Exhibit A



DRAFT RESOLUTION

Before the Zoning Administrator in and for the County of Monterey, State of California

In the matter of the application of:

RICKARD OLIVER C & BROOKE T KENNAMER TRS (PLN240159) RESOLUTION NO. 25-

Resolution by the County of Monterey Zoning Administrator:

- 1) Finding that the addition to an existing single-family dwelling qualifies for a Class 1 Categorical Exemption pursuant to section 15301 of the CEQA Guidelines, and there are no exceptions pursuant to section 15300.2; and
- 2) Approving a Combined Development Permit consisting of:
 - a) Coastal Administrative Permit and Design Approval to allow construction of a 1,084 square foot addition to an existing 2,585 square foot single-family dwelling, a 190 square foot attached office to an existing guesthouse, a 118 square foot attached accessory storage structure, and associated site improvements;
 - b) After-the-fact Coastal Administrative Permit to allow construction of an access bridge;
 - c) Coastal Development Permit for the development within 750 feet of a known archaeological resource; and
 - d) Coastal Development Permit to allow development within 50 feet of a coastal bluff.

[PLN240159, Oliver and Brooke Rickard, 5 Yankee Beach Way, Carmel, Carmel Area Land Use Plan, Coastal Zone. (Assessor's Parcel Number 243-141-008-000)]

The RICKARD OLIVER C & BROOKE T KENNAMER TRS application (PLN240159) came on for a public hearing before the County of Monterey Zoning Administrator on October 9, 2025, and December 11, 2025. Having considered all the written and documentary evidence, the administrative record, the staff report, oral testimony, and other evidence presented, the Zoning Administrator finds and decides as follows:

FINDINGS

1. FINDING:

CONSISTENCY – The Project, as conditioned, is consistent with the applicable plans and policies which designate this area as appropriate for development.

EVIDENCE:

- During the course of review of this application, the project has been reviewed for consistency with the text, policies, and regulations in:
 - the 1982 Monterey County General Plan (General Plan);
 - Carmel Area Land Use Plan (CAR LUP);
 - Monterey County Coastal Implementation Plan, Part 4, Regulations for Development in the Carmel Area Land Use Plan (CIP); and
 - Monterey County Zoning Ordinance (Title 20).

No conflicts were found to exist. No communications were received during the course of review of the project indicating any inconsistencies with the text, policies, and regulations in these documents.

Allowed Use. The property is located at 5 Yankee Beach Way, Carmel, Carmel Area Land Use Plan, Coastal Zone, Assessor's Parcel Number 243-141-008-000. The parcel is split-zoned with the majority of the property being Low Density Residential with a density of 1 acre per unit with a 20 foot height limit and Design Control Overlay (Coastal Zone) or "LDR/1-D(20')(CZ)" which allows the first single-family dwelling on a legal lot of record as principally allowed use subject to a Coastal Administrative Permit in each case (Title 20 Sections 20.14.040.A). Additionally, all development with a Design Control Overlay requires the granting of a Design Approval pursuant to Title 20 Chapter 20.44. The western boundary of the parcel is zoned Resource Conservation with a 20 foot height limit and Design Control Overlay (Coastal Zone) or "RC-D(20')(CZ)", which restricts development to resource enhancement and environmental protection.

The project involves the remodel of an existing 2,585 square foot single-family dwelling and construction of 1.084 square foot addition. resulting in a 3,518 square foot single-family dwelling with an attached 118 square foot storage building. The project also involves the remodel of an existing legal non-conforming 476 square foot guesthouse by reducing the overall square footage of the structure to 387 square feet. The remaining 89 square feet of the guesthouse will be converted into a portion of a newly proposed office, attached to the guesthouse. The proposed office will have a total of 190 square feet (89 square feet converted from the guesthouse) and will have no shared internal circulation with the attached guesthouse. Associated site improvements include landscaping and an after-the-fact construction of a bridge over an existing drainage easement to access the property. All proposed development will be located within the LDR zoning district of the property. There will be no development proposed within the RC zoning portion. Therefore, the project is an allowed land use for this site.

c) <u>Lot Legality.</u> The subject parcel (0.73 acres) is identified as Lot 17 on Tract No. 181, Yankee Point Acres, filed August 1, 1949 in Volume 5 of Cities and Towns at Page 37 in the Monterey County Recorder's Office. Therefore, this property is recognized as a legal lot of record.

