Landing segment of the MBSST will provide previously unavailable safe pedestrian access between the frequently visited Elkhorn Slough at the north end of Moss Landing and the popular shopping and dining options of south Moss Landing.

3. Removal of barrier to mobility? Yes No

- a. Type of barrier:
- Freeway

- b. Must provide a map identifying the barrier location and improvement.

MBSST Destinations Map051916.pdf

- c. Describe the existing negative effects of barrier to be removed and how the project addresses the existing barrier. (Max of 100 Words)

Words Remaining:

The Project will remove barriers to mobility by constructing a paved bicycle path and pedestrian/bicycle bridge away from SR-1 and over the Elkhorn Slough parallel to the SR-1 Bridge. Currently, the only path for bicycle and pedestrian traffic in this area is along the narrow shoulders of SR-1. With a high AADT volume of over 35,000+ traveling at speeds in excess of 55 miles per hour, SR-1 creates a dangerous situation for any pedestrian traveling along the shoulder. Construction of this trail will provide safe access for pedestrians. See Attachment E.



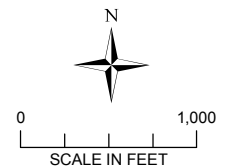
Note: Project located in a Disadvantaged Community (DAC).

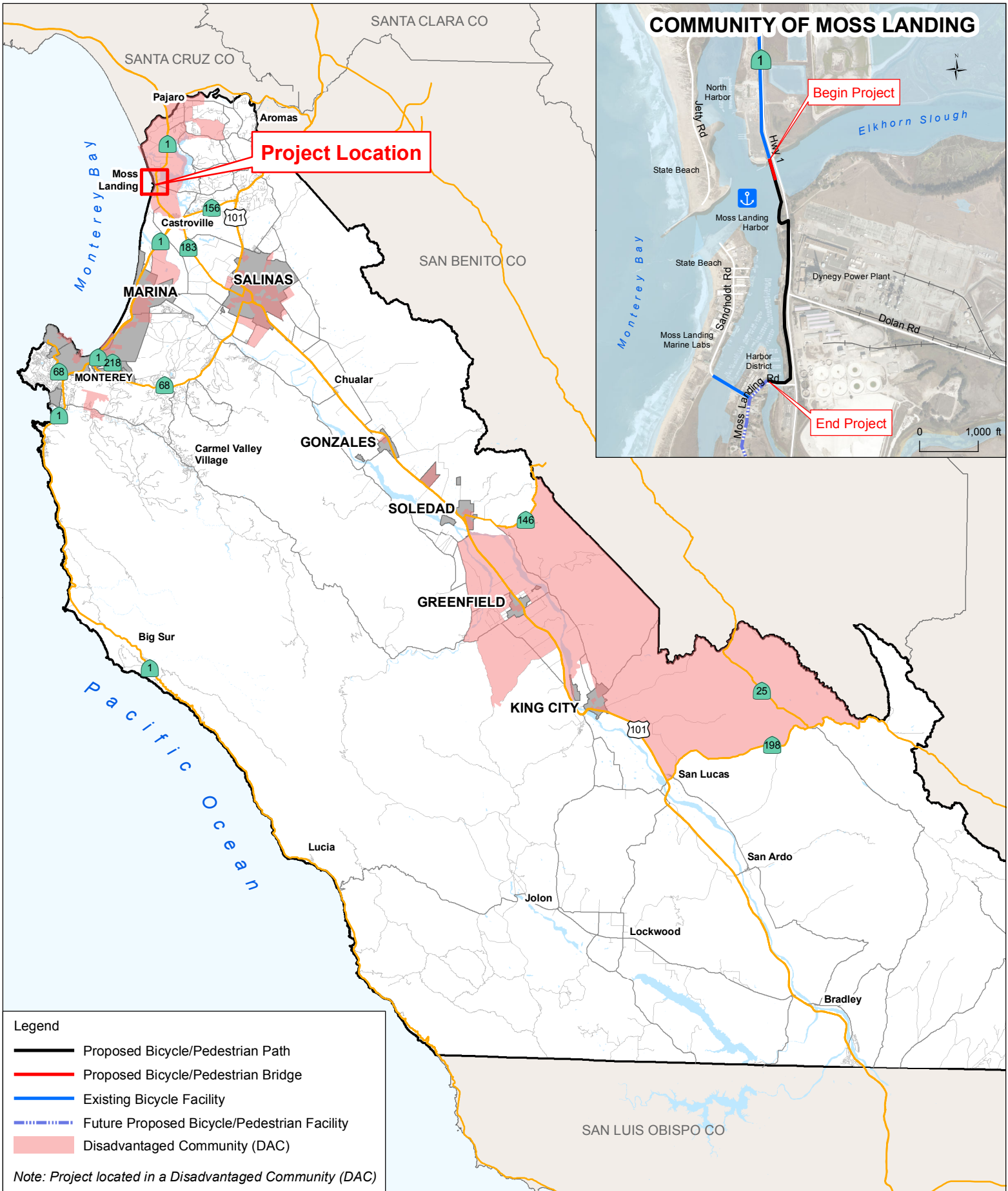


County of Monterey
 Resource Management Agency
 Public Works Department
 April 5, 2016

ATTACHMENT E MBSST DESTINATIONS MAP

MOSS LANDING COMMUNITY, MONTEREY COUNTY, CA





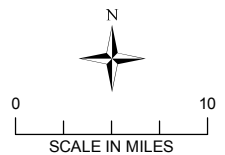
- Legend**
- Proposed Bicycle/Pedestrian Path
 - Proposed Bicycle/Pedestrian Bridge
 - Existing Bicycle Facility
 - Future Proposed Bicycle/Pedestrian Facility
 - Disadvantaged Community (DAC)

Note: Project located in a Disadvantaged Community (DAC)



County of Monterey
 Resource Management Agency
 Public Works Department
 April 5, 2016

ATTACHMENT D MBSST LOCATION MAP MONTEREY COUNTY, CA





- d. Describe how the project links or connects, or encourages use of existing routes to transportation-related and community identified destinations where an increase in active transportation modes can be realized, including but not limited to: schools, school facilities, transit facilities, community, social service or medical centers, employment centers, high density or affordable housing, regional, State or national trail system, recreational and visitor destinations or other community identified destinations. Specific destination must be identified. (Max of 100 Words)

Words Remaining: 2

Once completed, the project will connect existing trail facilities that run from Lovers Point in Pacific Grove through the City of Monterey, Sand City, Seaside to Marina and Santa Cruz County. The Project will provide new and enhanced access to scenic views, natural resources, beaches, parks, dunes, wetlands, employment centers, multi-purpose campuses, research facilities, and bus stops. Construction of the Moss Landing segment of the MBSST will provide previously unavailable safe pedestrian access between the frequently visited Elkhorn Slough at the north end of Moss Landing and the popular shopping and dining options at south Moss Landing.

4. Other improvements to routes?

 Yes No

- a. Must provide a map of the new improvement location.

MBSST Destinations Map051916.pdf

- b. Explain the improvement. (Max of 100 Words)

Words Remaining:

The Project creates operational and physical improvements that will be constructed to Caltrans MUTCD standards. These operational and physical improvements will reduce hazards from speeding vehicles providing a safer environment for pedestrians and bicyclists.

- c. Describe how the project links or connects, or encourages use of existing routes to transportation-related and community identified destinations where an increase in active transportation modes can be realized, including but not limited to: schools, school facilities, transit facilities, community, social service or medical centers, employment centers, high density or affordable housing, regional, State or national trail system, recreational and visitor destinations or other community identified destinations. Specific destination must be identified. (Max of 100 Words)

Words Remaining: 2

Once completed, the project will connect existing trail facilities that run from Lovers Point in Pacific Grove through the City of Monterey, Sand City, Seaside to Marina and Santa Cruz County. The Project will provide new and enhanced access to scenic views, natural resources, beaches, parks, dunes, wetlands, employment centers, multi-purpose campuses, research facilities, and bus stops. Construction of the Moss Landing segment of the MBSST will provide previously unavailable safe pedestrian access between the frequently visited Elkhorn Slough at the north end of Moss Landing and the popular shopping and dining options at south Moss Landing.

5. Plan for increasing biking and walking in the community?

 Yes No

6. Encourages and/or educates with the goal of increasing walking or biking in the community?

 Yes No

- a. Describe how the program encourages walking or biking to transportation-related and community identified destinations where an increase in active transportation modes can be realized, including but not limited to: schools, school facilities, transit facilities, community, social service or medical centers, employment centers, high density or affordable housing, regional, State or national trail system, recreational and visitor destinations or other community identified destinations. (Max of 100 Words)

Words Remaining: 3

The Project will include new interpretive/educational signs along the trail (e.g. cultural and historic significance, wildlife and habitat) and directional signage enhancing delineation between bicyclists and pedestrians. The Project will close a gap in the community of Moss Landing between the North Harbor (northwest side of the existing State Highway 1 Bridge) and Moss Landing Road. The Project will connect to existing Class II bicycle lanes along Moss Landing Road, employment centers, transit facilities, schools, two state beaches, and other traveler destinations in the community and eventually connecting to the larger trail network of MBSST.



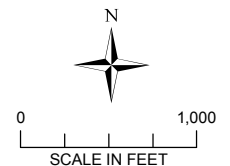
Note: Project located in a Disadvantaged Community (DAC).



County of Monterey
 Resource Management Agency
 Public Works Department
 April 5, 2016

ATTACHMENT E MBSST DESTINATIONS MAP

MOSS LANDING COMMUNITY, MONTEREY COUNTY, CA





Part B: Narrative Questions

Detailed Instructions for Question #3

QUESTION #3

POTENTIAL FOR REDUCING THE NUMBER AND/OR RATE OR THE RISK OF PEDESTRIAN AND BICYCLIST FATALITIES AND INJURIES, INCLUDING THE IDENTIFICATION OF SAFETY HAZARDS FOR PEDESTRIANS AND BICYCLISTS. (0-25 POINTS)

A. Describe the plan/program influence area or project location's history of collisions resulting in fatalities and injuries to non-motorized users and the source(s) of data used (e.g. collision reports, community observation, surveys, audits). (10 points max)

1. The following reported crashes must have all occurred within the project's influence area within the last 5 years (only crashes that the project has a chance to mitigate):

# of Crashes	Pedestrian	Bicycle	Total
Fatalities	0	0	0
Injuries	0	1	1
Total	0	1	1

2. Applicant can provide bicycle and pedestrian (only) crash rates in addition to the information required above. (Max of 200 Words)

Words Remaining: 140

Although crash data does not show more bicycle/pedestrian collisions, many more are likely to happen due to the high traffic volume along State Highway 1 and the relatively close proximity that pedestrians are forced to have due to the lack of a safe trail. This is an unsafe situation for non-motorized users that cannot be continued or promoted.

3. Discuss specific accident data. (Max of 200 Words)

Words Remaining: 79

Crash data from SWITRS for all of Monterey County was used from January 1, 2011 through December 31, 2015. There was a total of only one (1) bicycle collision between January 1, 2013 through December 31, 2013 along the Project corridor (State Highway 1a parallel route). Due to the amount of data collected, only the page showing this collision is attached. Although crash data does not show more bicycle/pedestrian collisions, many more are likely to happen due to the high traffic volume along State Highway 1 and the relatively close proximity that pedestrians are forced to have due to the lack of a safe trail. This is an unsafe situation for non-motorized users that cannot be continued or promoted.

Attach a scaled-map which shows that all documented bicycle and pedestrian collisions/incidents (only) are within the area of influence of the proposed plan, program, or project safety improvements. This data and map should demonstrate how the data illustrates a non-motorized (not vehicular) safety issue.

MossLandingBikeCollisionMap1.pdf

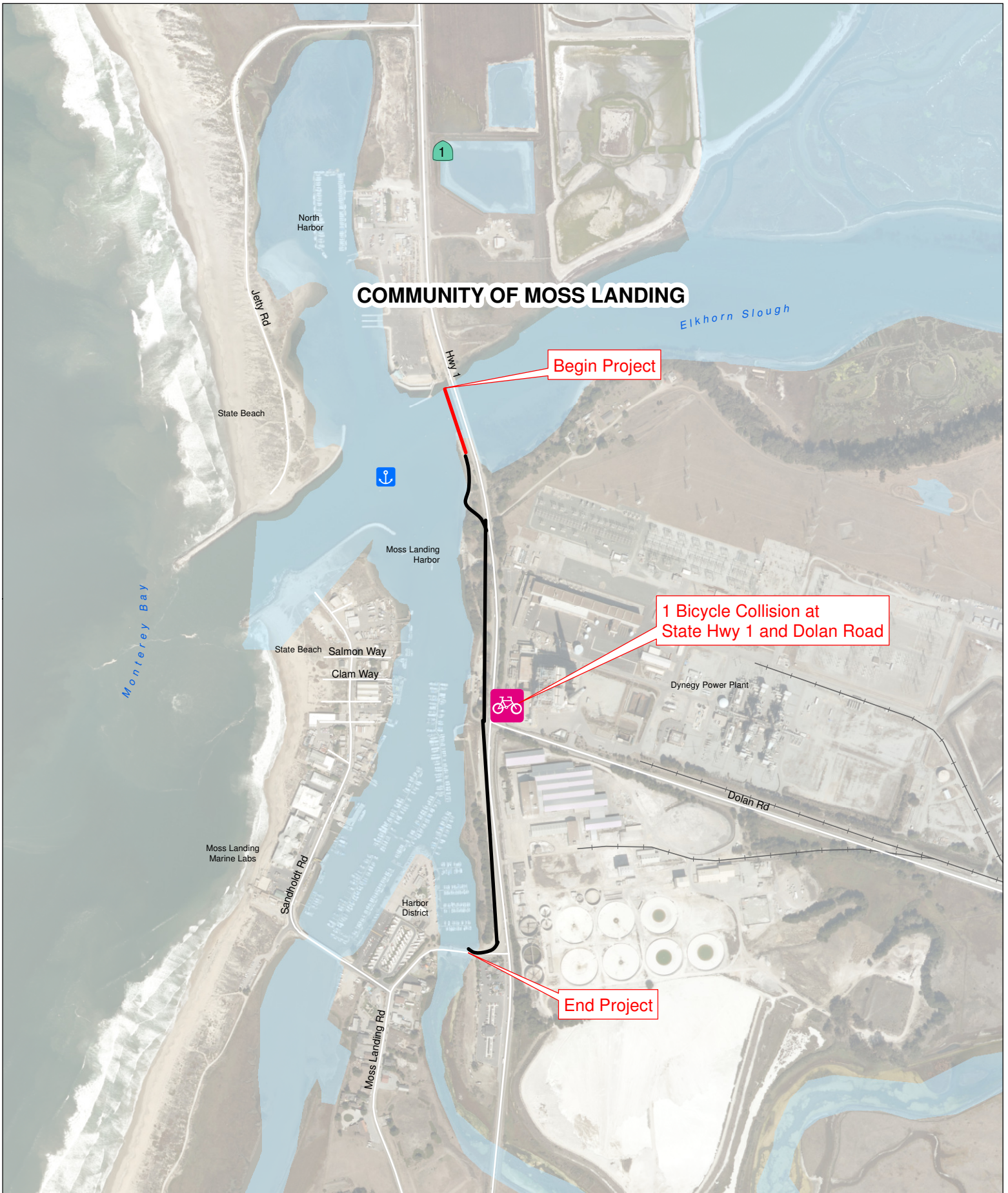
4. Attach a SWITRS or equivalent (i.e. UC Berkeley's TIMS tool) listing of all bicycle and pedestrian crashes (only) shown in the map above and in this application.

Moss Landing-SWITRS report.pdf

*Applications that do not have the crash data above OR that prefer to provide additional crash data and/or safety data in a different format can provide this data below. The corresponding methodology used must also be included. Input Data and methodologies here and/or include them via a separate attachment in the field below. (Max of 200 Words)

Words Remaining: 198

N/A



COMMUNITY OF MOSS LANDING

Begin Project

**1 Bicycle Collision at
State Hwy 1 and Dolan Road**

End Project



County of Monterey
Resource Management Agency
Public Works Department
April 5, 2016

MBSST BICYCLE COLLISION MAP
MONTEREY COUNTY, CA



BIKE

01/01/2013 thru 12/31/2013

Total Count: 164

County: Monterey

Include State Highways cases

Report Run On: 04/21/2016

Primary Rd RIDER AV		Distance (ft) 0	Direction	Secondary Rd MIMBRERA WY	NCIC 2708	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Salinas	County Monterey	Population 6	Rpt Dist	Beat 011	Type 0	CalTrans	Badge 54370	Collision Date 20131011	Time 1823	Day FRI															
Primary Collision Factor WRONG SIDE		Violation 216501	Collision Type	AUTO/PED	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20140430																
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2	Spec Cond 0																		
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DUSK/DAWN	Ped Action	Cntrl Dev	NT PRS/FCTR	Loc Type	Ramp/Int																
Party Info																									
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F BICY	26	M	H	HNBD		PROC ST	-	L	0400	-	-	3	N	-	-	-	BICY	COMP PN	26	M	9	3	-	-	
2 DRVR	24	M	H	HNBD		STOPPED	W	-	-00	GMC	2005	-	3	N	-	M	C	PASS		22	M	2	3	-	-
Party Info																									
Primary Rd RIKER ST		Distance (ft) 0	Direction	Secondary Rd WEST ACACIA ST	NCIC 2708	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Salinas	County Monterey	Population 6	Rpt Dist	Beat 007	Type 0	CalTrans	Badge 37663	Collision Date 20131121	Time 1752	Day THU															
Primary Collision Factor STOP SGN SIG		Violation 22450A	Collision Type	BROADSIDE	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20140527																
Weather1 CLOUDY		Weather2	Rdwy Surface DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2	Spec Cond 0																		
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DARK - ST	Ped Action	Cntrl Dev	FNCTNG	Loc Type	Ramp/Int																
Party Info																									
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F BICY	59	M	W	HBD-NUI		PROC ST	S	L	0400	-	-	3	A	21201	-	-	BICY	OTH VIS	59	M	1	0	P	V	
2 DRVR	57	F	H	HNBD		PROC ST	E	-	-00	MERCE	2002	-	3	N	-	M	G								
Party Info																									
Primary Rd RIO RD		Distance (ft) 0	Direction	Secondary Rd VIA NONA MARIE	NCIC 9730	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City UNINCORP.	County Monterey	Population 9	Rpt Dist	Beat 002	Type 3	CalTrans	Badge 015014	Collision Date 20131107	Time 1525	Day THU															
Primary Collision Factor R-O-W AUTO		Violation 21804A	Collision Type	OTHER	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20140508																
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2	Spec Cond 0																		
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FNCTNG	Loc Type	Ramp/Int																
Party Info																									
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1 DRVR	57	F	W	HNBD		PROC ST	W	A	0100	LEXUS	2012	-	3	N	-	M	G								
2F BICY	22	M	W	HNBD		LFT TURN	N	L	0400	-	-	-	3	N	-	-	BICY	COMP PN	22	M	1	1	P	V	
Party Info																									
Primary Rd ROCHEX AV		Distance (ft) 0	Direction	Secondary Rd N MAIN	NCIC 2708	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Salinas	County Monterey	Population 6	Rpt Dist	Beat 002	Type 0	CalTrans	Badge 59034	Collision Date 20130620	Time 1846	Day THU															
Primary Collision Factor WRONG SIDE		Violation 21650	Collision Type	BROADSIDE	Severity PDO	#Killed 0	#Injured 0	Tow Away? N	Process Date 20140920																
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2	Spec Cond 0																		
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FNCTNG	Loc Type	Ramp/Int																
Party Info																									
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F BICY	52	M	B	HNBD		WRONG WY	N	L	0400	-	-	-	A	21201	K	-									
2 DRVR	31	F	H	HNBD		RGT TURN	E	-	0000	NISSA	2012	-	-	-	-	M	G								
Party Info																									
Primary Rd RT 1		Distance (ft) 0	Direction	Secondary Rd DOLAN RD	NCIC 9730	State Hwy? Y	Route 1	Postmile Prefix	Postmile 96.101	Side of Hwy N															
City UNINCORP.	County Monterey	Population 9	Rpt Dist	Beat 074	Type 1	CalTrans 5	Badge 018766	Collision Date 20130622	Time 1023	Day SAT															
Primary Collision Factor R-O-W AUTO		Violation 21802A	Collision Type	SIDESWIPE	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20140305																
Weather1 CLOUDY		Weather2	Rdwy Surface DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2	Spec Cond 0																		
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FNCTNG	Loc Type 1	Ramp/Int 5																
Party Info																									
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F DRVR	52	M	H	HNBD		ENT TRAF	W	A	0700	-	1998	-	3	N	-	M	G								
2 BICY	53	M	W	HNBD		PROC ST	-	L	0400	-	-	-	3	N	-	-	BICY	COMP PN	53	M	1	0	P	W	

**B. Safety Countermeasures (15 points max)**

Describe how the project/program/plan will remedy (one or more) potential safety hazards that contribute to pedestrian and/or bicyclist injuries or fatalities (only); Countermeasures must directly address the underlying factors that are contributing to the occurrence of pedestrian and/or bicyclist collisions.