<u>Design and Visual Resources.</u> The property is subject to the Visual Resources protection policies of the CAR LUP due to the proximity of Highway 1, the applicable implementing regulations of those policies in the CIP, and the Design Control regulations outlined in Title 20 section 20.44.030, which requires a design review of structures and fences to assure protection of the public viewshed and neighborhood character. The residence was built with a Mid-Century Modern architectural style. The exterior colors and materials of the existing residence and accessory structures consist of brown board and batten exterior siding, orangishred wood trim, aluminum doors and windows, with a large mortared granite cobblestone decorating the exterior fireplace. The proposed additions to the property will match the colors and materials of the existing structures, and the bulk and mass of the development will be similar to other dwellings in the vicinity. Proposed exterior lighting included a brown metal wall-mounted sconce with white seeded glass. However, the project has been conditioned to include an exterior lighting plan to ensure the fixtures meet the County-approved exterior lighting guidelines (see Condition No. 6). As proposed, the finishes are consistent with other dwellings in the neighborhood and with the surrounding residential rural coastal neighborhood character, and the proposed materials help blend the development with the surrounding natural environment.

The proposed development is consistent with CAR LUP Scenic and Visual Resource Key Policy 2.2.2, which protects the scenic resources of the Carmel area in perpetuity. The project will not block significant public views and will not adversely impact the public viewshed or scenic character in the project vicinity. The project site is located off the west coast of the Pacific Ocean, facing a private cove and approximately 0.13 miles west of Highway 1. Staff conducted a site visit on August 20, 2025, to investigate possible impacts on the public viewshed. The project site is heavily vegetated and the existing development and proposed additions will not be visible from Highway 1 or Point Lobos Preserve. Therefore, the proposed additions will not impact the public viewshed.

e) Development Standards. The development standards for the LDR zoning district are identified in Title 20 section 20.14.060. Required setbacks for main dwelling units are 30 feet (front), 20 feet (rear), and 20 feet (sides). The maximum allowed height is 20 feet. The site contains an existing legal non-conforming single-family dwelling that meets the front and side setbacks, but slightly encroaches on the rear setback. All proposed additions to the main residence will be located outside of the existing setbacks, and no changes are proposed to the portion of the home that is within the rear setback. The existing residence and proposed additions will have a maximum height of 17 feet 7 inches above average natural grade, which is less than the 20 foot height limitation. The additions will include a storage structure attached to the main residence. Pursuant to Title 20 section 20.62.040.K, any accessory structure structurally attached to the main structure shall be subject to the same setback requirements as the main structure.

Therefore, the project would meet all required development standards for main structures.

Required setbacks for accessory structures are 50 feet (front), one foot (rear), and six feet on the front one-half of property; one foot on the rear one-half of the property (sides). The maximum allowed height is 15 feet. The project proposes a height of 11 feet 11 inches above average natural grade which is less than the maximum allowed. The proposed remodel and reduction in square footage to the guesthouse and newly proposed office will have setbacks that meet and exceed the minimum requirements. Therefore, the proposed project meets all required development standards for accessory structures.

The allowed site coverage maximum in the LDR zoning district is 15 percent. The property is 0.73 acres (31,695 square feet), which would allow site coverage of 4,755. The proposed additions to the existing development would result in structural coverage of 4,292 square feet or 13.5 percent. Therefore, the project meets the site coverage and development standards outlined in Title 20.

Biological Resources. Pursuant to policies of the CAR LUP and section 20.146.040 of the CIP, a biological report (Monterey County Document No. LIB250018) was prepared for the project site. On September 6, 2024, a biological assessment was conducted by qualified biologist Rob Thompson due to the project's proximity to environmentally sensitive habitat areas. The site was previously developed and landscaped with native and non-native trees and vegetation. The notable resources onsite contain coastal bluff scrub to the west, with planted Monterey cypress and Monterey pine trees spread throughout the site. A seasonal drainage with a presently dry culvert is located along the front of the property. To access the site, a bridge was previously constructed to pass over this drainage channel. This bridge failed and was replaced without the benefit of a discretionary permit. This drainage channel is largely comprised of non-native invasive vegetation and does not support any sensitive or protected flora or fauna. Accordingly, no impacts to sensitive biological resources likely occurred during construction of the replacement bridge. Based on the Biologist's site visit and due to the developed and previously disturbed condition of the lot, the Biologist confirmed that the subject parcel does not support federally and/or stateprotected special status species, sensitive habitat and/or environmentally sensitive habitat areas (ESHA), and none of these resources will be affected by proposed property development activities. There are no known occurrences of special status species, sensitive habitat or other protected resources occurring on the property and none were observed during the site visit. Additionally, actively nesting birds were not observed on or adjacent to the subject property during the site inspection. However, a condition of approval has been added to ensure compliance with the Migratory Bird Treaty Act should construction occur during the nesting season (Condition No. 7). Additionally, the project has been conditioned to include the installation of tree root protection prior to construction (Condition No. 8). Therefore, the

- proposed project will not impact biological resources. See finding No. 2, Evidence "b".
- g) Prior Work. Pursuant to Title 20 Chapter 20.79, an Emergency Permit may be issued without the normally required public hearing processes to rectify an emergency situation. The applicant applied for an Emergency Permit in March 2024 to repair a collapsed bridge, which is the only access to a single-family dwelling (HCD-Planning File No. PLN240087). Staff requested project plans from the applicant in order to process said permit. However, on March 25, 2025, the project agent submitted a letter requesting to withdraw that application as the replacement bridge had already been built, and include that work in the proposed Combined Development Permit (PLN240159).
- <u>Cultural Resources.</u> The property is in an area that is mapped as having a high sensitivity to the presence of archaeological resources. Pursuant to CIP section 20.146.090, a Phase I and II Archaeological Report (Monterey County Document No. LIB240287) was prepared for the property, which assessed the potential of the project area to contain archaeological resources. The report concluded that there were four previously recorded sites within 750 feet of the project site. As such, a Coastal Development Permit is required pursuant to CIP section 20.146.090. The Phase I report identified positive indicators on the site but outside of the project area, and therefore, a Phase II report was submitted with auger borehole testing in five locations for the proposed additions. The testing results were negative in the areas to be developed. The results of the Phase I assessment and Phase II subsurface testing program indicate that no significant resources will be impacted by the proposed project as planned. Therefore, potential impacts on archaeological resources are limited, and a standard condition of approval requiring work to stop if previously unidentified resources are found during construction has been incorporated into this permit (Condition No. 3). See Finding No. 6, Evidence "c".
- Historical Resources. Pursuant to the California Environmental Quality Act (CEOA), Public Resources Code section 21084.1(a), all properties fifty years of age or older must be reviewed for potential historic significance. A Phase I Historical Resource Assessment Study (Monterey County Document No. LIB240290) was submitted with the project application. On January 4, 2024, Dana Supernowicz analyzed the potential historical significance of the existing one-story singlefamily dwelling. The property was developed in 1953 with a Mid-Century Modern style wood-framed one-story single-family dwelling by the notable architectural firm Anshen and Allen. The residence is diamond-shaped with the pointed ends oriented east to west, with the west side cantilevered atop a bluff overlooking Yankee Point and the Pacific Ocean. The Historical Report concluded that the subject property does not possess any historical significance, and therefore, is not eligible for listing on any national, state, or local Historic Resources registry.
- j) <u>Fuel Management.</u> The subject property is identified as being in a very high fire hazardous area as mapped by the Monterey County Geographic Information System. As demonstrated in Finding 3,