1. Reduces speed or volume of motor vehicles in the proximity of non-motorized users? Yes No

a. Current speed and/or volume: (Max of 100 Words)

Words Remaining:

State Highway 1 has a 55 mile per hour (MPH) speed limit and a 2014 Annual Average Daily Traffic (AADT) of 31,000 south of Dolan Road and 37,000 north of Dolan Road in this section of the Project (Source: 2014 Traffic Volumes on California State Highways, Caltrans, Page 6, http://traffic-counts.dot.ca.gov/docs/2014_aadt_volumes.pdf).

b. Anticipated speed and/or volume after project completion : (Max of 100 Words)

Words Remaining:

Speed and/or volume will not change on State Highway 1 however this Project provides a separate continuous, convenient, and safe facility for bicycle and pedestrian traffic. The Project provides the only alternative for active transportation other than biking or walking with traffic along State Highway 1.

2. Improves sight distance and visibility between motorized and non-motorized users? Yes No

a. Current sight distance and/or visibility issue: (Max of 100 Words)

Words Remaining:

Currently, commuters and recreational bicyclists and pedestrians ride and walk on State Highway 1, a high speed, high volume facility. While there are shoulders on the road it is not designated as a class II facility and bikes and pedestrians are expected to share the road with motor vehicle traffic. The traffic volumes, speeds, and geometrics along State Highway 1 create a dangerous situation by not safely accommodating bicyclists and pedestrians.

b. Anticipated sight distance and/or visibility issue resolution: (Max of 100 Words)

Words Remaining:

Sight distance and visibility of bicyclist and pedestrians along State Highway 1 is improved by providing a separate facility for bicycle/pedestrian use. The only minor potential conflict point which will remain between automobile traffic and the pathway will be at the southern terminus of the pathway on Moss Landing Road (1,800 ADT and 25 MPH). At this location a pedestrian crossing will be installed that includes signs and striping which will be an improvement to current conditions.

3. Eliminates potential conflict points between motorized and non-motorized users, including creating physical separation between motorized and non-motorized users? Yes No

a. Current conflict point description: (Max of 100 Words)

Words Remaining:

Currently, commuters and recreational bicyclists and pedestrians ride and walk on State Highway 1, a high speed, high volume facility. While there are shoulders on the road it is not designated as a class II facility and bikes and pedestrians are expected to share the road with motor vehicle traffic. The traffic volumes, speeds, and geometrics along State Highway 1 create a dangerous situation by not safely accommodating bicyclists and pedestrians. See Attachment E, Photos of Existing Conditions.

b. Improvement that addresses conflict point: (Max of 100 Words)

Words Remaining:

By creating this path and providing non-motorized users an alternative to State Highway 1, potential conflicts at State Highway 1 and Moss Landing Road (N & S) will be eliminated and any new intersections that are created with the path will have significantly lower approach speeds and significant improvements to crossings and signage.

4. Improves compliance with local traffic laws for both motorized and non-motorized users? Yes No**5. Addresses inadequate vehicular traffic control devices?** Yes No

a. List traffic controls that are inadequate: (Max of 100 Words)

Words Remaining:

State Highway 1 has 6-8 foot wide shoulders that are not marked as bike lanes.

b. How are they inadequate? (Max of 100 Words)

Words Remaining:

Since the majority of the development along the roadway is on the west side, non-motorized users are given two options. The first is to cross State Highway 1 at any number of uncontrolled intersections, proceed to their destination, and then cross the roadway again. The second, is to illegally proceed along the west shoulder in the wrong direction of travel.

c. How does the project address the inadequacies? (Max of 100 Words)

Words Remaining:

Construction of this facility will provide a safe and legal way for non-motorized traffic to move through and throughout the north and south ends of the Moss Landing community.

a. List bicycle facilities, trails, crosswalks and/or sidewalks that are inadequate: (Max of 100 Words)

Words Remaining:


6. Addresses inadequate or unsafe bicycle facilities, trails, crosswalks and/or sidewalks?
 Yes No

There are currently no facilities, trails, crosswalks and/or sidewalks in the Project area.

b. How are they inadequate? (Max of 100 Words)
Words Remaining:

Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, a high speed, high volume facility. These high traffic volumes, high speeds, and geometrics do not safely accommodate non-motorized users. See Attachment E.

c. How does the project address the inadequacies? (Max of 100 Words)
Words Remaining:

Construction of a Class I bicycle path and bridge along State Highway 1, over the Elkhorn Slough to Moss Landing Road will provide a safe, high quality bicycle/pedestrian facility where none currently exist.

7. Eliminates or reduces behaviors that lead to collisions involving non-motorized users?
 Yes No

a. List of behaviors: (Max of 100 Words)
Words Remaining:

Non-motorized users currently cross State Highway 1 at uncontrolled intersections and travel along the west shoulder in the wrong direction of travel. A separated bicycle/pedestrian path and bridge would eliminate the potential for collisions between vehicles, bicyclist and or pedestrians.

b. How will the project will eliminate or reduce these behaviors? (Max of 100 Words)
Words Remaining:

A separated bicycle/pedestrian path and bridge will eliminate the potential for potential future collisions between vehicles, bicyclist, and/or pedestrians.

Attach a map to show how these hazards relate to the crashes documented in sub-questions "A". The map from sub-question "A" can be used or a new map can be created.

MossLandingBikeCollisionMap1.pdf

Plans

Describe how the plan will identify and plan to address hazards identified in the plan area, including the potential for mitigating safety hazards as a prioritization criterion, and/or including countermeasures that address safety hazards. (Max of 200 Words)

Words Remaining:

N/A

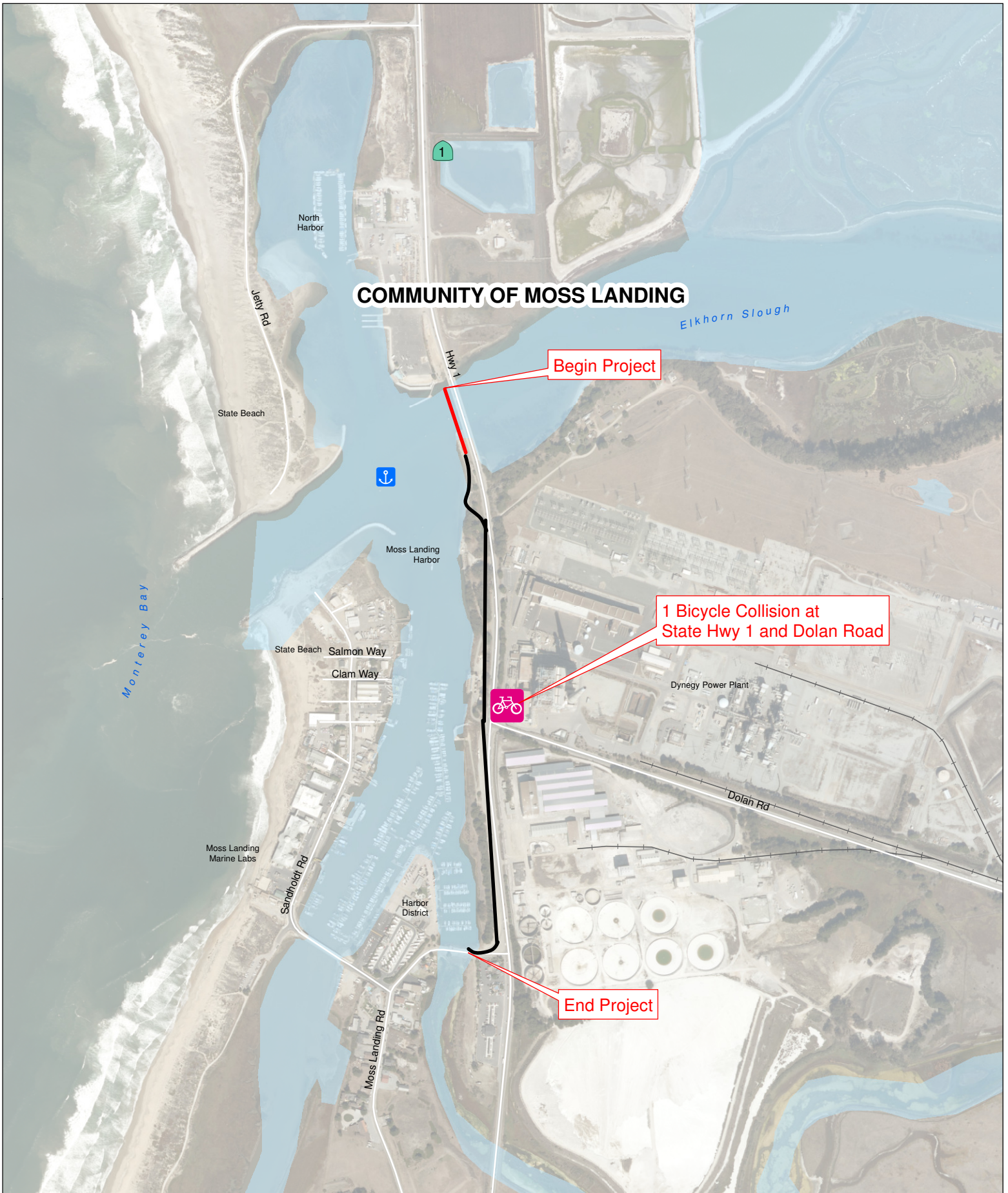
Non-Infrastructure

Describe how the program educates bicyclists, pedestrians, and/or drivers about safety hazards for pedestrians and bicyclists. Describe how the program encourages this safe behavior. If available, include documentation of effectiveness of similar programs in encouraging safe behavior. (Max of 200 Words)

Words Remaining:

N/A

Include, if applicable, a map identifying safety hazards and/or photos of safety hazards. Programs should address safety hazards that have been identified through police reports, collision history, field observations, and/or other verifiable source.



COMMUNITY OF MOSS LANDING

Begin Project

**1 Bicycle Collision at
State Hwy 1 and Dolan Road**

End Project



County of Monterey
Resource Management Agency
Public Works Department
April 5, 2016

MBSST BICYCLE COLLISION MAP
MONTEREY COUNTY, CA





Part B: Narrative Questions

Detailed Instructions for Question #4

QUESTION #4

PUBLIC PARTICIPATION and PLANNING (0-10 POINTS)

Describe the community based public participation process that culminated in the project/program proposal or will be utilized as part of the development of a plan.

- A. What is/was the process of defining future policies, goals, investments and designs to prepare for future needs of users of this project? How did the applicant analyze the wide range of alternatives and impacts on the transportation system to influence beneficial outcomes? (3 points max) (Max of 200 words)**

Words Remaining: 3

This segment of the Project is a collaborate effort between public agencies, non-profits and the public to construct a "Class I" trail as part of a larger goal to connect Monterey Bay from Pacific Grove to the Santa Cruz County line. Completion of this trail section, would move the MBSST one step closer to fulfilling its role in connecting the larger "California Coastal Trail," a planned 1,200 mile continuous interconnected public trail along the California Coast. The Monterey Bay Sanctuary Scenic Trail (MBSST) Master Plan was developed to articulate this vision and provide documentation for the proposed trail and was adopted in 2007 by the Transportation Agency for Monterey County (TAMC).

During the planning process for the MBSST Master Plan, key Project stakeholders (property owners, and other special interest groups with direct ties to the Project area), were identified and asked to contribute to the planning effort by participating in meetings and design workshops. Since 2001, when Congressman Farr convened meetings to develop support for the trail, there have been eight meetings between public agencies, non-profit organizations, as well as public participation at the two community meetings held for input on the overall project.

- B. Who: Describe who was/will be engaged in the identification and development of this project/program/plan (for plans: who will be engaged) and how they were/will be engaged. Describe and provide documentation of the type, extent, and duration of outreach and engagement conducted to relevant stakeholders. (3 points max) (Max of 200 words)**

Words Remaining: 2

The Project is a segment of a collaborate effort between public agencies, non-profit organizations and the public to construct a trail that would span Monterey Bay from Lovers Point in Pacific Grove to Wilder Ranch in Santa Cruz as part of the California Coastal Trail, a 1,200 mile trail along the California Coast, called for the by California State Legislature in 2000.

- 2001 - Congressman Sam Farr (D-Carmel) convened meeting of public agencies and non-profits eventually forming a Steering Committee.
- 2007 - Transportation Agency for Monterey County (TAMC) developed the Monterey Bay Sanctuary Scenic Trail (MBSST) Master Plan.
- 11/7/2012 – Project presented to the Bicycle Pedestrian Committee for a status update. See attached agenda in Attachment J-4B.
- 8/22/2013 – Project presented to the Agricultural Advisory Committee Land Use. See attached agenda in Attachment J-4B.
- 3/4/2015 – Community Meeting. See attached flyer in Attachment J-4B.
- 4/23/2015 – Community Meeting. See attached flyer in Attachment J-4B.
- 4/17/2014 – Meeting with Coastal Commission.

Congressman Sam Farr (D-Carmel) has been a strong supporter since the Project's inception working to provide funding to make completion of the project a reality.

- C. What: Describe the feedback received during the stakeholder engagement process and describe how the public participation and planning process has improved the project's overall effectiveness at meeting the purpose and goals of the ATP. (3 points max) (Max of 200 words)**

Words Remaining: 86

The Community and stakeholders have provided positive feedback. At the March 4, 2015 public meeting the community had the opportunity to participate in choosing aesthetics and signage for the Project where they stated that the Project will help connect North and South Moss Landing without having to drive on State Highway 1 or risk biking or walking along State Highway 1. The Project will provide a safe and convenient bicycle/pedestrian facility to the various destinations and activities between north and south Moss Landing. The Project meets the purpose and goals of the ATP by providing gap closure, local connections, regional connections, multi-modal connections, employment connections, activity center connections, safety, and improving public health.

- D. Describe how stakeholders will continue to be engaged in the implementation of the project/program/plan. (1 point max) (Max of 200 words)**

Words Remaining: 147

Final recommendations for the design and aesthetics of the trail and bridge were made at the April 23, 2015 public meeting by the community members. Additional community meetings will be held, if necessary, and announcements of the Project's progress will be posted on the County's website to keep the community informed.



Part B: Narrative Questions

Detailed Instructions for Question #5

QUESTION #5

IMPROVED PUBLIC HEALTH (0-10 POINTS)

- **NOTE: Applicants applying for the disadvantaged community set aside must respond to the below questions with health data specific to the disadvantaged communities. All applicants must cite information specific to project location and targeted users. Failure to do so will result in lost points.**

- A. Describe the health status of the targeted users of the project/program/plan. Describe how you considered health benefits when developing this project or program (for plans: how will you consider health throughout the plan). (5 points max) (Max of 200 words)**

Words Remaining:

North County in Monterey County, including Moss Landing, experiences socioeconomic and health inequities. In 2014, about 13% of the population was below the poverty line, compared to 10% in the City of Monterey a short 20 minute drive away. Almost 31% of North County residents are obese and 28% report they have only fair or poor health, compared to 26% and 19% in California. However, 32% of adults walked for transportation or leisure 150 minutes or more in the prior week (CHIS 2014). Notably there are no local gyms or exercise facilities in the community. A 2014/2015 community assessment of 4,220 adult residents across the county found obesity and lack of exercise as top two health problems of concern for North County residents. One of the top things residents in North County indicated they needed help with to exercise regularly was having safe neighborhoods to live, work, and play in along with specifically bike lanes, traffic controls, exercise paths, and dog parks. They also listed access to better local transportation options as a top concern. Thus, improved bikeability/walkability was considered when developing this project in order to support improved community built environment health benefits. (Source: Health Dept.)

- B. Describe how you expect your project/proposal/plan to promote healthy communities and provide outreach to the targeted users. (5 points max) (Max of 200 words)**

Words Remaining:

Monterey County Public Works is working with the Health Department to get community input and encourage use of newly developed infrastructure by targeted users in order to reduce health inequities in this community through a multi-pronged community engagement approach. The Health Department is involved in ongoing community engagement opportunities which will continue as part of this project. These include holding a community-focused civic engagement academy, supporting North County community action teams for the early childhood development initiative, and participating in a North County cradle-to-career initiative. These programs and initiatives have routine meetings which will include outreach and engagement by trained Health Department staff for these projects and which are developing a cadre of community members that are informed and active in community improvement. Additionally, the Health Department is supporting the development of small grassroots active community projects and engaging parents with the schools. The Health Department is also actively involved with connecting engaged community members with area transportation and community planning to encourage healthier community development approaches as part of its Health in All Policies councils and will use these to promote healthy community approaches as part of this project.



Part B: Narrative Questions

Detailed Instructions for Question #6

QUESTION #6

COST EFFECTIVENESS (0-5 POINTS)

A project's cost effectiveness is considered to be the relative costs of the project in comparison to the project's benefits as defined by the purpose and goals of the ATP. This includes the consideration of the safety and mobility benefit in relation to both the total project cost and the funds provided.

Explain why the project is considered to have the highest Benefit to Cost Ratio (B/C) with respect to the ATP purpose and goals of "increased use of active modes of transportation". (5 points max.) (Max of 200 words)

Words Remaining: 66

The total benefit of the Project was calculated looking at Collision Reduction Factor (CRF) R37, see below, for bicyclist and pedestrians to travel along State Highway 1 (a high traffic volume and highway speeds facility). The only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1. The Project will create a separate path and bridge thus reducing the potential for serious bicycle and pedestrian injuries or fatalities.

For the collision measure with identified CRF, the following depicts the CRF and its anticipated Project life.

R37 - Install Sidewalk/pathway (to avoid walking along roadway)

Affects Crash Type - Peds & Bike

CRF (%) - 80.00%

Service Life (years) - 20

Source: Caltrans' Local Highway Safety Improvement Program, Local Roadway Safety Manual

http://www.dot.ca.gov/hq/LocalPrograms/HSIP/prepare_now.htm



Part B: Narrative Questions

Detailed Instructions for Question #7

QUESTION #7

LEVERAGING OF NON-ATP FUNDS (0-5 POINTS)

A. The application funding plan will show all federal, state and local funding for the project: (5 points max.)

Based on the project funding information provided earlier in the application, the following Leveraging and Matching amounts are designated for this project. Applicants must review and verify these values meet the following criteria:

Leveraging Funds

Non-ATP funds; either already expended by the applicant or funds to be programmed for use on elements within the requested ATP project. This non-ATP funding can only be considered "Leveraging" funding if it goes towards ATP eligible costs.

Matching Funds

The portion of the Leveraging funding that can be used as the local match if Federal ATP funding is programmed. These must be non-federal funds not yet expended and provided by the applicant in a specific project phase.

If these numbers do not match this criteria and/or the applicant's expectations, the numbers inputted earlier need to be revised.

Funding in \$1,000s

PA&ED Phase Project Delivery Costs:

Leveraging Funding: \$795
Match Funding: \$0

Designate the Funding Type: Federal
Designate the Funding Type: _____

PS&E Phase Project Delivery Costs:

Leveraging Funding: \$1,952
Match Funding: \$0

Designate the Funding Type: Federal
Designate the Funding Type: _____

Right of Way Phase Project Delivery Costs:

Leveraging Funding: \$305
Match Funding: \$0

Designate the Funding Type: Local agency funds
Designate the Funding Type: _____

Construction Phase Project Delivery Costs:

Leveraging Funding: \$2,788
Match Funding: \$0

Designate the Funding Type: Federal
Designate the Funding Type: _____

NON-INFRASTRUCTURE (NI) AND "PLAN" PROJECTS:

Leveraging Funding: \$0
Match Funding: \$0

Designate the Funding Type: _____
Designate the Funding Type: _____

OVERALL TOTALS FOR PROJECT/APPLICATION:

Total Project Costs: \$13,427

Leveraging Funding: \$5,840

% of Total Project Cost: 43.49 %

Match Funding: \$0

% of Total Project Cost: 0.00 %

Total Points received for "leveraging funding": (Auto-calculated)

Optional: If desired, clarifications can be added to explain the leveraging funding and its intended use on the ATP project.

(Max of 100 Words)

Words Remaining:

The County has secured \$4.3 million in Local, State, and Federal grant funds and is pending \$1.5 million in State funding or a total of 43.49 percent (%) of the total Project cost.

The following describes the costs for the past milestones and estimated costs for future milestones:

Design/Environmental - \$2,747,000 Secured Funding - Past Milestone

Right-of-way/Utilities - \$305,000 Secured Funding - Past Milestone

ATP CYCLE 3 APPLICATION FORM

DLA-001 (NEW 4/2016)
v1.2



Construction Management - \$1,353,198 Secured/Partial Pending Funding - Future Milestone
Construction - \$9,021,320 Partially Secured Funding - Future Milestone



Part B: Narrative Questions

Detailed Instructions for Question #8

QUESTION #8

USE OF CALIFORNIA CONSERVATION CORPS (CCC) OR A CERTIFIED COMMUNITY CONSERVATION CORPS (0 or -5 POINTS)

- Applicant has not coordinated with both corps, or Tribal Corps (if applicable) (-5 points)
- Applicant contacted the corps; but does not intend to partner with any corps (-5 points)

Step 1: The applicant must submit the following information via email concurrently to both the CCC AND certified community conservation corps at least 5 days prior to application submittal to Caltrans. The CCC and certified community conservation corps will respond within five (5) business days from receipt of the information.