- evidence "c", the project, as proposed and conditioned, will be consistent with all the fuel management requirements for such areas.
- k) Development within 50 Feet of a Coastal Bluff. Title 20 section 20.70.120.B.1, requires a Coastal Development Permit for improvements to any structure within 50 feet of a coastal bluff edge because they involve risk of environmental impact. The project, as proposed and conditioned, is consistent with applicable policies of the CAR LUP regarding protection of resources. See Finding No. 4 and supporting evidence.
- l) <u>Public Access.</u> As proposed and conditioned, the development is consistent with applicable public access policies of the CAR LUP. See Finding No. 7 and supporting evidence.
- m) Land Use Advisory Committee (LUAC) Review. Based on the LUAC procedure guidelines adopted by the Monterey County Board of Supervisors Resolution No. 15-103, the project was referred to the Carmel Highlands LUAC for review on June 2, 2025. One concern from the neighbor was raised regarding the potential for the proposed additions to block her views of the ocean. The architect was present and addressed her concerns and no design changes were recommended. The LUAC reviewed the project and recommended approval as proposed by a vote of 4-0 with one member absent.
- n) The project planner conducted a site inspection on August 20, 2025 to verify that the project on the subject parcel conforms to the plans listed above.
- o) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.
- 2. **FINDING:** SITE SUITABILITY The site is physically suitable for the proposed development and/or use.
 - a) The project has been reviewed for site suitability by the following departments and agencies: HCD-Planning, HCD-Engineering Services, HCD-Environmental Services, Environmental Health Bureau, and Carmel Highlands Fire Protection District (FPD). County staff reviewed the application materials and plans to verify that the project on the subject site conforms to the applicable plans and regulations, and there has been no indication from these departments/agencies that the site is not suitable for the development. Conditions recommended have been incorporated.
 - b) Staff identified potential impacts to archaeological, biological, historical, geological and soil resources. The following reports have been prepared:
 - "Phase I & Phase II Archaeological Assessment Report" (LIB240287) prepared by Susan Morley and Brenna Wheelis with Achasta Archaeological Services, Marina, CA, January 2025.
 - "Geotechnical Investigation Report" (LIB240289) prepared by Lawrence Grice with Grice Engineering Inc., Salinas, CA, August 30, 2024.

EVIDENCE:

- "Phase I Historical Resource Assessment Report" (LIB240290) prepared by Dana Supernowicz with Historic Resources Associates, Pebble Beach, CA, January 4, 2024.
- "Biological Assessment Report" (LIB250018) prepared by Rob Thompson with Thompson Wildland Management, Monterey, CA, September 21, 2023.
- "Geological Report" (LIB250098) prepared by Robert Barminski with CapRock Geology, Inc., Aromas, CA, February 27, 2025, including Addendum to original report submitted on October 22, 2025.
- c) County staff independently reviewed these reports and concurs with their conclusions. There are no physical or environmental constraints that would indicate that the site is not suitable for the use. All development shall be in accordance with these reports.
- d) Staff conducted a site inspection on August 20, 2025 to verify that the site is suitable for this use.
- e) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.

3. FINDING:

HEALTH AND SAFETY – The establishment, maintenance, or operation of the project applied for will not under the circumstances of this particular case be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood of such proposed use, or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the County.

EVIDENCE:

- a) The project was reviewed by HCD-Planning, HCD-Engineering Services, HCD-Environmental Services, Environmental Health Bureau, and Carmel Highlands FPD. The respective agencies have recommended conditions, where appropriate, to ensure that the project will not have an adverse effect on the health, safety, and welfare of persons either residing or working in the neighborhood.
- b) Necessary public facilities exist on the subject property. Potable water to serve the development is provided by the Cal-Am public water system. Wastewater is treated by two Onsite Wastewater Treatment Systems (OWTS), which independently serve the guesthouse and main residence. A performance evaluation for the guesthouse remodel was not required since the addition did not exceed 500 square feet. However, an evaluation was required for the additions to the main dwelling. On June 20, 2024, Hastings Construction submitted a performance evaluation of the existing 2,000-gallon septic system. The Environmental Health Bureau reviewed the evaluation and found the OWTS had the capacity to serve the proposed development, and that the system was observed to be in acceptable condition.
- c) The property is located in an area mapped as having a very high fire hazard according to the Monterey County Geographic Information System. Pursuant to CAR LUP Policy 2.7.3.1, new development shall be sited and designed to minimize risk from fire hazards. The proposed additions to the existing single-family dwelling are sited in previously

developed areas and the materials used are fire-resistant. The project site has existing landscape and overgrown vegetation. The project has been conditioned to require a Landscape and Fuel Management Plan to address fire hazards by including vegetation fuel management requirements from 0-30 feet from the home, and from 30-100 feet from the home (Condition No. 10). Therefore, the project, as conditioned, will be consistent with Policy 2.7.3.1.

- d) Staff conducted a site inspection on August 20, 2025 to verify that the project will not have an adverse effect on the health, safety, and welfare of persons either residing or working in the neighborhood.
- e) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.

4. FINDING:

DEVELOPMENT WITHIN 50 FEET OF A COASTAL BLUFF -

There is no alternative location to position the proposed development and the location of the proposed development better achieves the goals, policies, and objectives of the applicable land use plan than other development alternatives.