- Project Title
- Project Description
- Detailed Estimate
- Project Schedule
- Project Map
- Preliminary Plan

Click on the following links for the California Conservation Corps and community conservation corps Representative ATP contact information:

<http://calocalcorps.org/active-transportation-program/>

<http://www.ccc.ca.gov/work/programs/ATP/Pages/ATP%20home.aspx>

The applicant must also attach any email correspondence from the CCC and certified community conservation corps or Tribal corps (if applicable) to the application verifying communication/participation. Failure to attach their email responses will result in a loss of 5 points.

Attach submittal email, response email and any attachment(s) from the CCC:

Re ATP Proposal-County of Monterey MBSST.pdf

Attach submittal email, response email and any attachment(s) from the certified community conservation corps:

Re ATP Grant Submittal - MBSST.pdf

Attach submittal email, response email and any attachment(s) from the Tribal corps (If applicable):

Step 2: The applicant has coordinated with the CCC AND with the certified community conservation corps, or the Tribal corps and determined the following: (check appropriate box)

- Applicant intends to utilize the CCC, certified community conservation corps, or the Tribal corps on the following items listed below. (0 points) (Max of 50 Words)

Words Remaining:

The CCC will participate in the landscaping portion of the Project.

- No corps can participate in the project. (0 points)
- At the time that the application was submitted, the applicant had not received a response from the following corps: (0 points)
- the CCC the community conservation corps the Tribal corps (if applicable)

From: Active Transportation Program [inquiry@atpcommunitycorps.org]
Sent: Wednesday, May 25, 2016 12:56 PM
To: Carranza, Ogarita x5174
Subject: Re: ATP Grant Application Submittal - MBSST - Moss Landing Bike/Ped Path & Bridge Project

Hello Ogarita,

Thank you for contacting the Local Conservation Corps. Unfortunately, we are unable to participate in this project. Please include this email with your application as proof that you reached out to the Local Conservation Corps.

Thank you,
Dominique

On Thu, May 12, 2016 at 11:07 AM, Carranza, Ogarita x5174 <carranzao@co.monterey.ca.us> wrote:

The County of Monterey is submitting Active Transportation Program grant applications for two projects (MBSST – Moss Landing Segment and Las Lomas Drive Bicycle Lane and Pedestrian Improvements). At this time I am only sending the information for the MBSST project in separate emails due to the size of the plans (25MB). Information for the Las Lomas Drive project will be sent separately. As required, attached are the project description, project schedule, project map, detailed estimate, and preliminary plans for the Monterey Bay Sanctuary Scenic Trail (MBBST) – Moss Landing Segment project. Please feel free to contact me know if you need anything else and thank you in advance for your review of the projects.

Ogarita Carranza
Management Analyst II

County of Monterey
Department of Public Works
855 E Laurel Drive, Bldg. B
Salinas, CA 93905
[\(831\) 755-5174](tel:8317555174)
[\(831\) 755-4958](tel:8317554958) Fax
e-mail: carranzao@co.monterey.ca.us

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Dominique Lofton | Program Assistant
Environmental & Energy Consulting
1121 L Street, Suite 400
Sacramento, CA 95814
[916.426.9170](tel:916.426.9170) | inquiry@atpcommunitycorps.org

Carranza, Ogarita x5174

From: Wallace, Melanie@CCC [Melanie.Wallace@ccc.ca.gov] on behalf of ATP@CCC [ATP@CCC.CA.GOV]
Sent: Monday, May 16, 2016 8:55 AM
To: Carranza, Ogarita x5174
Subject: RE: ATP Proposal- County of Monterey MBSST Project

Good morning Ogarita,

The CCC is able to participate in the landscaping portion of this project. Please include this email with your application as proof of contacting us.

Kind regards,

Melanie Wallace
Chief Deputy Analyst
California Conservation Corps
1719 24th Street
Sacramento, CA 95816
O (916)341-3153
M (916)508-1167
F (877)315-5085
melanie.wallace@ccc.ca.gov

Every Californian should conserve water. Find out how at:



SaveOurWater.com · Drought.CA.gov

From: Wohlgemuth, Janet@CCC
Sent: Monday, May 16, 2016 8:39 AM
To: Wallace, Melanie@CCC <Melanie.Wallace@ccc.ca.gov>
Subject: RE: ATP Proposal- County of Monterey MBSST Project

Melanie
We will be able to do the landscaping only for this project
Thanks
Janet

From: Wallace, Melanie@CCC
Sent: Thursday, May 12, 2016 11:35 AM
To: Wohlgemuth, Janet@CCC <Janet.Wohlgemuth@CCC.CA.GOV>
Subject: FW: ATP Proposal- County of Monterey MBSST Project

Sorry for the confusion, Janet. This information is only in reference to the Monterey Bay Sanctuary Scenic Trail (MBBST) – Moss Landing Segment project. The bike lane project info will be sent separately.

Thanks again,

Melanie

From: Wallace, Melanie@CCC

Sent: Thursday, May 12, 2016 11:25 AM

To: Wohlgemuth, Janet@CCC <Janet.Wohlgemuth@CCC.CA.GOV>

Cc: Burks-Herrmann, Brenda@CCC <Brenda.Burks-Herrmann@CCC.CA.GOV>; ATP@CCC <ATP@CCC.CA.GOV>

Subject: ATP Proposal- County of Monterey MBSST Project

Hi Janet,

Attached, is project information from the County of Monterey regarding their Moss Landing Segment and Las Lomas Drive Bicycle Lane and Pedestrian Improvements (MBSST) project proposal. Please review the information and let me know by Wednesday, May 18 if Monterey may be able to participate. Please contact Ogarita directly if you need additional information.

Thank you,

Melanie Wallace
916.341.3153





Part B: Narrative Questions

Detailed Instructions for Question #9

QUESTION #9

APPLICANT'S PERFORMANCE ON PAST ATP FUNDED PROJECTS (0 - 10 points)

For Caltrans use only.



Part C: Application Attachments

Applicants must ensure all data in this part of the application is fully consistent with the other parts of the application. See the Application Instructions and Guidance document for more information and requirements related to Part C.

List of Application Attachments

The following attachment names and order must be maintained for all applications. Depending on the Project Type (I, NI or Plans) some attachments will be intentionally left blank. All non-blank attachments must be identified in hard-copy applications using “tabs” with appropriate letter designations

Application Signature Page (Required for all applications)	Attachment A
Attachment A - Signature Page.pdf	
Engineer's Checklist (Required for Infrastructure & Combo Projects)	Attachment B
Attachment B - Engineer's Checklist.pdf	
Project Location Map (Required for all applications)	Attachment C
MBSST Location Map.pdf	
Project Map/Plans showing existing and proposed conditions (Required for all Infrastructure Projects; Optional for 'Non-Infrastructure' and 'Plan' Projects)	Attachment D
Attachment D - Plans_MBSST.pdf	
Photos of Existing Conditions (Required for all applications)	Attachment E
Attachment E - MBSST Photos.pdf	
Project Estimate (Required for all Infrastructure Projects)	Attachment F
MBSST_EngrEstimate_(Attachment F)_4-31-16.xlsm	
Non-Infrastructure Work Plan (Form 22-R) (Required for all projects with Non-Infrastructure Elements)	Attachment G
Letters of Support (10 maximum) (Required or recommended for all projects as designated in the instructions) (All letters must be scanned into one document.)	Attachment H
Attachment H - Letters of Support_MBSSTrev3.pdf	
Exhibit 22-F State Funding	Attachment I
Additional Attachments (Additional attachments may be included. They should be organized in a way that allows application reviews easy identification and review of the information.) (All additional attachments must be scanned into one document.)	Attachment J
Attachment J - Additional Attachments.pdf	

Part C: Attachments Attachment A: Signature Page

IMPORTANT: Applications will not be accepted without all required signatures.

Implementing Agency: Chief Executive Officer, Public Works Director, or other officer authorized by the governing board

The undersigned affirms that their agency will be the "Implementing Agency" for the project if funded with ATP funds and they are the Chief Executive Officer, Public Works Director or other officer **authorized by their governing board with the authority to commit the agency's resources and funds.** They are also affirming that the statements contained in this application package are true and complete to the best of their knowledge. For infrastructure projects, the undersigned affirms that they are the manager of the public right-of-way facilities (responsible for their maintenance and operation) or they have authority over this position.

Signature:  Date: 6/9/2016
 Name: PENNY J. YOUNG Phone: 831-7554831
 Title: DIRECTOR OF PUBLIC WORKS e-mail: youngb@co.monterey.ca.us

For projects with a Partnering Agency: Chief Executive Officer or other officer authorized by the governing board
(For use only when appropriate)

The undersigned affirms that their agency is committed to partner with the "Implementing Agency" and agrees to assume the responsibility for the ongoing operations and maintenance of the facility upon completion by the implementing agency and they intend to document such agreement per the CTC guidelines. The undersigned also affirms that they are the Chief Executive Officer or other officer authorized by their governing board with the authority to commit the agency's resources and funds. They are also affirming that the statements contained in this application package are true and complete to the best of their knowledge.

Signature: _____ Date: _____
 Name: _____ Phone: _____
 Title: _____ e-mail: _____

For projects with encroachments on the State right-of-way: Caltrans District Traffic Operations Office Approval*
(For use only when appropriate)

If the application's project proposes improvements within a freeway or state highway right-of-way, whether it affects the safety or operations of the facility or not, it is required that the proposed improvements be reviewed by the district traffic operations office and either a letter of support/acknowledgement from the traffic operations office be attached or the signature of the traffic manager be secured in the application. The Caltrans letter and/or signature does not imply approval of the project, but instead is only an acknowledgement that Caltrans District staff is aware of the proposed project; and upon initial review, the project appears to be reasonable and acceptable.

Is a letter of support/acknowledgement attached? Yes If yes, no signature is required. If no, the following signature is required.

Signature: _____ Date: _____
 Name: _____ Phone: _____
 Title: _____ e-mail: _____

* Contact the District Local Assistance Engineer (DLAE) for the project to get Caltrans Traffic Ops contact information. DLAE contact information can be found at <http://www.dot.ca.gov/hq/LocalPrograms/dlae.htm>

ATP Engineer's Checklist for Infrastructure Projects

Required for "Infrastructure" applications ONLY

This application checklist is to be used by the engineer in "responsible charge" of the preparation of this ATP application to ensure all of the primary elements of the application are included as necessary to meet the CTC's requirements for a PSR-Equivalent document (per CTC's ATP Guidelines and CTC's Adoption of PSR Guidelines - Resolution G-99-33) and to ensure the application is free of critical errors and omissions; allowing the application to be accurately ranked in the statewide and regional ATP selection processes.

Special Considerations for Engineers before they Sign and Stamp this document attesting to the accuracy of the application:

Chapter 7; Article 3; Section 6735 of the Professional Engineer's Act of the State of California requires engineering calculation(s) or report(s) be either prepared by or under the responsible charge of a licensed civil engineer. Since the corresponding ATP Infrastructure-application defines the scope of work of a future civil construction project and requires complex engineering principles and calculations which are based on the best data available at the time of the application, the application must be signed and stamped by a licensed civil engineer.

By signing and stamping this document, the engineer is attesting to this application's technical information and engineering data upon which local agency's recommendations, conclusions, and decisions are made. This action is governed by the Professional Engineer's Act and the corresponding Code of Professional Conduct, under Sections 6775 and 6735.


The following checklist is to be completed by the engineer in "responsible charge" of defining the project's Scope, Cost and Schedule per the expectations of the CTC's PSR Equivalent. The checklist is expected to be used during the preparation of the documents, but not initialed and stamped by the engineer until the final application and application attachments are complete and ready for submission to Caltrans.

1. Vicinity map /Location map

Engineer's Initials: 

- a. The project limits must be clearly depicted in relationship to the overall agency boundary

2. Project layout-plan/map showing existing and proposed conditions must:

Engineer's Initials: 

- a. Be to a scale which allows the visual verification of the overall project "construction" limits and limits of each primary element of the project. Scale must be shown on the plan/map
- b. Show the full scope of the proposed project, including any non-participating construction items
- c. Show all changes to existing motorized/non-motorized lane and shoulder widths. Label the proposed widths
- d. Show agency's right of way (ROW) lines when permanent or temporary ROW impacts are possible. (As appropriate, also show Caltrans', Railroad, and all other government agencies ROW lines)

3. Typical cross-section(s) showing existing and proposed conditions.

Engineer's Initials: 

(Include cross-section for each controlling configuration that varies significantly from the typical)

- a. Show and dimension: changes in lane widths, ROW lines, side slopes, etc.

4. Detailed Engineer's Estimate

Engineer's Initials: 

- a. The Caltrans Project Estimate (Attachment F) must be filled out per the instructions and attached to the application, in the appropriate location.
- b. Each of the main project elements are broken out into separate construction items. The costs for each item are based on calculated quantities and appropriate corresponding unit costs
- c. All non-participating costs in relation to the ATP funding are clearly identified and accounted for separately from the eligible costs. The non-participating (or ineligible) costs must be consistent with Caltrans guidelines as shown in Local Assistance Program Guidelines chapter 22.6
- d. All project elements the applicant intends to utilize the CCC, certified community conservation corps, or tribal corps on need to be clearly identified and accounted for
- e. All project development costs to be funded by the ATP need to be accounted for in the total project cost

5. **Crash/Safety Data, Collision maps and Countermeasures:**

Engineer's Initials: RC

- a. Confirmation that crash data shown is depicted accurately, is shown to scale, and occurred within influence area of proposed improvements.

6. **Project Schedule and Requested programming of ATP funding**

Engineer's Initials: RC

- a. All applicants must anticipate receiving federal ATP funding for the project and therefore the project schedules and programming included in the application must account for all applicable federal requirements and timeframes.
- b. "Completed Dates" for project Milestone Dates shown in the application have been reviewed and verified
- c. "Expected Dates" for project Milestone Dates shown in the application account for all reasonable project timetables, including: Interagency MOUs, Caltrans agreements, CTC allocations, FHWA authorizations, federal environmental studies and approvals, federal right-of-way acquisitions, federal consultant selections, project permits, etc.
- d. The fiscal year and funding amounts shown in the PPR must be consistent with Implementing Agency's expected project milestone dates and available matching funds.

7. **Warrant studies/guidance (Check if not applicable)**

Engineer's Initials: _____

N/A

- a. For new Traffic Control Signals – an engineering study that includes analysis of Signal Warrants 1- 9 (CA MUTCD) must be submitted. For ATP funding, warrants 4, 5 or 7 should be met but the final decision to install a signal must be made by the engineer. The engineering study (and any additional documentation of the engineering judgment supporting the Traffic Control Signal, if needed) must include the name and license number of the responsible engineer and must be attached to the application in the "Additional Attachments" section.

8. **Additional narration and documentation:**

Engineer's Initials: RC

- a. The text in the "Narrative Questions" in the application is consistent with and supports the engineering logic and calculations used in the development of the plans/maps and estimate
- b. When needed to clarify non-standard ATP project elements (i.e. vehicular roadway widening necessary for the construction of the primary ATP elements); appropriate documentation is attached to the application to document the engineering decisions and calculations requiring the inclusion of these non-standard elements.

Licensed Engineer:

Engineer's Stamp:

Name (Last, First): Chapman, Ryan

Title: Traffic Engineer

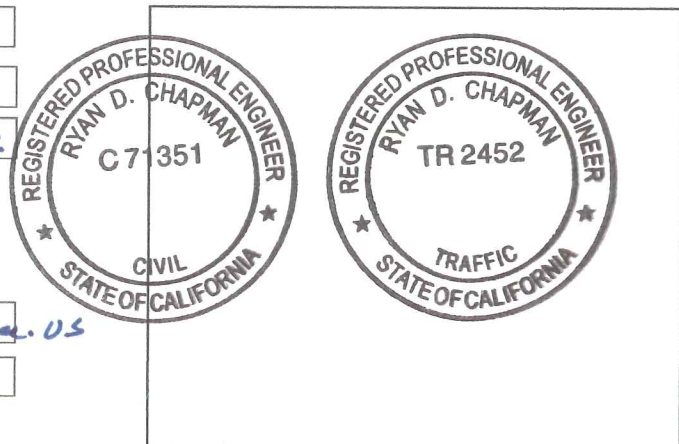
Engineer License Number: C 71351, Tr 2452

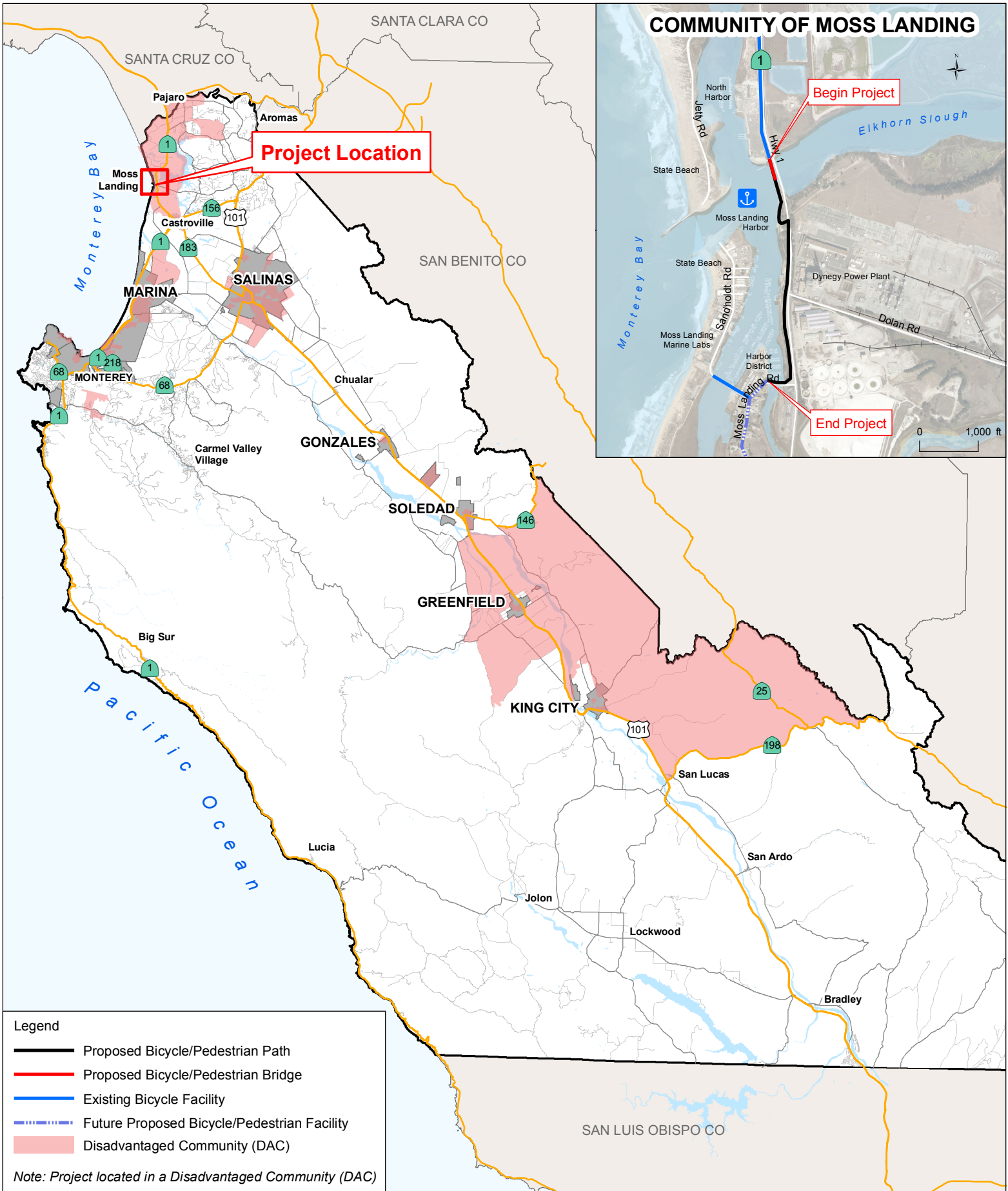
Signature: RC

Date: 6/8/16

Email: chapmanr@co.monterey.ca.us

Phone: (931) 716-3009





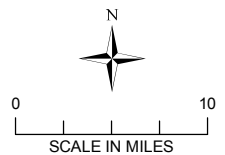
- Legend**
- Proposed Bicycle/Pedestrian Path
 - Proposed Bicycle/Pedestrian Bridge
 - Existing Bicycle Facility
 - Future Proposed Bicycle/Pedestrian Facility
 - Disadvantaged Community (DAC)

Note: Project located in a Disadvantaged Community (DAC)



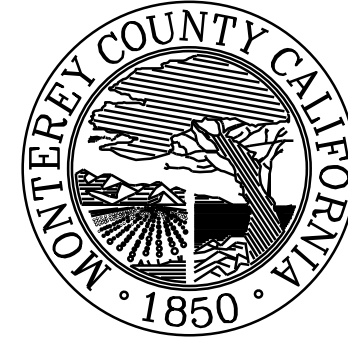
County of Monterey
 Resource Management Agency
 Public Works Department
 April 5, 2016

ATTACHMENT D MBSST LOCATION MAP MONTEREY COUNTY, CA



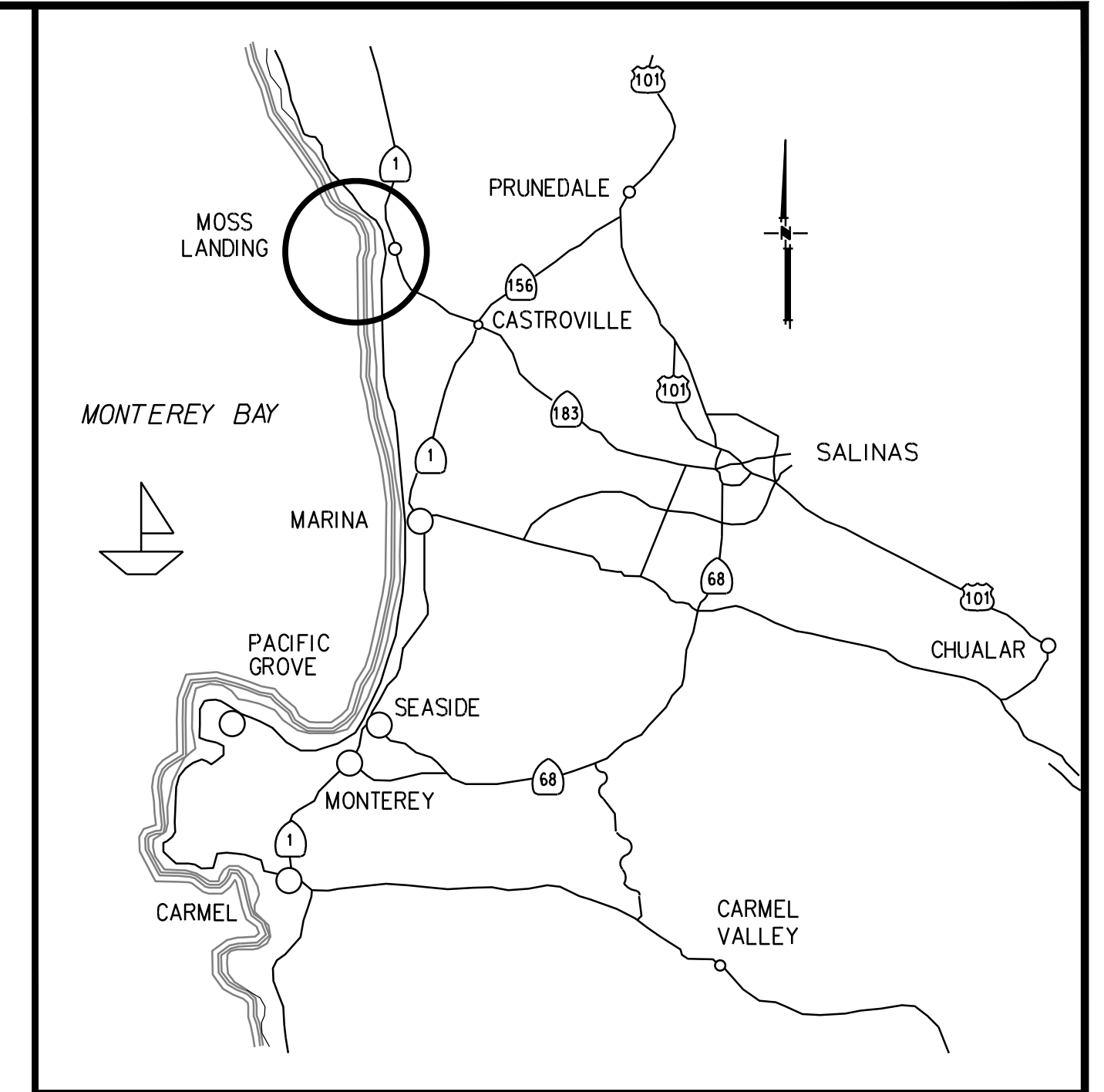
SHEET INDEX

1. TITLE SHEET
2. SURVEY CONTROL PLAN
3. TYPICAL CROSS SECTIONS
4. TYPICAL CROSS SECTIONS
5. TYPICAL CROSS SECTIONS
6. PLAN AND PROFILE (STA 55+82.08 TO 60+50.00)
7. PLAN AND PROFILE (STA 60+50.00 TO 70+50.00)
8. PLAN AND PROFILE (STA 70+50.00 TO 81+00.00)
9. PLAN AND PROFILE (STA 81+00.00 TO 92+25.00)
10. PLAN AND PROFILE (STA 92+25.00 TO 101+00.00)
11. REVETMENT PLAN
12. CONSTRUCTION DETAILS
13. SIGNING AND STRIPING PLAN (STA 55+82.08 TO 81+00.00)
14. SIGNING AND STRIPING PLAN (STA 81+00.00 TO 101+00.00)
15. TRAFFIC HANDLING PLAN
16. LANDSCAPE SITE DETAILS
17. TREE REMOVAL PLAN (STA 55+82.08 TO 81+00)
18. TREE REMOVAL PLAN (STA 81+00 TO 101+00)
19. EROSION CONTROL PLAN
20. RETAINING WALL NO. 1 LAYOUT NO. 1
21. RETAINING WALL NO. 1 LAYOUT NO. 2
22. RETAINING WALL NO. 2 LAYOUT
23. RETAINING WALL NO. 3 LAYOUT
24. RAILING DETAILS
25. RETAINING WALL DETAILS (RW1)
26. RETAINING WALL DETAILS (RW2)
27. RETAINING WALL DETAILS (RW3)
- 28-46 ELKHORN SLOUGH BIKE BRIDGE SHEETS (B1-B19)
- 47-57 PIPE UNDERCROSSING (S1-S11)

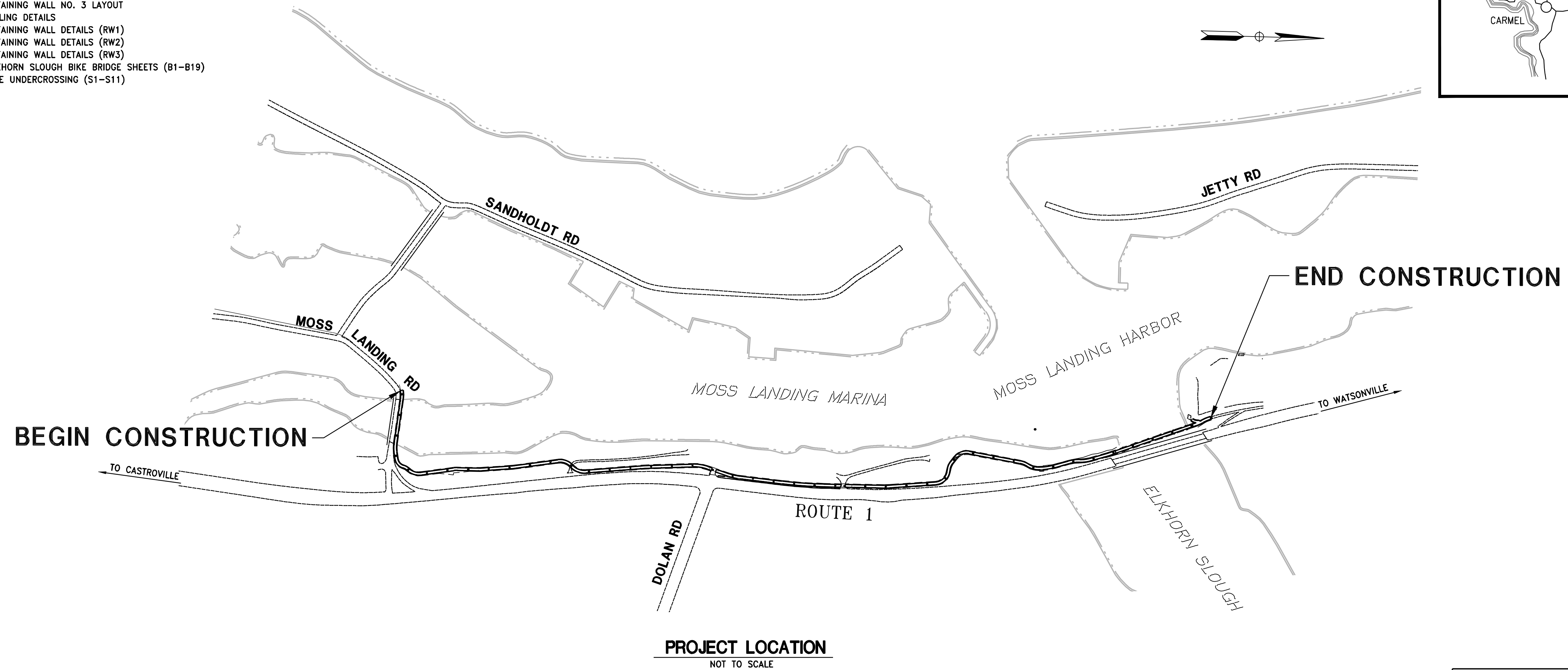


**COUNTY OF MONTEREY
MONTEREY BAY SANCTUARY SCENIC TRAIL
MOSS LANDING SEGMENT**

To be supplemented by Caltrans Standard Plans Dated 2010
Federal Aid Project No. HPLU-5944 (085)
Monterey County Project No. 866865



VICINITY MAP
NOT TO SCALE

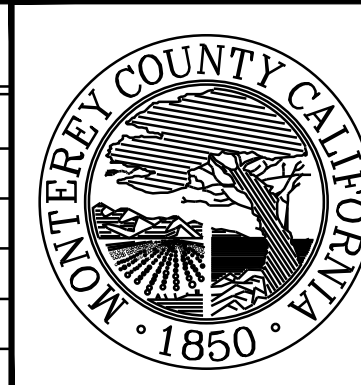


APPROVED BY: _____
COUNTY OF MONTEREY DIRECTOR OF PUBLIC WORKS DATE _____

SUBMITTED BY: LUCAS J. FUSON, P.E.	
DESIGN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
DRAWN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
SPECIFICATIONS WRITTEN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
APPROVAL RECOMMENDED BY: ALI HEMMATI, P.E.	



NO.	DATE	REVISION	APPROVED



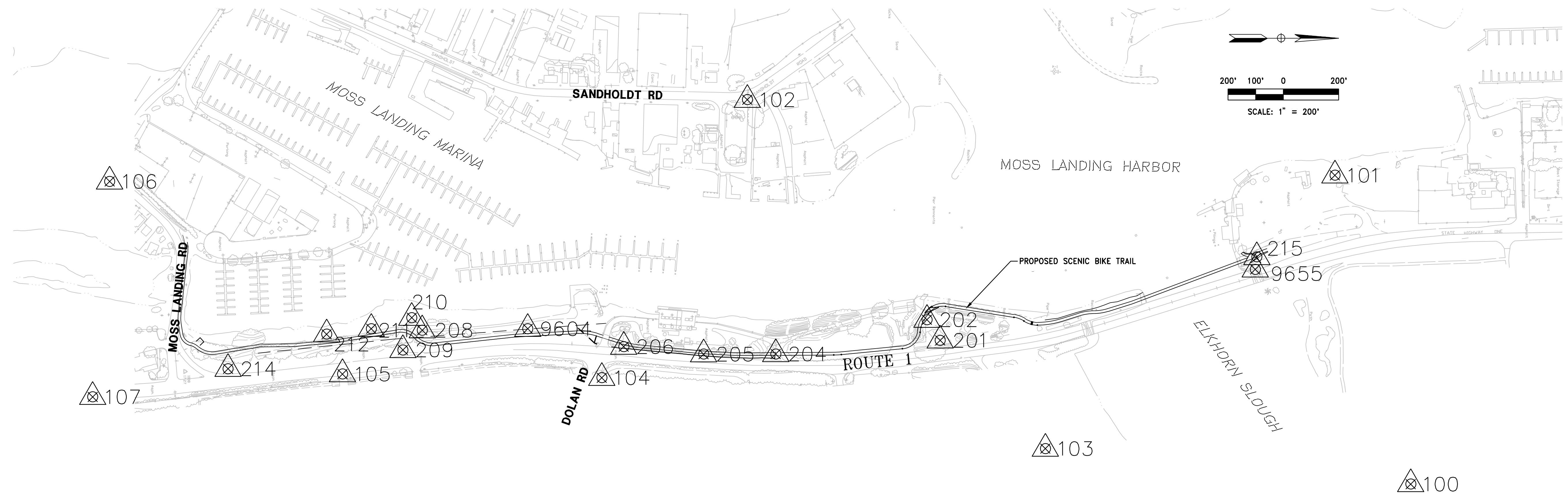
COUNTY OF MONTEREY
RESOURCE MANAGEMENT AGENCY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING SECTION
168 WEST ALISAL STREET, 2ND FLOOR
SALINAS, CALIFORNIA 93901-2680
(831) 755-4800/FAX (831) 755-4958

MONTEREY BAY SANCTUARY SCENIC TRAIL MOSS LANDING SEGMENT		
TITLE SHEET		
DATE 11/01/15	DRAWING VIEW Layout1	SHEET 1 OF 57
SCALE		

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LEGEND

 SURVEY CONTROL POINT



SURVEY MONUMENT CONTROL DATA				
NO.	COORDINATES		ELEVATION	DESCRIPTION AND LOCATION
	NORTHING	EASTING		
100	2190578.947	5747258.676	8.342	REBAR WITH CAP STAMPED WOOD RODGERS
101	2190304.789	5746142.704	11.651	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
102	2188181.136	5745869.314	11.771	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
103	2189258.34	5747130.624	29.109	5/8 REBAR WITH CAP STAMPED WOODRODGERS
104	2187655.161	5746874.356	32.777	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
105	2186718.056	5746860.907	29.974	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
106	2185876.33	5746164.995	7.076	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
107	2185815.191	5746943.478	13.648	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
201	2188878.211	5746739.438	33.525	5/8 REBAR WITH CAP STAMPED WOODRODGERS
202	2188830.503	5746664.612	27.357	5/8 REBAR WITH CAP STAMPED WOODRODGERS
204	2188283.978	5746788.639	31.662	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
205	2188023.033	5746788.979	33.217	5/8 REBAR WITH CAP STAMPED WOODRODGERS
206	2187734.622	5746761.898	33.431	5/8 REBAR WITH CAP STAMPED WOODRODGERS
208	2187006.393	5746702.705	31.133	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
209	2186936.47	5746774.028	31.376	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
210	2186968.356	5746657.418	25.99	5/8 REBAR WITH CAP STAMPED WOODRODGERS
211	2186822.023	5746697.351	20.335	5/8 REBAR WITH CAP STAMPED WOODRODGERS
212	2186660.802	5746717.087	15.236	5/8 REBAR WITH CAP STAMPED WOODRODGERS
214	2186304.354	5746841.856	19.748	60D NAIL WITH WASHER STAMPED WOOD RODGERS
215	2190020.318	5746440.311	11.296	MAG NAIL WITH WASHER STAMPED WOOD RODGERS
9604	2187387.63	5746697.398	32.966	FD CAL TRANS IP W/CTL PP WITH NAIL
9655	2190016.849	5746482.905	16.959	FD CAL TRANS 2" BRASS CAP STAMPED 96.55

NOTES:

- COORDINATE INFORMATION IS BASED ON CALIFORNIA STATE COORDINATE SYSTEM NAD 83, ZONE 4 "GROUND" DISTANCES. TO OBTAIN GRID DISTANCE, SCALE BY 0.9999492592
- VERTICAL CONTROL IS BASED ON CALTRANS POINT NO.96.04, (MON 1 PM 96.04) NGVD29 ELEVATION = 32.966

STATION

SUBMITTED BY: LUCAS J. FUSON, P.E.

DESIGN BY: LUCAS J. FUSON, P.E.

DRAWN BY: LUCAS J. FUSON, P.E.

SPECIFICATIONS WRITTEN BY: LUCAS J. FUSON, P.E.

APPROVAL RECOMMENDED BY: ALI HEMMATI, P.E.



NO.	DATE	REVISION	APPROVED



COUNTY OF MONTEREY
 RESOURCE MANAGEMENT AGENCY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING SECTION
 168 WEST ALISAL STREET, 2ND FLOOR
 SALINAS, CALIFORNIA 93901-2680
 (831) 755-4800/FAX (831) 755-4958