EVIDENCE: a)

- The project is located within 50 feet of a coastal bluff according to Monterey County GIS information. As identified in Title 20 section 20.70.120.A.1, a Coastal Development Permit is required for any development within 50 feet of a coastal bluff. Additionally, pursuant to CAR LUP Policy 2.7.4.3, a geological hazards report is required for such development.
- Conditions of approval and changes in the development are required as deemed necessary to ensure compliance with Title 20, Section 20.64.230.E.2, and to ensure the stability of the development. During the construction permit phase, the project would be required to comply with Monterey County Code Chapter 16.12, Erosion Control, which outlines the required provisions for preparing erosion control plans, implementing runoff control measures, conducting land clearing, and managing winter operations. Additionally, it establishes procedures for administering these provisions to minimize erosion during construction. Additionally, consistent with Title 20 section 20.64.230.E.1, the contractor is required to comply with applicable building code requirements (including those pertaining to health, life, and safety) and resource protection measures such as erosion control plan review and approval, grading plan review and approval, inspections by Environmental Services staff, and geotechnical plan review and certification. In summary, overall site development would be subject to current regulations regarding control of erosion and drainage, and the applicant is required to address post-construction requirements and runoff reduction.
- d) Shoreline Erosion, Bluff Retreat, and Clearance of Structures. The subject property is on the seaward edge of a coastal bluff off Highway 1 on Yankee Point. The steep coastal bluff slopes toward the Pacific Ocean and is approximately 70 feet above sea level. Per the geotechnical and geological report prepared for the project by CapRock Geology, Inc. (Monterey County Document No. LIB250098),

development of the project site would neither create a geologic hazard nor diminish the area's stability. The report determined that the site is underlain with granitic bedrock, the coastal bluff is stable, the historical bluff recession rate is less than moderate, and excavation for the addition would not adversely impact or undermine the coastal bluff. Per the geological report, review of aerial photographs spanning 1949 through 2020 indicates there has been a less than moderate discernible retreat in the bluff edge and terrain seaward of the existing development in that period (approximately 7.1 feet over 71 years, or about 0.1 feet per year).

The high end of the average annual long-term bluff edge recession rates over that period suggests that approximately 10 feet of recession could occur at the subject property in the next 100 years. The geological report established a recommended setback of 10 feet has been applied to the project, to which all proposed additions to the main house are located outside this setback. The existing single-family dwelling was constructed with a cantilevered terrace overhanging the coastal bluff. The terrace frame is structurally attached to the foundation of the main residence, and the foundation is also outside of the setback. However, as part of this project, a retrofit of the cantilevered terrace is planned to ensure the adequate safety of the existing development. The repair work consists of replacing the rusted and failing beams supporting the cantilever. The geologic hazards report concluded that the site was suitable for the proposed development, given that the recommendations in it are followed. The notice of report condition has been applied to ensure that the geotechnical engineer's recommendations are adhered to (Condition No. 9). Additionally, in accordance with CAR LUP Hazards Policy 2.7.3.4, the project has been conditioned to require the owner to record a deed restriction on the property describing the nature of the properties hazards (Coastal Hazards, including but not limited to waves, storms, flooding, landslide, bluff erosion, and earth movement, many of which will worsen with future sea level rise) and long-term maintenance requirements (Condition No. 11).

- e) As designed and located, the project would comply with applicable policies of the CAR LUP Chapter 2.7, Hazardous Areas. Specifically, consistent with LUP Policy 2.7.4.7, the prepared geotechnical and geological reports demonstrate that the site would be stable for development. As designed, it is anticipated that the proposed addition to the existing single-family dwelling would not be subject to failure over the course of its economic life span.
- f) The application, plans, and supporting materials submitted by the project applicant to the County of Monterey HCD-Planning for the proposed development are found in project file PLN240159.
- **5. FINDING: VIOLATIONS** The subject property is not in compliance with all rules and regulations pertaining to zoning uses, subdivision, and any other applicable provisions of the County's zoning ordinance. Violations exist on the property.
 - **EVIDENCE:** a) Staff reviewed Monterey County HCD-Planning and HCD-Building Services records and is not aware of any active code enforcement cases

- on the subject property. However, as described in Finding No. 1, Evidence "g", the Applicant/Owner replaced a failed bridge without obtaining a required Emergency Permit or Coastal Development Permit. Issuance of this permit, and obtaining after-the-fact construction permits for the permanent bridge, will abate the violation.
- b) Staff conducted a site inspection on August 20, 2025 and researched County records to assess if any violation exists on the subject property.
- The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.
- 6. FINDING:

CEQA (Exempt) – The project is categorically exempt from environmental review and no unusual circumstances were identified to exist for the proposed project.