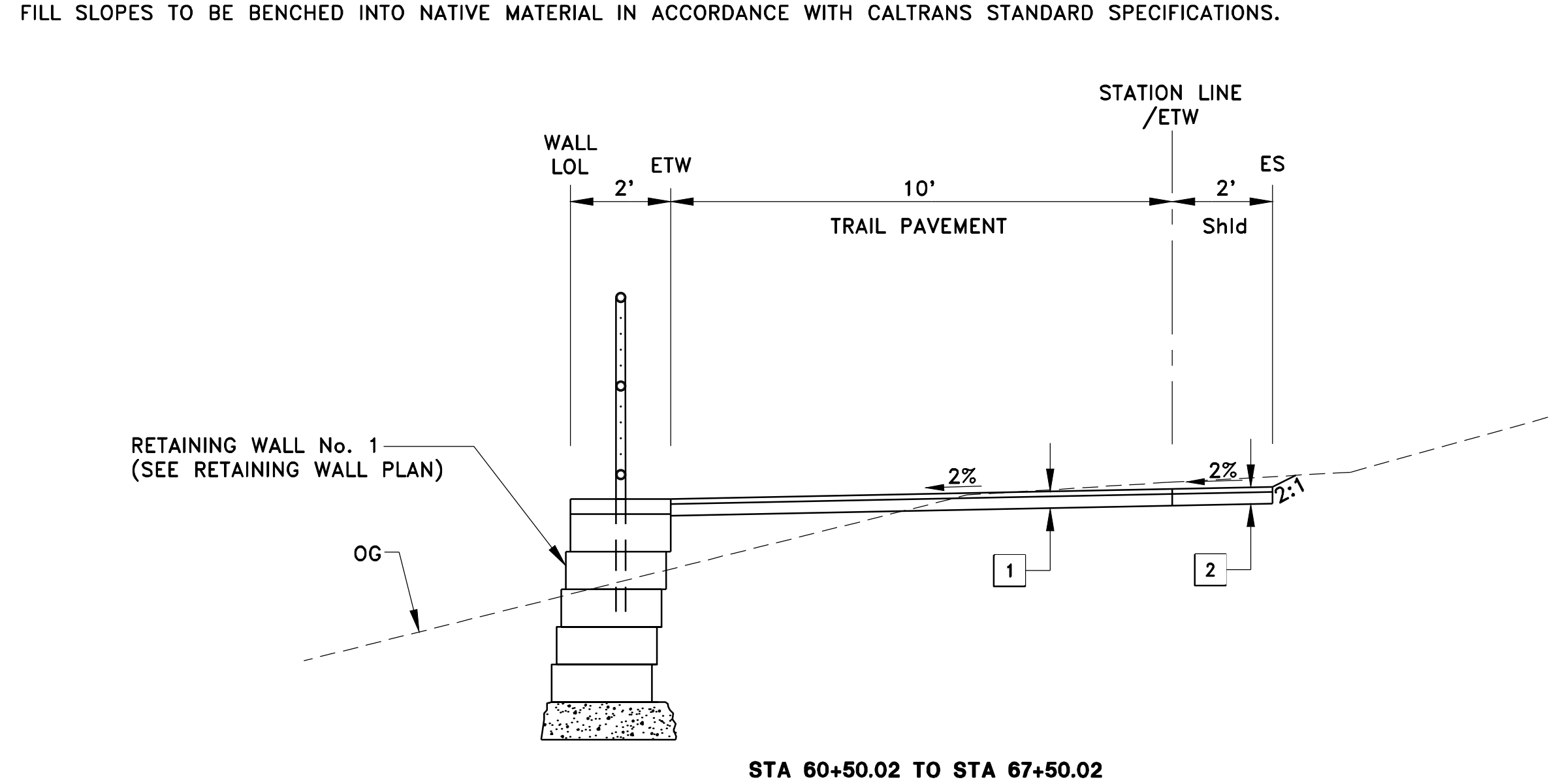
**MONTEREY BAY SANCTUARY SCENIC TRAIL
 MOSS LANDING SEGMENT**

SURVEY CONTROL PLAN

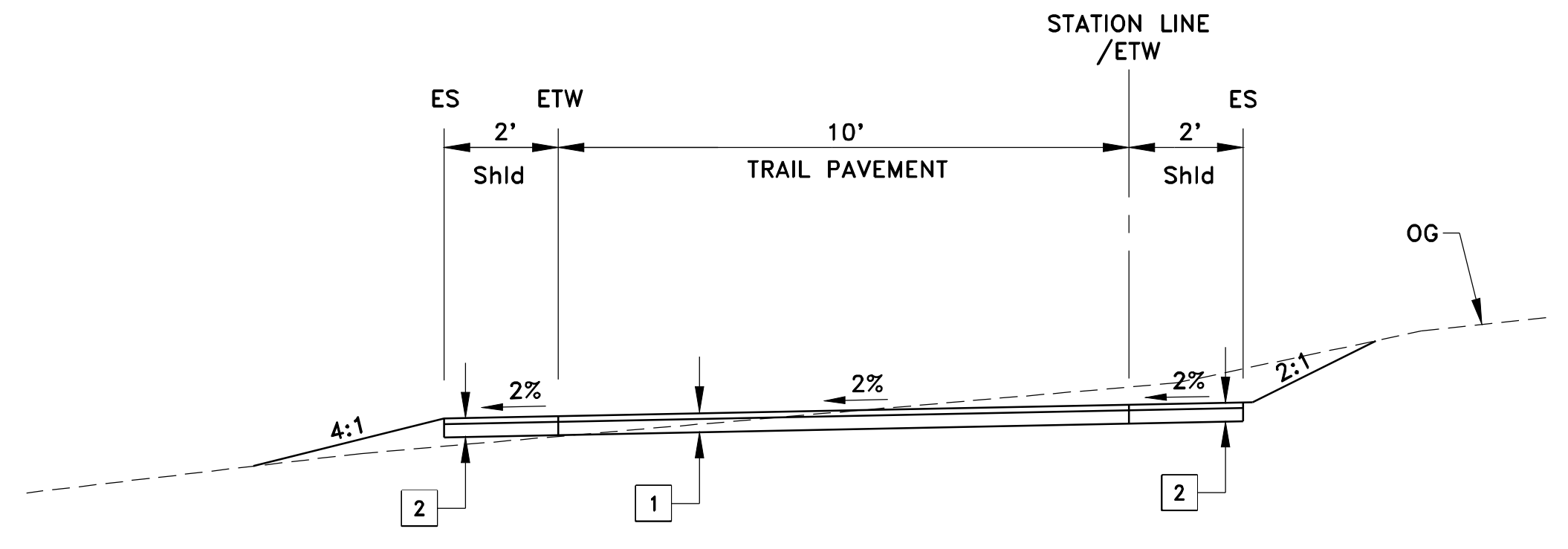
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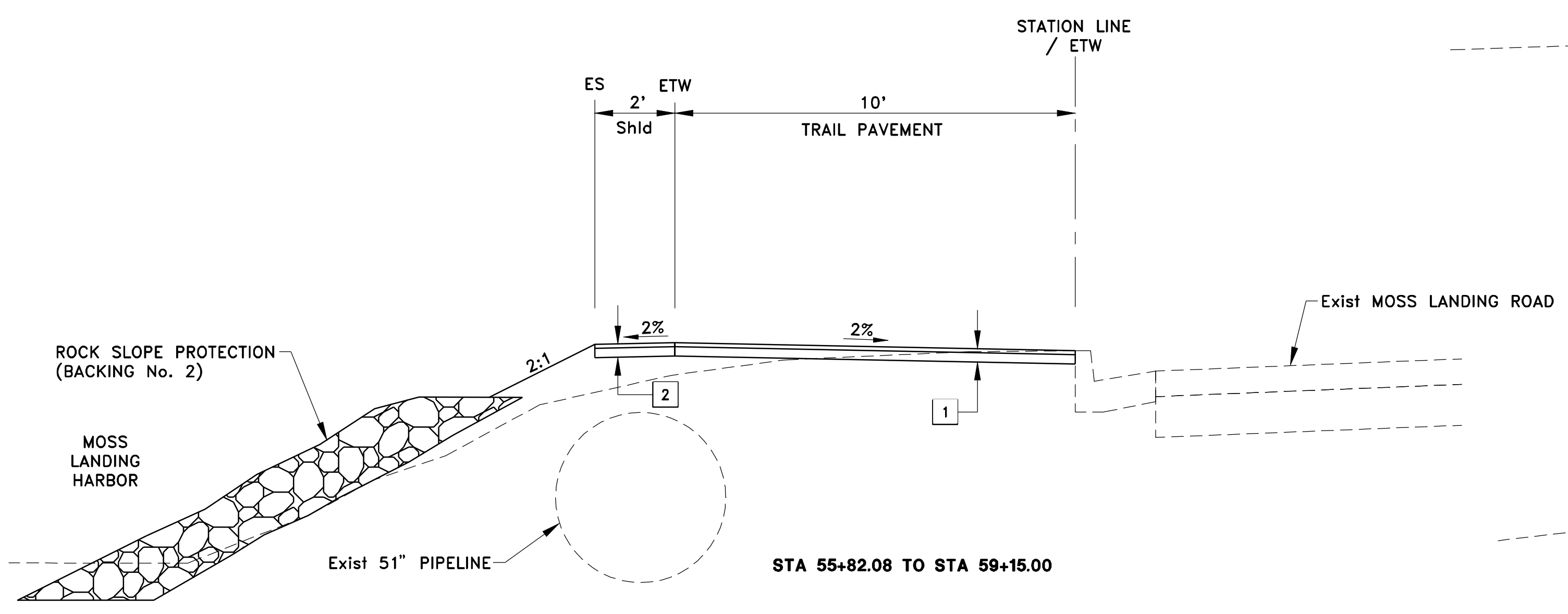
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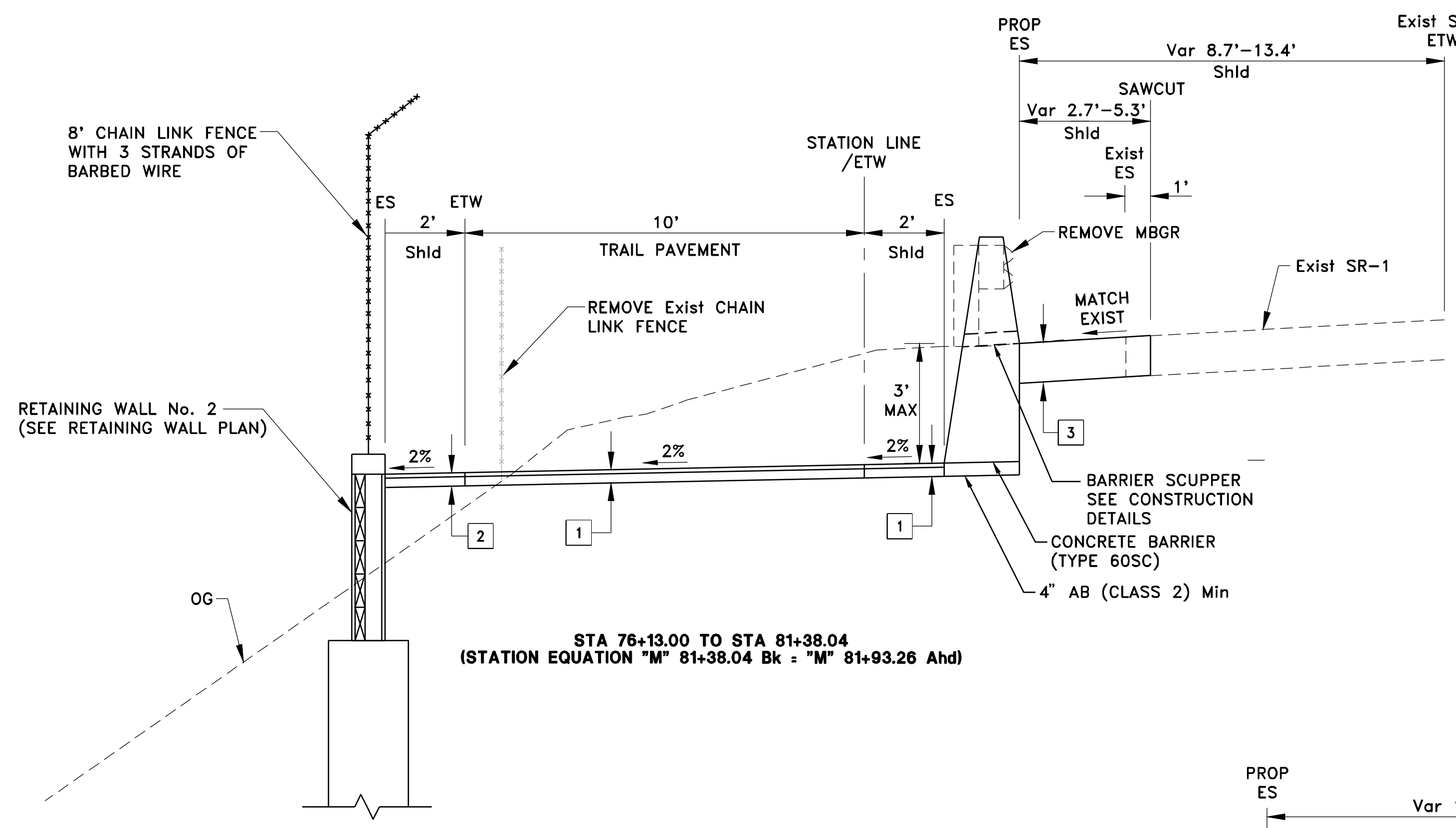
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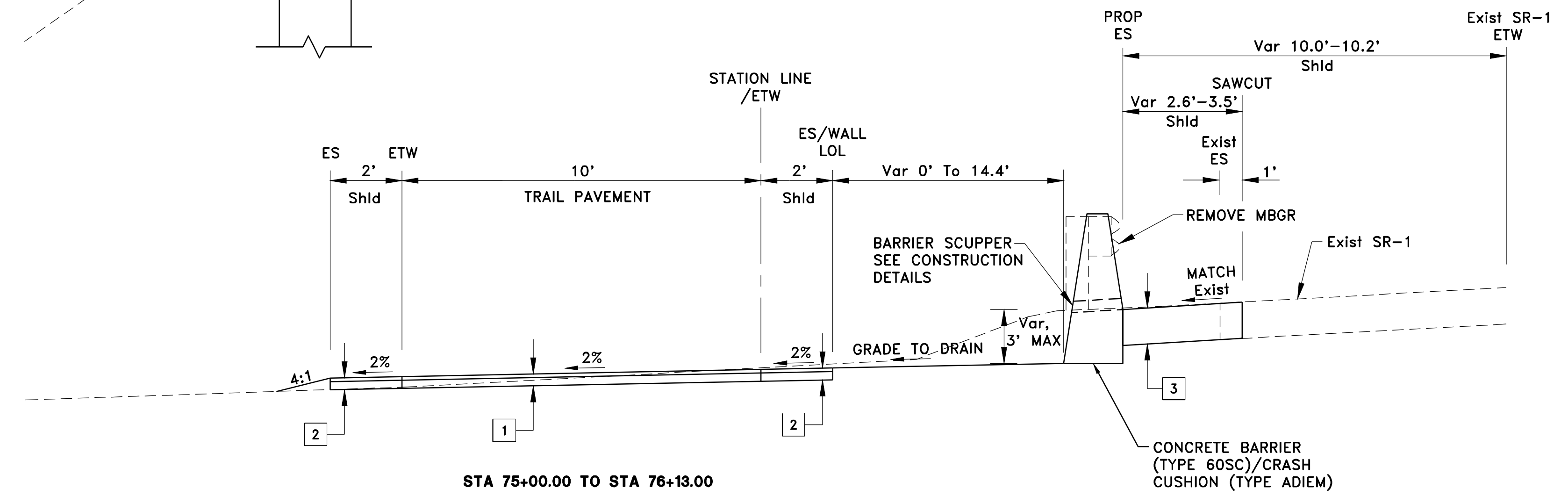
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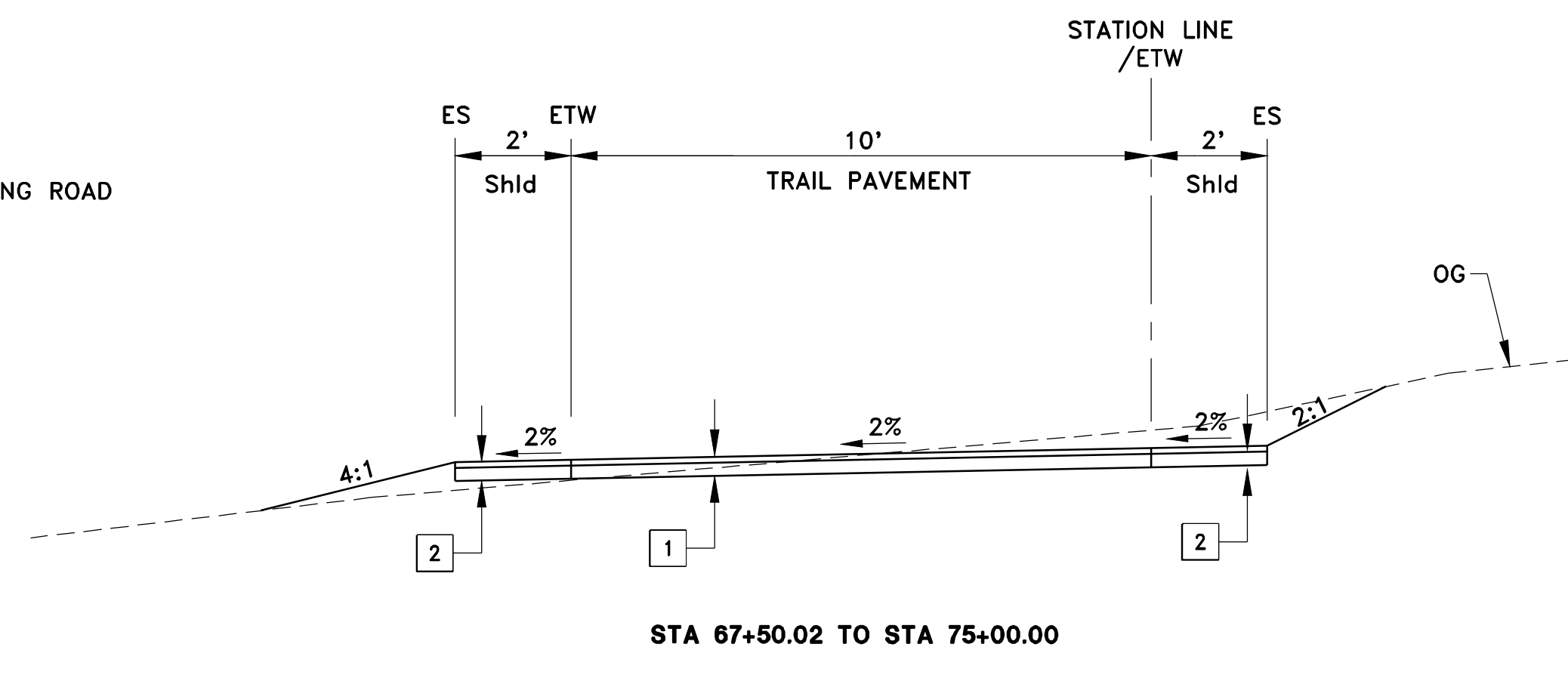
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STA 76+13.00 TO STA 81+38.04
 (STATION EQUATION "M" 81+38.04 Bk = "M" 81+93.26 Ahd)



STA 75+00.00 TO STA 76+13.00



STA 67+50.02 TO STA 75+00.00

- 1 [2" HMA (TYPE A)]
- 2 [4" AB (CLASS 2)]
- 3 [2" DECOMPOSED GRANITE]
- 4 [4" AB (CLASS 2)]
- 5 [16" HMA (TYPE A)]
- 6 [8" PCC]

SUBMITTED BY: LUCAS J. FUSON, P.E.
 DESIGN BY: LUCAS J. FUSON, P.E.
 DRAWN BY: LUCAS J. FUSON, P.E.
 SPECIFICATIONS WRITTEN BY: LUCAS J. FUSON, P.E.
 APPROVAL RECOMMENDED BY: ALI HEMMATI, P.E.



NO.	DATE	REVISION	APPROVED



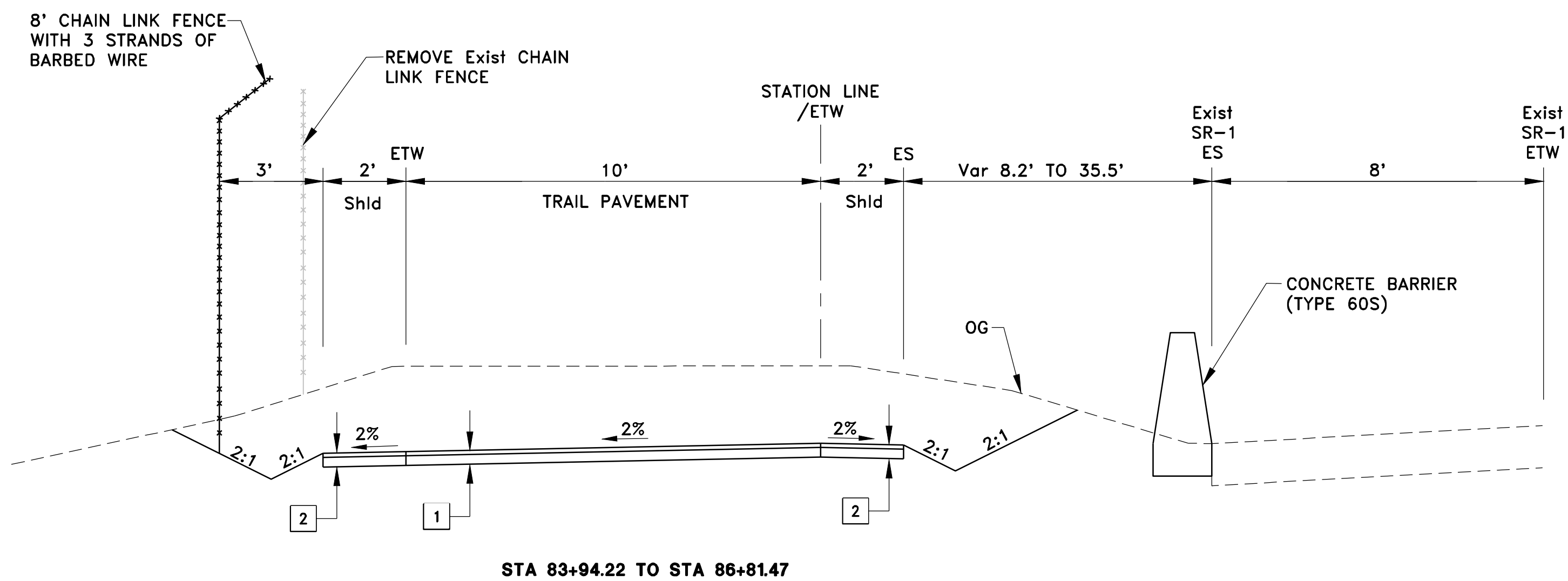
COUNTY OF MONTEREY
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 ENGINEERING SECTION
 168 WEST ALISAL STREET, 2ND FLOOR
 SALINAS, CALIFORNIA 93901-2680
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MONTEREY BAY SANCTUARY SCENIC TRAIL
 MOSS LANDING SEGMENT
 TYPICAL CROSS SECTIONS