EVIDENCE:

- a) California Environmental Quality Act (CEQA) Guidelines section 15301(e)(1) for a Class 1 categorical exemption allows for a single-family dwelling addition and the permitting of existing facilities. Therefore, the proposed development is consistent with CEQA Guidelines section 15301.
- b) The project meets the criteria of this exemption because it's a remodel and addition to an existing single-family dwelling and detached non-habitable accessory structure. The expansion will not add any new residential units, will not intensify the water use (or wastewater generation), and does not substantially alter the floor area ratio over 50 percent of the existing development, making it a negligible expansion of existing use. In addition, the existing bridge is an existing structure that is not proposed to be modified under this permit. Therefore, the project was found to meet the qualifications for a Class 1 Categorical Exemption pursuant to CEQA Guidelines section 15301.
- None of the exceptions under CEOA Guidelines section 15300.2 apply to this project. The project does not involve a designated historical resource, a hazardous waste site, development located near or within view of a scenic highway, and does not cause any unusual circumstances that would result in a significant effect or development that would result in a cumulative significant impact. On August 19, 2024, a Phase I pedestrian survey report was submitted with the application (see Finding No. 2, Evidence "b") since the project is located in a high archaeological sensitivity area. The results of that survey were positive for sparse marine shell fragments on the western coastal bluff of the property. On January 2, 2025, a Phase II subsurface test was conducted and resulted in negative findings for precolonial and historical site indicators. Therefore, the project, as designed and conditioned, was found to have no impacts on archaeological resources. However, a standard condition of approval has been incorporated to ensure construction work is halted if archaeological resources are accidentally uncovered (Condition No. 3).
- d) No adverse environmental effects were identified during staff review of the development application during a site visit on August 20, 2025.

- e) See supporting Finding Nos. 1 and 2. The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.
- 7. FINDING: PUBLIC ACCESS The project is in conformance with the public access and recreation policies of the Coastal Act (specifically Chapter 3 of the Coastal Act of 1976, commencing with Section 30200 of the Public Resources Code) and applicable Local Coastal Program, and does not interfere with any form of historic public use or trust rights.
 - **EVIDENCE:** a) No public access is required as part of the project as no substantial adverse impact on access, either individually or cumulatively, as described in Section 20.146.130 of the Monterey County Coastal Implementation Plan can be demonstrated.
 - b) No evidence or documentation has been submitted or found showing the existence of historic public use or trust rights over this property.
 - The subject property is not described as an area where the Local Coastal Program requires visual or physical public access (Figure 3, Visual Resources, and Figure 8, Major Public Access and Recreational Facilities, in the CAR LUP).
 - d) The application, project plans, and related support materials submitted by the project applicant to Monterey County HCD-Planning can be found in Project File PLN240159.
- **8. FINDING: APPEALABILITY** The decision on this project may be appealed to the Board of Supervisors and to the California Coastal Commission.
 - EVIDENCE: a) Board of Supervisors. Section 20.86.030 of the Monterey County Zoning Ordinance (Title 20). An appeal may be made to the Board of Supervisors by any public agency or person aggrieved by a decision of an Appropriate Authority other than the Board of Supervisors.
 - b) <u>Coastal Commission.</u> Pursuant to Section 20.86.080.A of the Monterey County Zoning Ordinance (Title 20), the project is subject to appeal by/to the California Coastal Commission because it involves development between the sea and the first through public road paralleling the sea and located within three hundred (300) feet of the top of the seaward face of a coastal bluff.

DECISION

NOW, THEREFORE, based on the above findings and evidence, the County of Monterey Zoning Administrator does hereby:

- 1. Find that the addition to an existing single-family dwelling qualifies for a Class 1 Categorical Exemption pursuant to section 15301 of the CEQA Guidelines, and there are no exceptions pursuant to section 15300.2; and
- 2. Approve the Combined Development Permit consisting of:
 - a) Coastal Administrative Permit and Design Approval to allow construction of a 1,084 square foot addition to an existing 2,585 square foot single-family dwelling, a 190 square foot attached office to an existing guesthouse, a 118 square foot attached accessory storage structure, and associated site improvements;
 - b) After-the-fact Coastal Administrative Permit to allow construction of an existing access bridge;

- c) Coastal Development Permit for the development within 750 feet of a known archaeological resource; and
- d) Coastal Development Permit to allow development within 50 feet of a coastal bluff.