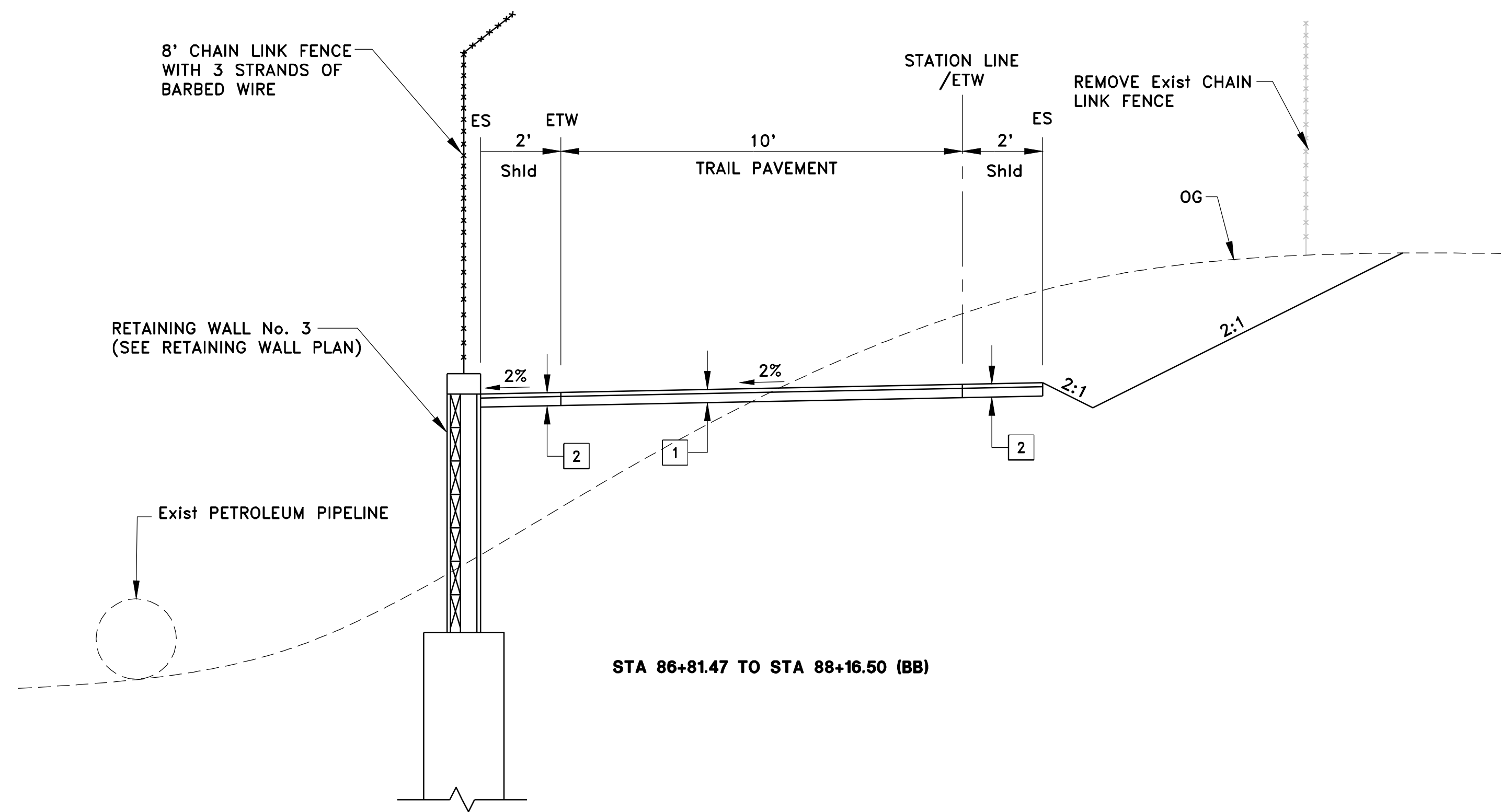
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SHEET 3 OF 57

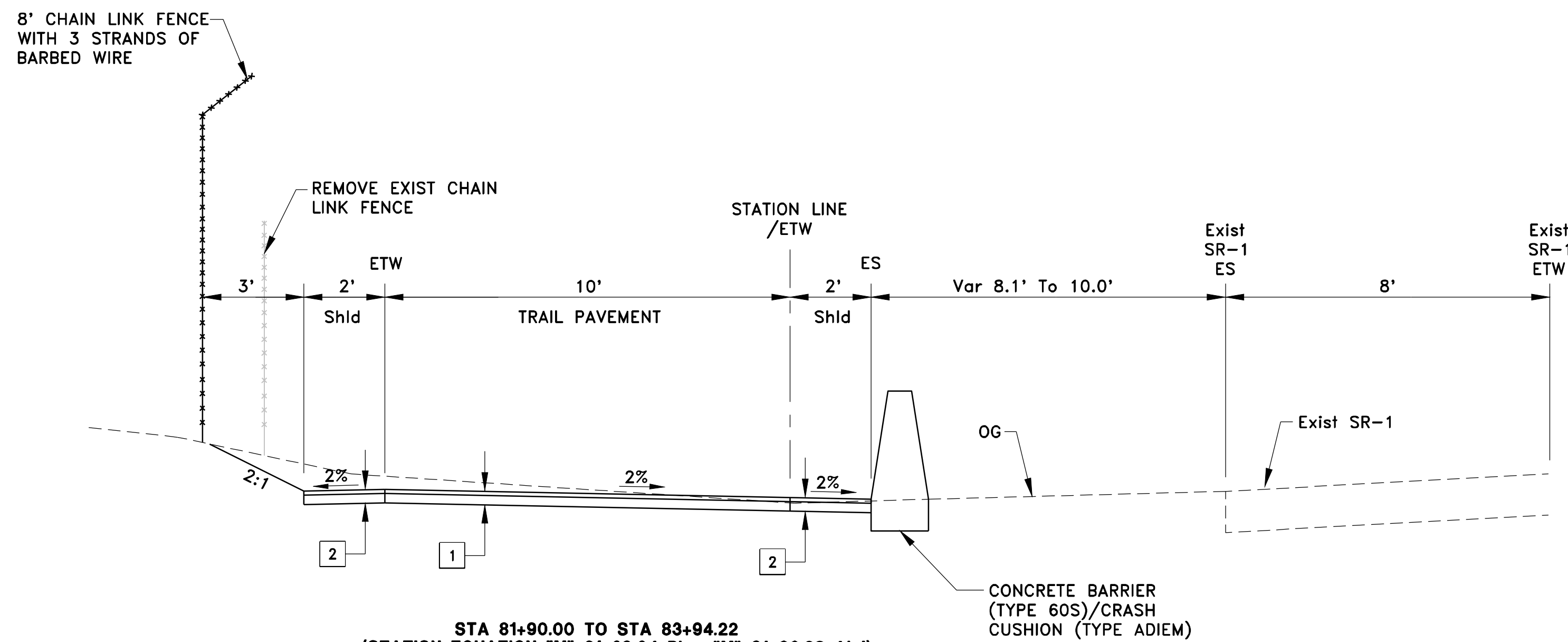
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STA 83+94.22 TO STA 86+81.47

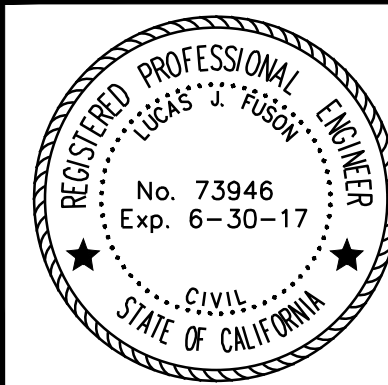


STA 86+81.47 TO STA 88+16.50 (BB)



STA 81+90.00 TO STA 83+94.22
(STATION EQUATION "M" 81+38.04 Bk = "M" 81+93.26 Ahd)

SUBMITTED BY: LUCAS J. FUSON, P.E.	
DESIGN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
DRAWN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
SPECIFICATIONS WRITTEN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH
APPROVAL RECOMMENDED BY: ALI HEMMATI, P.E.	



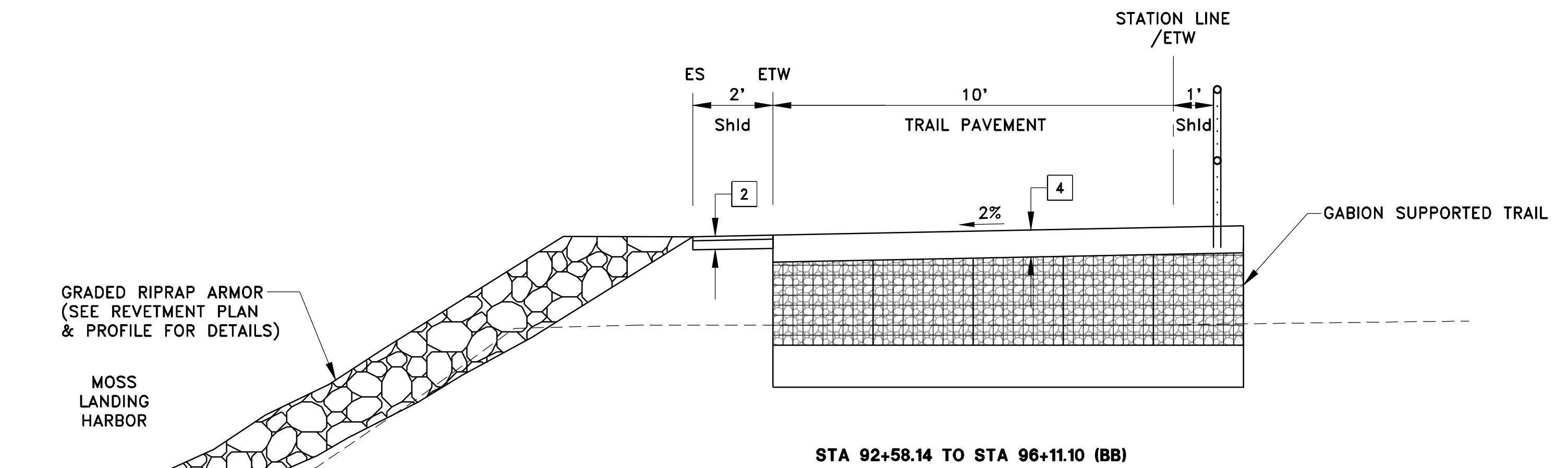
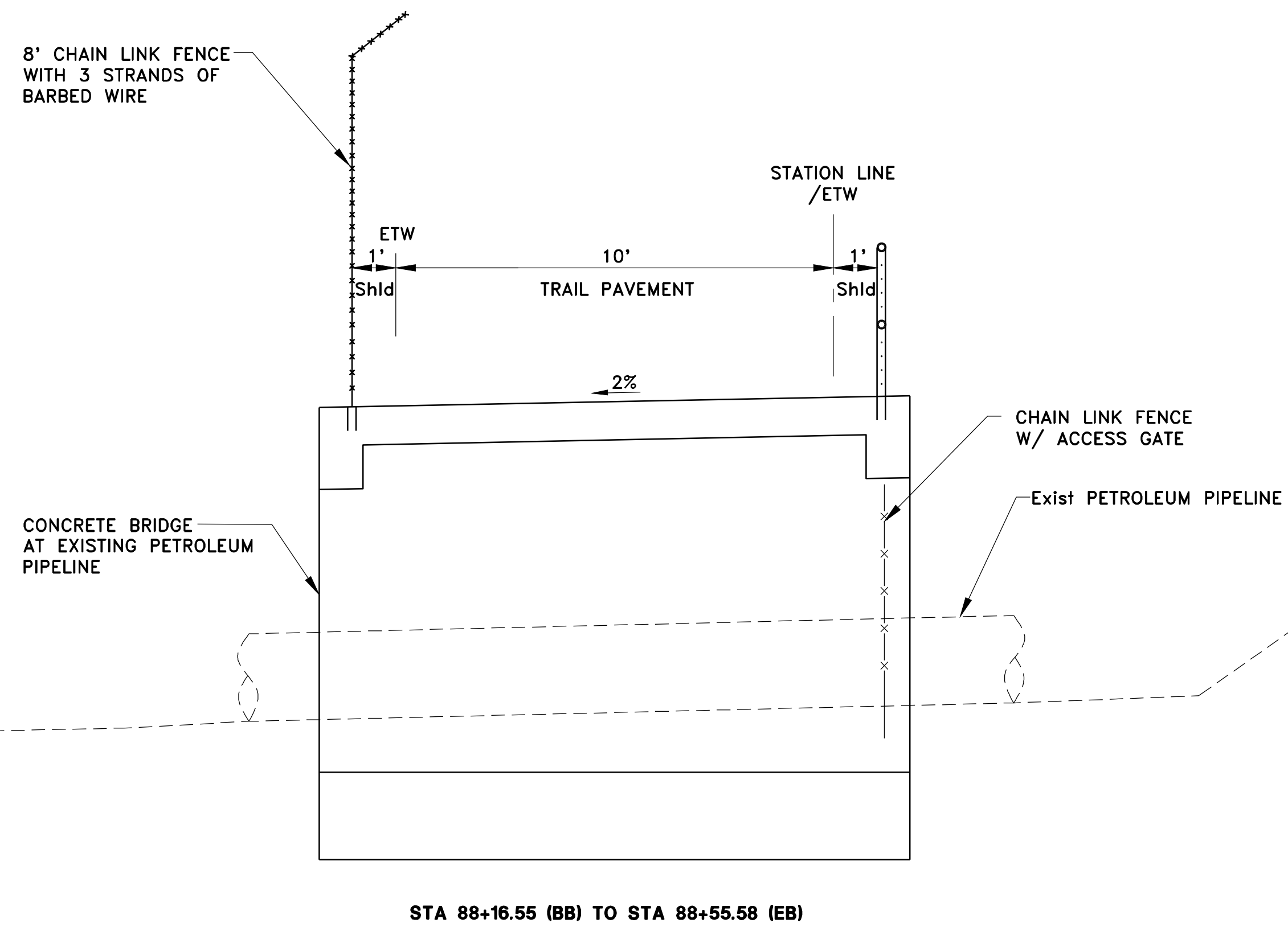
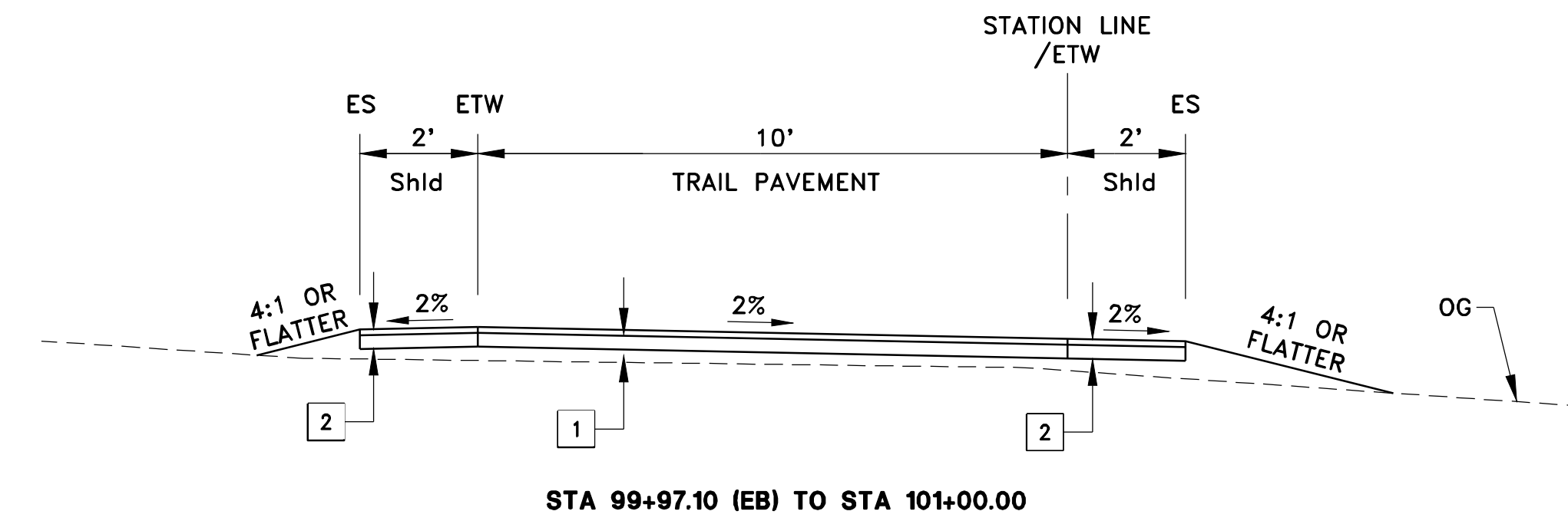
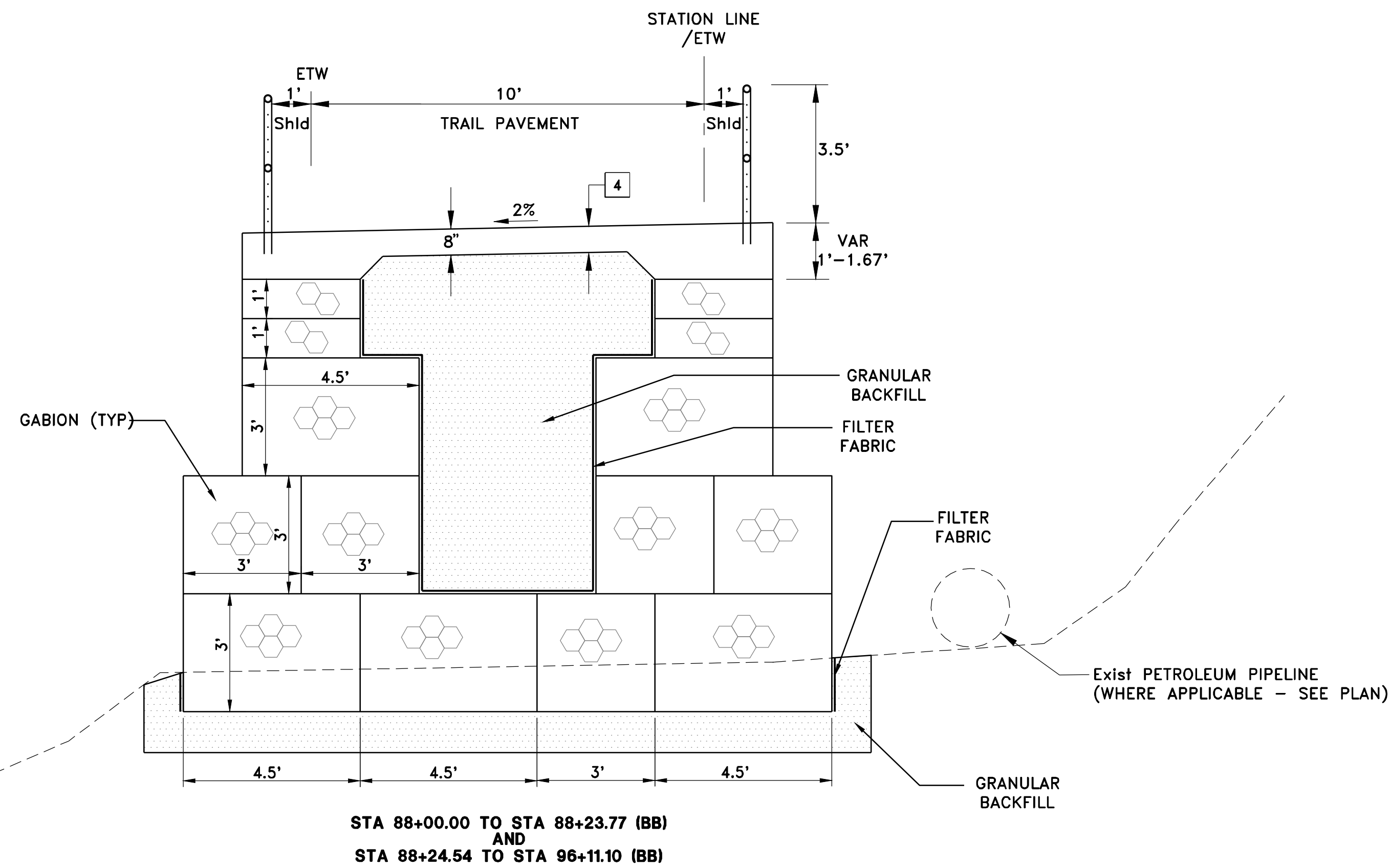
NO.	DATE	REVISION	APPROVED
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COUNTY OF MONTEREY
RESOURCE MANAGEMENT AGENCY
DEPARTMENT OF PUBLIC WORKS
ENGINEERING SECTION
168 WEST ALISAL STREET, 2ND FLOOR
SALINAS, CALIFORNIA 93901-2680
(831) 755-4800/FAX (831) 755-4958

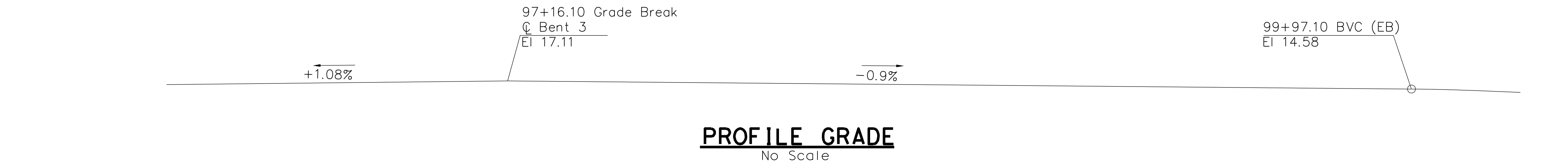
MONTEREY BAY SANCTUARY SCENIC TRAIL MOSS LANDING SEGMENT TYPICAL CROSS SECTIONS		
DATE 11/01/15	DRAWING VIEW Layout1	SHEET 4 OF 57
SCALE NTS		

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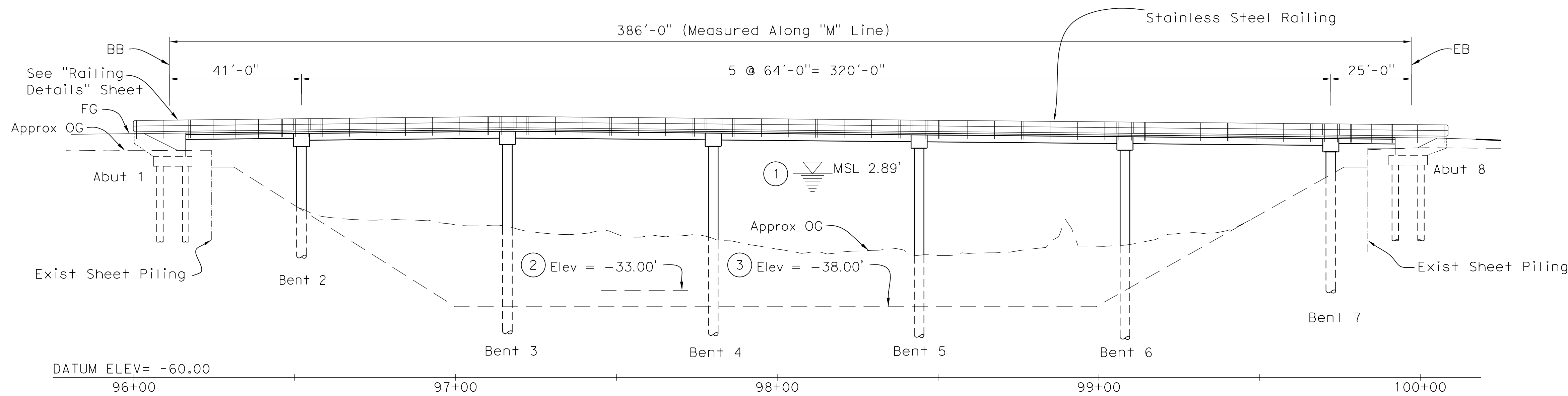


SUBMITTED BY: LUCAS J. FUSON, P.E.			NO.	DATE	REVISION	APPROVED	<p>COUNTY OF MONTEREY RESOURCE MANAGEMENT AGENCY DEPARTMENT OF PUBLIC WORKS ENGINEERING SECTION 168 WEST ALISAL STREET, 2ND FLOOR SALINAS, CALIFORNIA 93901-2680 (831) 755-4800/FAX (831) 755-4958</p>	MONTEREY BAY SANCTUARY SCENIC TRAIL MOSS LANDING SEGMENT TYPICAL CROSS SECTIONS		DATE	DRAWING	SHEET 5 OF 57
DESIGN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH		▲					11/01/15				
DRAWN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH		▲					SCALE	VIEW			
SPECIFICATIONS WRITTEN BY: LUCAS J. FUSON, P.E.	CHKD BY: AAH		▲					NTS	L0you11			
APPROVAL RECOMMENDED BY: ALI HEMMATI, P.E.			▲									

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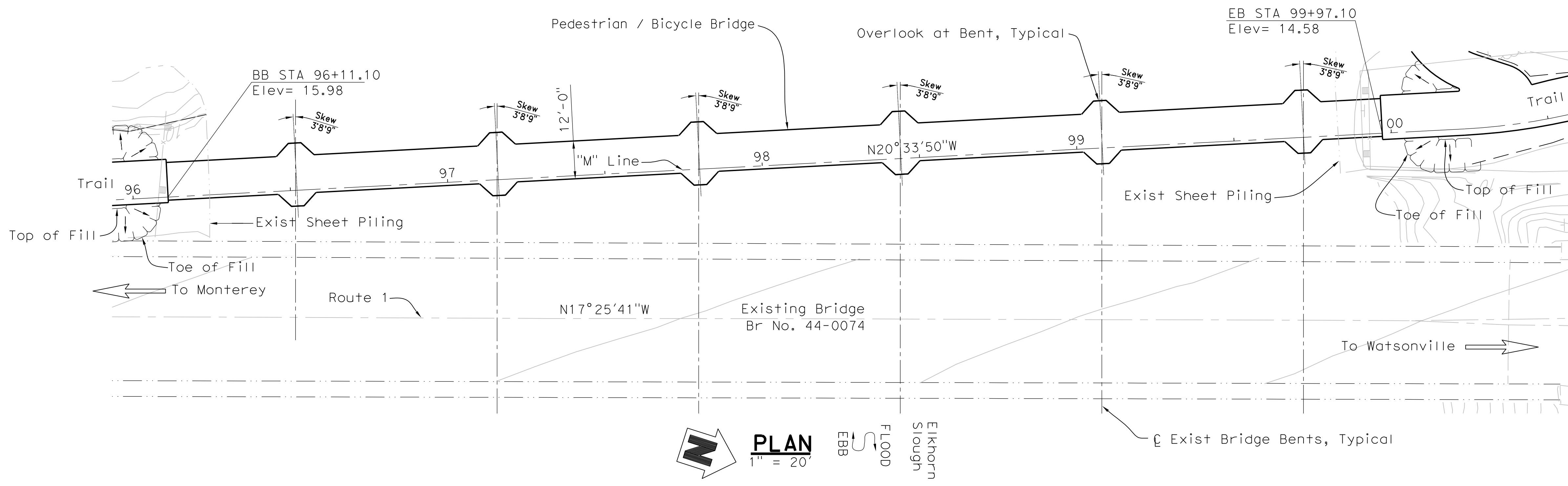


PROFILE GRADE
No Scale



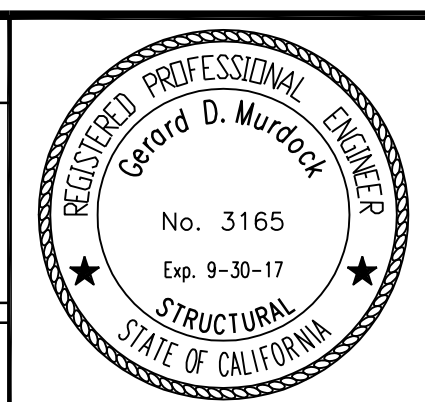
ELEVATION
1" = 20'

- NOTES:
- ① For Hydrologic Summary see "FOUNDATION PLAN" sheet.
 - ② Existing channel bottom elevation at harbor entrance.
 - ③ Channel bottom elevation at projected maximum scour depth.
- For General Notes see "INDEX TO PLANS" sheets.



PLAN
1" = 20'

SUBMITTED BY: LUKE FUSON, P.E.
 DESIGN BY: DWAYNE P. SLAVIN S.E.
 DRAWN BY: JOSE F. MENDOZA
 SPECIFICATIONS WRITTEN BY: xxx
 APPROVAL RECOMMENDED BY:



NO.	DATE	REVISION	APPROVED
▲			
▲			
▲			
▲			

COUNTY OF MONTEREY
 RESOURCE MANAGEMENT AGENCY
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING SECTION
 168 WEST ALISAL STREET
 SALINAS, CALIFORNIA 93901-2680
 (831) 755-4800/FAX (831) 755-4958

**MONTEREY BAY SANCTUARY SCENIC TRAIL
 ELKHORN SLOUGH BIKE BRIDGE**

GENERAL PLAN NO. 1

DATE 08/31/10	DRAWING B-1	SHEET 28 OF 57
SCALE	VIEW Structural	

MBSST – Moss Landing Segment
Class I Bicycle/Pedestrian Path and Bridge Project



Pedestrian traveling south on State Highway 1



Pedestrian traveling south on State Highway 1 towards Moss Landing Road
Near the southern limit of the trail



Bicyclists traveling north on State Highway 1



Bicyclist traveling north on State Highway 1



View at the southwest end of State Highway 1 Bridge crossing Elkhorn Slough



Pedestrians traveling southwest towards Moss Landing Harbor and Monterey Bay from the north end of State Highway 1 Bridge crossing Elkhorn Slough



Pedestrians traveling southwest towards Moss Landing Harbor and Monterey Bay from the north end of State Highway 1 Bridge crossing Elkhorn Slough



Bicyclists traveling north on the southwest end of State Highway 1 Bridge crossing Elkhorn Slough



Bicyclists traveling southwest towards Moss Landing Harbor and Monterey Bay from the north end of State Highway 1 Bridge near Elkhorn Slough



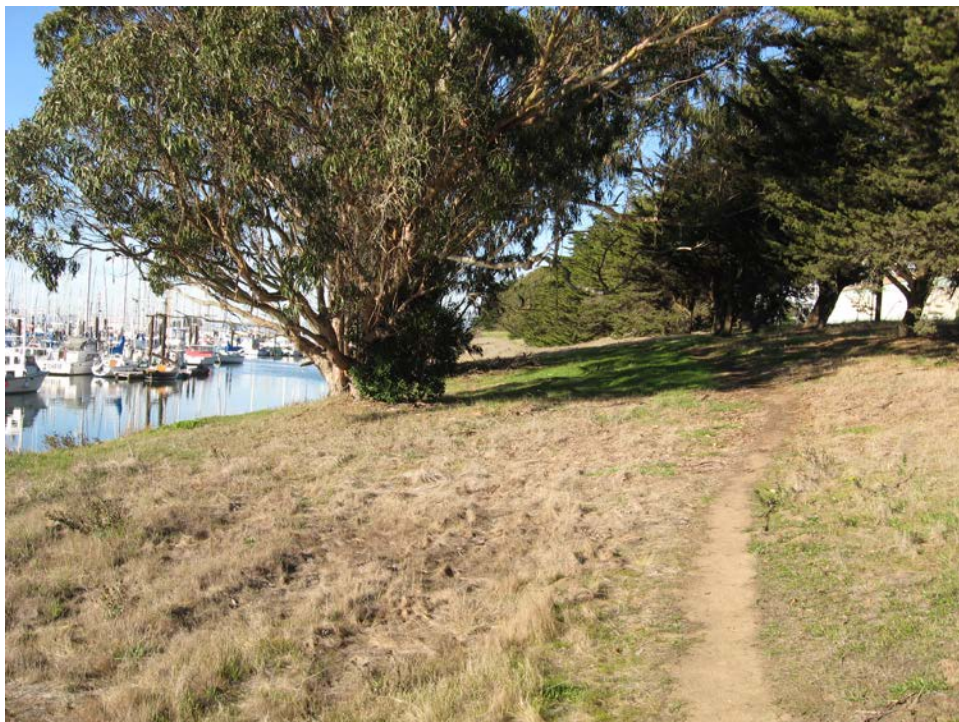
Bicyclists traveling southwest towards Moss Landing Harbor and Monterey Bay from the north end of State Highway 1 Bridge crossing Elkhorn Slough



North end of project looking south toward Moss Landing Harbor. The Elkhorn Slough Bike Bridge will span Elkhorn Slough parallel to the existing State Route 1 Bridge. The Elkhorn Slough Bike Bridge will include rest area to stop and enjoy the scenery.



Looking north from the knoll across invaded dune mat habitat towards the Elkhorn Slough Crossing.



Near the southern limit of the trail looking north at the proposed alignment.



Pedestrians traveling to the southwest towards Moss Landing Harbor and Monterey Bay from the north end of State Highway 1 Bridge crossing Elkhorn Slough



Looking south near the southern end of the alignment.



At the southern limit of the Moss Landing Segment the trail will be located between Moss Landing Harbor and Moss Landing Road.



Looking west on Moss Landing Road where the proposed trail will head west on the north side of Moss Landing Road.

Detailed Engineer's Estimate and Total Project Costs

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: Monterey County Resource Management Agency Department of Public Works	Date: 5/31/2016
Project Description: Construction of a 10' paved bicycle path with 2' wide decomposed granite shoulders on either side of the path and a 12' wide 386' long bike/ped bridge	
Project Location: North Monterey County in the community of Moss Landing extending from the North Harbor (northwest side of the existing State Highway)	
Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Ryan Chapman	
License #: C71351	

Engineer's Estimate and Cost Breakdown:

Engineer's Estimate (for Construction Items Only)							Cost Breakdown					
							ATP Eligible Costs/Items		ATP Ineligible Costs/Items		Corps/CCC to construct	
Item No.	Item	F, D or M	Quantity	Units	Unit Cost	Total Item Cost	%	\$	%	\$	%	\$
General Overhead-Related Construction Items												
1	Mobilization		1	LS	\$745,564	\$745,564	100%	\$745,564				
2	Traffic Control		1	LS	\$44,645	\$44,645	100%	\$44,645				
3	Stormwater Protection Plan		1	LS	\$25,353	\$25,353	100%	\$25,353				
4	Clearing and Grubbing (Trail)		1	LS	\$22,046	\$22,046	100%	\$22,046				
5	Erosion Control (Hydroseed) (Trail)		0.22	AC	\$8,818	\$1,940	100%	\$1,940			100%	
6	Temporary Fiber Roll (Trail)		550	LF	\$4	\$2,122	100%	\$2,122				
7							100%					
8							100%					
General Construction Items (non-decorative only)												
9	Channelizer (Surface Mounted) (Trail)		12	EA	\$33	\$397	100%	\$397				
10	Temporary Railing (Type K) with Side Reflectors (Trail)		1560	LF	\$33	\$51,588	100%	\$51,588				
11	Temporary Crash Cushion (Array "TU14") (Trail)		6	EA	\$3,307	\$19,841	100%	\$19,841				
12	Temporary Drainage Inlet Protection (Trail)		1	EA	\$276	\$276	100%	\$276				
13	Temporary Silt Fence (Trail)		3000	LF	\$3	\$9,921	100%	\$9,921				
14	Temporary Construction Entrance (Trail)		2	EA	\$5,512	\$11,023	100%	\$11,023				
15	Street Sweeping (Trail)		1	LS	\$5,512	\$5,512	100%	\$5,512				
16	Temporary Concrete Washout (Trail)		1	EA	\$1,653	\$1,653	100%	\$1,653				
17	Temporary Fence (Type ESA) (Trail)		2600	LF	\$4	\$11,464	100%	\$11,464				
18	Remove Fence (Trail)		1325	LF	\$3	\$4,382	100%	\$4,382				
19	Remove Roadside Sign (Trail)		2	EA	\$221	\$441	100%	\$441				
20	Remove Asphalt Concrete Pavement (Trail)		5500	SQFT	\$2	\$9,094	100%	\$9,094				
21	Remove Asphalt Concrete Dike (Trail)		679	LF	\$11	\$7,485	100%	\$7,485				
22	Remove Metal Beam Guard Railing (Trail)		550	LF	\$11	\$6,063	100%	\$6,063				
23	Relocate Roadside Sign (Trail)		1	EA	\$276	\$276	100%	\$276				
24	Roadway Excavation (Trail)		2500	CY	\$44	\$110,230	100%	\$110,230				
25	Structure Excavation (Rock Slope Protection) (Trail)		2548	CY	\$66	\$168,520	100%	\$168,520				
26	Ditch Excavation (Trail)		30	CY	\$33	\$992	100%	\$992				
27	Decomposed Granite (Trail)		12627	SQFT	\$3	\$41,756	100%	\$41,756				
28	Class 2 Aggregate Base (Trail)		632	CY	\$66	\$41,799	100%	\$41,799				
29	Hot Mix Asphalt (Type A) (Trail)		694	TON	\$220	\$153,000	100%	\$153,000				
30	Architectural Treatment (Barrier) (Trail)		6900	SF	\$11	\$76,059	100%	\$76,059				
31	Roadside Sign - One Post (Trail)		6	EA	\$221	\$1,323	100%	\$1,323				
32	30" Reinforced Concret Pipe (Trail)		16	LF	\$138	\$2,205	100%	\$2,205				
33	Rock Slope Protection Fabric (Class 8) (Trail)		2286	SY	\$6	\$12,599	100%	\$12,599				
34	Rock Slope Protection (1/4 T, Method B) (Trail)		2103	CY	\$110	\$231,814	100%	\$231,814				
35	Rock Slope Protection (No. 2, Method B) (Trail)		1552	CY	\$88	\$136,862	100%	\$136,862				
36	Minor Concrete (Retaining Curb) (Trail)		2	CY	\$441	\$882	100%	\$882				
37	Chain Link Fence (Type CL-8) with Barbed Wire (Trail)		1230	LF	\$39	\$47,454	100%	\$47,454				
38	Alternative Crash Cushion (Trail)		3	EA	\$22,046	\$66,138	100%	\$66,138				
39	Pedestrian Handrailing (Trail)		1926	LF	\$193	\$371,531	100%	\$371,531				
40	Concrete Barrier (Type 60S) (Trail)		376	LF	\$66	\$24,868	100%	\$24,868				
41	Concrete Barrier (Type 60SC) (Trail)		517	LF	\$99	\$51,290	100%	\$51,290				
42	Paint 4" Traffic Stripe (2-Coat) (Trail)		4558	LF	\$2	\$7,536	100%	\$7,536				
43	Bollard (Trail)		12	EA	\$386	\$4,630	100%	\$4,630				
44	24"X36"X1/2" FreeStanding Interpretive Sign Board (Graphic Printed Onto Resin) (Trail)		3	EA	\$551	\$1,653	100%	\$1,653				
45	Custom Metal Stand with Concrete footing for FreeStanding Interpretive Sign (Trail)		3	EA	\$5,512	\$16,535	100%	\$16,535				
46	24"X36"X1/2" Rail-Mounted Interpretive Sign Board (Graphic Printed Onto Resin) (Trail)		2	EA	\$551	\$1,102	100%	\$1,102				
47	Custom Metal Stand with Custom Rail Mount for Rail-Mounted Interpretive Sigh (Trail)		2	EA	\$7,165	\$14,330	100%	\$14,330				
48	6' Composite Bench with Back Rest		3	EA	\$1,653	\$4,960	100%	\$4,960				

Detailed Engineer's Estimate and Total Project Costs

Important: Read the Instructions in the first sheet (tab) before entering data. Do not enter data in shaded fields (with formulas).

Project Information:

Agency: Monterey County Resource Management Agency Department of Public Works	Date: 5/31/2016
Project Description: Construction of a 10' paved bicycle path with 2' wide decomposed granite shoulders on either side of the path and a 12' wide 386' long bike/ped bridge	
Project Location: North Monterey County in the community of Moss Landing extending from the North Harbor (northwest side of the existing State Highway)	
Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate: Ryan Chapman	
License #: C71351	

Item #	Description	Quantity	Unit	Rate	Total Cost	% Complete	Total Cost	Remaining Cost	Start Date	End Date
49	4"x4"x4' Pressure Treated Post in Concrete Footing (Trail)	4	EA	\$441	\$1,764	100%	\$1,764			
50	Gravity Keystone Wall (Retaining Wall #1)	1662	SQFT	\$28	\$45,801	100%	\$45,801			
51	Reinforced Keystone Wall (Retaining Wall #1)	750	SQFT	\$55	\$41,336	100%	\$41,336			
52	Steel Pipe/Cable Railing (Retaining Wall #1)	700	LF	\$165	\$115,742	100%	\$115,742			
53	Structure Excavation (Soldier Pile Wall) - Pier (Retaining Wall #2)	99	CY	\$121	\$12,004	100%	\$12,004			
54	Structure Excavation (Soldier Pile Wall) Wall (Retaining Wall #2)	183.6	CY	\$121	\$22,262	100%	\$22,262			
55	Structure Backfill (Soldier Pile Wall) (Retaining Wall #2)	370.3	CY	\$66	\$24,491	100%	\$24,491			
56	Concrete Piers (Retaining Wall #2)	98.6	CY	\$375	\$36,954	100%	\$36,954			
57	Steel Soldier Piles (W12X65) (Retaining Wall #2)	1185.5	LF	\$84	\$99,315	100%	\$99,315			
58	Timber Lagging (Retaining Wall #2)	1.42	MFMB	\$7,716	\$10,957	100%	\$10,957			
59	Chain Link Fence (Retaining Wall #2)	500	LF	\$33	\$16,535	100%	\$16,535			
60	Structure Excavation (Soldier Pile Wall) - Pier (Retaining Wall #3)	33.4	CY	\$121	\$4,050	100%	\$4,050			
61	Structure Excavation (Soldier Pile Wall) - Wall (Retaining Wall #3)	111.9	CY	\$121	\$13,568	100%	\$13,568			
62	Structure Backfill (Soldier Pile Wall) (Retaining Wall #3)	219.3	CY	\$66	\$14,504	100%	\$14,504			
63	Concrete Piers (Retaining Wall #3)	33.4	CY	\$375	\$12,518	100%	\$12,518			
64	Steel Soldier Piles (W12X65) (Retaining Wall #3)	397.5	LF	\$84	\$33,301	100%	\$33,301			
65	Timber Lagging (Retaining Wall #3)	0.416	MFMB	\$7,716	\$3,210	100%	\$3,210			
66	Chain Link Fence (Retaining Wall #3)	120	LF	\$33	\$3,968	100%	\$3,968			
67	Structure Excavation (Soldier Pile Wall) - Pier (Retaining Wall #4)	4.2	CY	\$121	\$509	100%	\$509			
68	Structure Excavation (Soldier Pile Wall) - Wall (Retaining Wall #4)	18.9	CY	\$121	\$2,292	100%	\$2,292			
69	Structure Backfill (Soldier Pile Wall) (Retaining Wall #4)	21	CY	\$66	\$1,389	100%	\$1,389			
70	Concrete Piers (Retaining Wall #4)	4.2	CY	\$375	\$1,574	100%	\$1,574			
71	Steel Soldier Piles (W12X65) (Retaining Wall #4)	51	LF	\$84	\$4,273	100%	\$4,273			
72	Timber Lagging (Retaining Wall #4)	0.032	MFMB	\$7,719	\$247	100%	\$247			
73	Chain Link Fence (Retaining Wall #4)	10	LF	\$33	\$331	100%	\$331			
74	Ditch Excavation (Gabion Elevated Trail)	213	CY	\$143	\$30,523	100%	\$30,523			
75	Structure Backfill (Retaining Wall) (Gabion Elevated Trail)	213	CY	\$165	\$35,219	100%	\$35,219			
76	Concrete Pavement (Gabion Elevated Trail)	143	CY	\$364	\$52,018	100%	\$52,018			
77	Gabion	951	CY	\$639	\$608,008	100%	\$608,008			
78	Filter Fabric Gabion	6590	SQFT	\$1	\$7,264	100%	\$7,264			
79	Structure Excavation (Bridge) (Pipe Undercrossing Bridge)	136.3	CY	\$110	\$15,024	100%	\$15,024			
80	Structure Backfill (Bridge) (Pipe Undercrossing Bridge)	90	CY	\$138	\$12,401	100%	\$12,401			
81	Cast-In-Place Concrete Walls and Lid (Pipe Undercrossing Bridge)	51	CY	\$601	\$30,639	100%	\$30,639			
82	Cast-In-Place Footing (Pipe Undercrossing Bridge)	17	CY	\$601	\$10,213	100%	\$10,213			
83	Joint Seal (MR 2") (Pipe Undercrossing Bridge)	39.8	LF	\$99	\$3,948	100%	\$3,948			
84	Reinforcing Steel (Pipe Undercrossing Bridge)	12200	LB	\$2	\$20,172	100%	\$20,172			
85	Steel Pipe/Cable Railing (Pipe Undercrossing Bridge)	35	LF	\$165	\$5,787	100%	\$5,787			
86	Chain Link Fence (Pipe Undercrossing Bridge)	36	LF	\$33	\$1,190	100%	\$1,190			
87	Structure Excavation (Bridge) (Elkhorn Slough)	60	CY	\$110	\$6,614	100%	\$6,614			
88	Structure Backfill (Bridge) (Elkhorn Slough Bike)	110	CY	\$138	\$15,157	100%	\$15,157			
89	Furnish 24" Cast-In-Steel Shell Concrete Piling	875	LF	\$303	\$265,242	100%	\$265,242			
90	Drive 24" Cast-In-Steel Shell Concrete Pile	12	EA	\$49,604	\$595,244	100%	\$595,244			
91	Furnish 36" Cast-In-Steel Shell Concrete Piling	1730	LF	\$441	\$762,794	100%	\$762,794			
92	Drive 36" Cast-In-Steel Shell Concrete Pile	18	EA	\$88,184	\$1,587,316	100%	\$1,587,316			
93	Structural Concrete, Bridge Footing	54	CY	\$661	\$35,715	100%	\$35,715			
94	Structural Concrete Bridge	190	CY	\$1,157	\$219,909	100%	\$219,909			

Detailed Engineer's Estimate and Total Project Costs

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Licensed Engineer in responsible charge of preparing or reviewing this PSR-Equivalent Cost Estimate:	Ryan Chapman
License #:	C71351

Item	Description	QTY	UNIT	UNIT PRICE	TOTAL PRICE	PERCENT	TOTAL PRICE	PERCENT	TOTAL PRICE
95	Furnish Precast Prestressed Concrete Slab (Type	282	SQFT	\$33	\$9,325	100%	\$9,325		
96	Furnish Precast Prestressed Concrete Slab (Type	474	SQFT	\$33	\$15,675	100%	\$15,675		
97	Furnish Precast Prestressed Concrete Slab (Type	3660	SQFT	\$44	\$161,377	100%	\$161,377		
98	Erect Precast Prestressed Concrete Voids Slab	21	EA	\$1,929	\$40,510	100%	\$40,510		
99	Joint Seal (MR 2") (Elkhorn Slough Bike Bridge)	24	LF	\$99	\$2,381	100%	\$2,381		
100	Bar Reinforcing Steel (Bridge) (Epoxy Coated)	160026	LB	\$2	\$352,794	100%	\$352,794		
101	Bar Reinforcing Steel (Epoxy Coated)	20925	LB	\$2	\$46,131	100%	\$46,131		
102	Steel Pipe/Cable Railing	772	LB	\$165	\$127,647	100%	\$127,647		
103								100%	

Decorative & Landscaping-related Items (Label items as "F" for Functional, "D" for Decorative, or "M" for a mix of Decorative and Functional)

104	Remove Tree (Trail)	F	51	EA	\$606	\$30,920	100%	\$30,920		
105	Shrubs/groundcover			SQFT				100%		
106	Irrigation / Water Connection			LS				100%		
107								100%		
108								100%		
109								100%		
110								100%		

Subtotal of Construction Items: \$8,201,200

\$410,060 <= 5% of eligible CON costs (max. decorative, if applicable)

Construction Item Contingencies (% of Construction Items): 10.00% \$820,120

Total (Construction Items & Contingencies) cost: \$9,021,320

Project Delivery Costs:

Type of Project Cost	Cost \$	ATP Eligible Costs	Non-participating Costs	"PE" costs / "CON" costs
Preliminary Engineering (PE)				
Environmental Studies and Permits(PA&ED):	\$ 794,958	\$794,958		
Plans, Specifications and Estimates (PS&E):	\$ 1,951,797	\$1,951,797		
Total PE:	\$ 2,746,755	\$2,746,755		30% 25% Max
Right of Way (RW)				
Right of Way Engineering:	\$ 154,920	\$154,920		
Acquisitions and Utilities:	\$ 149,884	\$149,884		
Total RW:	\$ 304,804	\$304,804		
Construction Engineering (CE)				
Construction Engineering (CE):	\$ 1,353,198	\$1,353,198		15% 15% Max
Total Project Delivery:	\$4,404,757	\$4,404,757		
Total Construction Costs:	\$10,374,518			
Total Project Cost:	\$13,426,077	\$13,426,077		

Documentation of Ineligible (Non-Participating) Costs:

The Engineer's logic and/or calculations for splitting costs between ATP-Eligible and Non-participating costs must be documented in this section of the Estimate form. Separate logic is required for each construction item listed above which is partly ineligible for ATP funding or is required for the construction of an ineligible item/element of the project.

Item Number(s):	Description of Engineer's Logic: (See examples shown in the Instructions)

SAM FARR
20TH DISTRICT, CALIFORNIA

COMMITTEE ON APPROPRIATIONS

SUBCOMMITTEES:

AGRICULTURE, RURAL DEVELOPMENT, FOOD AND
DRUG ADMINISTRATION, AND RELATED AGENCIES

MILITARY CONSTRUCTION, VETERANS' AFFAIRS,
AND RELATED AGENCIES

CO-CHAIR, CONGRESSIONAL ORGANIC CAUCUS

CO-CHAIR, CONGRESSIONAL TRAVEL AND
TOURISM CAUCUS

CO-CHAIR, HOUSE OCEANS CAUCUS

1126 LONGWORTH HOUSE OFFICE BUILDING
WASHINGTON, DC 20515-0520
(202) 225-2861

100 WEST ALISAL
SALINAS, CA 93901
(831) 424-2229

701 OCEAN STREET
ROOM 318
SANTA CRUZ, CA 95060
(831) 429-1976

www.farr.house.gov

Congress of the United States
House of Representatives
Washington, DC 20515-0520

March 29, 2016

CALTRANS

Division of Local Assistance, MS 1

Attn: Office of Active Transportation and Special Programs

P.O. Box 942874

Sacramento, CA 94274-0001

Re: Monterey County Request for Moss Landing Trail Segment Funding

To Whom It May Concern:

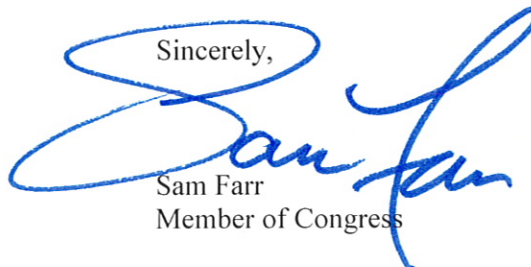
I am writing to share my strong support for the County of Monterey's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Segment Class I Bicycle/Pedestrian Path & Bridge Project. The project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

I cannot overstate my strong support for the Sanctuary Scenic Trail project. The trail expands the transportation network, enhances safety for bicyclists and pedestrians, and is an asset for recreation, education, health, eco-tourism, coastal access, economic vitality, and other visitor-services. When completed the trail will be among the top visitor attractions of the region and will be a significant addition to the Monterey Bay Area's quality of life.

Thank you for your time and consideration of this request.

Sincerely,



Sam Farr
Member of Congress

SF/aa

STATE CAPITOL
P.O. BOX 942849
SACRAMENTO, CA 94249-0030
(916) 319-2030
FAX (916) 319-2130

DISTRICT OFFICE
100 WEST ALISAL STREET, SUITE 134
SALINAS, CA 93901
(831) 759-8676
FAX (831) 759-2961

E-MAIL
Assemblymember.Alejo@assembly.ca.gov
June 3, 2016

Assembly California Legislature



LUIS A. ALEJO
ASSEMBLYMEMBER, THIRTIETH DISTRICT

COMMITTEES
CHAIR: CALIFORNIA LATINO
LEGISLATIVE CAUCUS
CHAIR: ENVIRONMENTAL SAFETY
AND TOXIC MATERIALS
LOCAL GOVERNMENT
JUDICIARY
GOVERNMENTAL ORGANIZATION
VETERANS AFFAIRS

CALTRANS

Division of Local Assistance, MS-1
Attn: Chief, Office of Active Transportation and Special Program
P.O. Box 942874
Sacramento, CA 95814

Re: ACTIVE TRANSPORTATION PROGRAM CYCLE 3 – COUNTY OF MONTEREY, MONTEREY BAY SANCTUARY SCENIC TRAIL – MOSS LANDING SEGMENT PROJECT

To Whom It May Concern:

I am writing to express support for Monterey County's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project. The project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of Highway 1. In this area of Highway 1, there is very high traffic volumes and automobiles that are traveling at highway speeds.

The MBSST – Moss Landing Project is included in the 2011 Transportation Agency for Monterey County (TAMC) Bicycle and Pedestrian Master Plan. The Master Plan states that this project represents "one of the top active transportation priorities for the community....encouraging increased use of active modes of transportation such as walking or biking".

We support the Monterey County's effort to provide a convenient and safe bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage.

In advance, I want thank you for your attention and consideration of this request. Please let me know if you have any questions or need additional information, I can be reached at (831) 759-8676 or via email at assemblymember.alejo@assembly.ca.gov

Sincerely,

LUIS A. ALEJO
Assemblymember
30th District

COMMITTEES
BANKING AND FINANCE
HUMAN SERVICES
NATURAL RESOURCES

SELECT COMMITTEES
CHAIR: COASTAL PROTECTION
CHAIR: EXPANDING ACCESS TO
CALIFORNIA'S NATURAL RESOURCES

Assembly California Legislature



MARK STONE
CHAIR, JUDICIARY
ASSEMBLYMEMBER, TWENTY-NINTH DISTRICT

STATE CAPITOL
P.O. BOX 942849
SACRAMENTO, CA 94249-0029
(916) 319-2029
FAX (916) 319-2129

DISTRICT OFFICES
701 OCEAN STREET, SUITE 318B
SANTA CRUZ, CA 95060
(831) 425-1503 or
(408) 782-0647
FAX (831) 425-2570

99 PACIFIC STREET, SUITE 575G
MONTEREY, CA 93940
(831) 649-2832
FAX (831) 649-2935

June 8, 2016

CALTRANS

Division of Local Assistance, MS-1

Attn: Chief, Office of Active Transportation and Special Program

P.O. Box 942874

Sacramento, CA 95814

**RE: Active Transportation Program Cycle 3: County of Monterey, Monterey Bay
Sanctuary Scenic Trail – Moss Landing Segment Project**

To Whom It May Concern:

I am writing to express my support of the County of Monterey's Active Transportation Program (ATP) Cycle 3 Grant Application for the *Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project*. This project is located in North Monterey County, adjacent to Highway 1 which is the main corridor connecting the Central Coast.

This project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

An ATP grant will help provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

I support the County of Monterey's effort to provide a convenient and safe bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage. Thank you for your consideration of this very worthy program. If you have any questions, please feel free to contact me at (831) 425-1503.

Sincerely,

A handwritten signature in blue ink, appearing to read "Mark Stone".

Mark Stone

Assemblymember

California State Assembly – 29th District





March 29, 2016

CALTRANS
Division of Local Assistance, MS-1
Attn: Chief, Office of Active Transportation and Special Program
P.O. Box 942874
Sacramento, CA 95814

SUBJECT: ACTIVE TRANSPORTATION PROGRAM CYCLE 3 – COUNTY OF MONTEREY, MONTEREY BAY SANCTUARY SCENIC TRAIL – MOSS LANDING SEGMENT PROJECT

To Whom It May Concern:

The State Coastal Conservancy is submitting this letter in support of the County of Monterey's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project. The project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

The MBSST – Moss Landing Project is included in the 2011 Transportation Agency for Monterey County (TAMC) *Bicycle and Pedestrian Master Plan* and represents "one of the top active transportation priorities for the community....encouraging increased use of active modes of transportation such as walking or biking."

As one of the state agencies responsible for implementing the California Coastal Trail, the Coastal Conservancy supports the County of Monterey's effort to provide a convenient and safe

1330 Broadway, 13th Floor
Oakland, California 94612-2530
510-286-1015 Fax: 510-286-0470



bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage.

Should you have any questions, please contact Rachel Couch at (805)845-8853. Thank you for your consideration.

Sincerely,

A handwritten signature in cursive script that reads "Rachel Couch". The signature is written in black ink and is positioned above the printed name and title.

Rachel Couch
Project Manager



BOARD OF HARBOR COMMISSIONERS

Russell Jeffries
Tony Leonardini
Vincent Ferrante
Andrew Amaral
Margaret "Peggy" Shirrel, Ph.D.

7881 SANDHOLDT ROAD
MOSS LANDING, CA 95039

TELEPHONE – 831.633.5417
FACSIMILE – 831.633.4537



**GENERAL MANAGER
HARBORMASTER**

Linda G. McIntyre, Esq.

June 3, 2016

CALTRANS

Division of Local Assistance, MS-1

Attn: Chief, Office of Active Transportation and Special Program

P.O. Box 942874

Sacramento, CA 95814

**SUBJECT: ACTIVE TRANSPORTATION PROGRAM CYCLE 3 – COUNTY OF
MONTEREY, MONTEREY BAY SANCTUARY SCENIC TRAIL – MOSS LANDING SEGMENT
PROJECT**

To Whom It May Concern:

The Moss Landing Harbor District supports the County of Monterey's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project. The project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

This Project is included in the 2011 Transportation Agency for Monterey County (TAMC) *Bicycle and Pedestrian Master Plan* and represents one of the top active transportation priorities for the community....encouraging increased use of walking or biking.

We support the County of Monterey's effort to provide a convenient and safe alternative along the coast of the Bay for coastal access, recreational, and commute usage.

Should you have any questions, please feel free to contact the undersigned at 831.633.5417. Thank you for your consideration.

Sincerely,

Moss Landing Harbor District

Linda G. McIntyre
General Manager



June 7, 2016

Caltrans
Division of Local Assistance, MS 1
Attn: Office of Active Transportation and Special Programs
P.O. Box 942874
Sacramento, CA 94274-0001

**SUBJECT: Active Transportation Program Cycle 3 – County of Monterey,
Monterey Bay Sanctuary Scenic Trail – Moss Landing**

To Whom It May Concern:

The Transportation Agency for Monterey County is submitting this letter in support of the County of Monterey's Active Transportation Program Grant Application for the Monterey Bay Sanctuary Scenic Trail – Moss Landing Bicycle / Pedestrian Path and Bridge Project. The project will provide a bicycle and pedestrian path and bridge in the community of Moss Landing.

An Active Transportation Program grant will provide funding for the construction of a Class I bicycle and pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

The Monterey Bay Sanctuary Scenic Trail – Moss Landing Project is included in the Transportation Agency's *Bicycle and Pedestrian Master Plan*. Our agency supports the County of Monterey's effort to provide a convenient and safe bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage.

Should you have any questions, please contact Michael Zeller, Senior Transportation Planner, of my staff at 831-775-4416. Thank you for your consideration.

Sincerely,

Debra L. Hale
Executive Director

Cc: County of Monterey, Ogarita Carranza



June 7, 2016

CALTRANS

Division of Local Assistance, MS-1

Attn: Chief, Office of Active Transportation and Special Program

P.O. Box 942874

Sacramento, CA 95814

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Executive Director

Mark Silberstein

Mailing Address

P.O. Box 267
Moss Landing
California 95039

Tel: (831) 728-5939

Fax: (831) 728-7031

www.elkhornslough.org

**SUBJECT: ATP CYCLE 3 – COUNTY OF MONTEREY, MONTEREY BAY
SANCTUARY SCENIC TRAIL – MOSS LANDING SEGMENT**

To Whom It May Concern:

The Elkhorn Slough Foundation enthusiastically supports the County of Monterey's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project. The project will provide a bicycle/pedestrian path and bridge through Moss Landing and ultimately provide a link to the entire coastal trail. This has been a long-time vision of our community.

An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds. We want to see a safe and sound access to this remarkable place.

The MBSST – Moss Landing Project is included in the 2011 Transportation Agency for Monterey County (TAMC) *Bicycle and Pedestrian Master Plan* and represents "one of the top active transportation priorities for the community....encouraging increased use of active modes of transportation such as walking or biking." This will provide very positive benefits both to the community of Moss Landing and to the citizens of California broadly as we ensure access to the shore.

We support the County of Monterey's effort to provide a convenient and safe bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage.

Please feel free to contact me if you have any questions about our support.

Sincerely,

Mark Silberstein
Executive Director



MONTEREY COUNTY



Monterey County Board of Supervisors

John M. Phillips
Supervisor District 2

Josh Stratton
Chief of Staff

Claudia J. Link
Aide to the Supervisor

Monica S. Muñoz
Administrative Assistant

P.O. Box 787
Castroville, CA 95012
831-755-5022
831-633-0201
District2@co.monterey.ca.us

June 7, 2016

CALTRANS

Division of Local Assistance, MS-1

Attn: Chief, Office of Active Transportation and Special Program

P.O. Box 942874

Sacramento, CA 95814

**SUBJECT: ACTIVE TRANSPORTATION PROGRAM CYCLE 3 – COUNTY OF MONTEREY,
MONTEREY BAY SANCTUARY SCENIC TRAIL – MOSS LANDING SEGMENT PROJECT**

To Whom It May Concern:

As the Monterey County Supervisor representing the project area, I would like to submit this letter in support of the County of Monterey's Active Transportation Program (ATP) Grant Application for the Monterey Bay Sanctuary Scenic Trail (MBSST) – Moss Landing Bicycle/Pedestrian Path and Bridge Project. The project will provide a bicycle/pedestrian path and bridge in the community of Moss Landing.

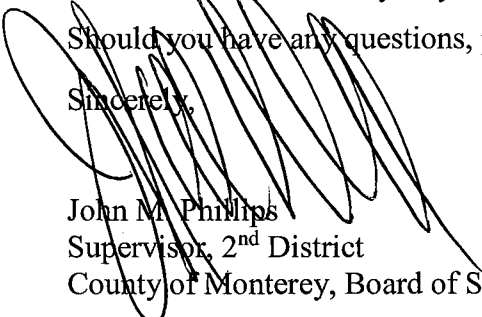
An ATP grant will provide funding for the construction of a Class I bicycle/pedestrian path from the North Harbor (northwest side of the existing State Highway 1 Bridge) to Moss Landing Road with a 386 foot bridge over the Elkhorn Slough. Currently, the only alternative for bicycle and pedestrian traffic in this area is along the shoulders of State Highway 1, which has high traffic volumes and highway speeds.

The MBSST – Moss Landing Project is included in the 2011 Transportation Agency for Monterey County (TAMC) *Bicycle and Pedestrian Master Plan* and represents "one of the top active transportation priorities for the community....encouraging increased use of active modes of transportation such as walking or biking."

I support the County of Monterey's effort to provide a convenient and safe bicycle and pedestrian facility along the coast of the Monterey Bay for coastal access, recreational, and commute usage.

Should you have any questions, please contact me at 831-755-5022. Thank you for your consideration.

Sincerely,


John M. Phillips
Supervisor, 2nd District
County of Monterey, Board of Supervisors