All of which are in general conformance with the attached sketch and subject to the attached conditions, all being attached hereto and incorporated herein by reference.

PASSED AND ADOPTED this 11th day of December 2025.

Mike Novo, AICP
Zoning Administrator

COPY OF THIS DECISION MAILED TO APPLICANT ON DATE

THIS APPLICATION IS APPEALABLE TO THE BOARD OF SUPERVISORS. IF ANYONE WISHES TO APPEAL THIS DECISION, AN APPEAL FORM MUST BE COMPLETED AND SUBMITTED TO THE CLERK TO THE BOARD ALONG WITH THE APPROPRIATE FILING FEE ON OR BEFORE DATE.

THIS PROJECT IS LOCATED IN THE COASTAL ZONE AND IS APPEALABLE TO THE COASTAL COMMISSION. UPON RECEIPT OF NOTIFICATION OF THE FINAL LOCAL ACTION NOTICE (FLAN) STATING THE DECISION BY THE FINAL DECISION MAKING BODY, THE COMMISSION ESTABLISHES A 10 WORKING DAY APPEAL PERIOD. AN APPEAL FORM MUST BE FILED WITH THE COASTAL COMMISSION. FOR FURTHER INFORMATION, CONTACT THE COASTAL COMMISSION AT (831) 427-4863 OR AT 725 FRONT STREET, SUITE 300, SANTA CRUZ, CA.

This decision, if this is the final administrative decision, is subject to judicial review pursuant to California Code of Civil Procedure Sections 1094.5 and 1094.6. Any Petition for Writ of Mandate must be filed with the Court no later than the 90th day following the date on which this decision becomes final.

NOTES

1. You will need a building permit and must comply with the Monterey County Building Ordinance in every respect.

Additionally, the Zoning Ordinance provides that no building permit shall be issued, nor any use conducted, otherwise than in accordance with the conditions and terms of the permit granted or until ten days after the mailing of notice of the granting of the permit by the appropriate authority, or after granting of the permit by the Board of Supervisors in the event of appeal.

Do not start any construction or occupy any building until you have obtained the necessary permits and use clearances from Monterey County HCD-Planning and HCD-Building Services Department office in Salinas.

2. This permit expires 3 years after the above date of granting thereof unless construction or use is started within this period.

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County of Monterey HCD Planning

DRAFT Conditions of Approval/Implementation Plan/Mitigation Monitoring and Reporting Plan

PLN240159

1. PD001 - SPECIFIC USES ONLY

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

This Combined Development Permit (PLN240159) allows a Coastal Administrative Permit and Design Approval to allow construction of a 1,084 square foot addition to an existing 2,585 square foot single-family dwelling, a 190 square foot attached office to an existing guesthouse, a 118 square foot attached accessory storage structure, and associated site improvements; After-the-fact Coastal Administrative Permit to allow construction of an access bridge; Coastal Development Permit for the development within 750 feet of a known archaeological resource; and Coastal Development Permit to allow development within 50 feet of a coastal bluff. The property is located at 5 Yankee Beach Way, Carmel (Assessor's Parcel Number 243-141-008-000), Carmel Area Land Use Plan, Coastal Zone. This permit was approved in accordance with County ordinances and land use regulations subject to the terms and conditions described in the project file. Neither the uses nor the construction allowed by this permit shall commence unless and until all of the conditions of this permit are met to the satisfaction of the Director of HCD - Planning. Any use or construction not in substantial conformance with the terms and conditions of this permit is a violation of County regulations and may result in modification or revocation of this permit and subsequent legal action. No use or construction other than that specified by this permit is allowed unless additional permits are approved by the appropriate authorities. To the extent that the County has delegated any condition compliance or mitigation monitoring to the Monterey County Water Resources Agency, the Water Resources Agency shall provide all information requested by the County and the County shall bear ultimate responsibility to ensure that conditions and mitigation measures are properly fulfilled. (HCD - Planning)

Compliance or Monitoring Action to be Performed:

The Owner/Applicant shall adhere to conditions and uses specified in the permit on an on-going basis unless otherwise stated.

Print Date: 12/4/2025 2:20:30PM Page 1 of 10

2. PD002 - NOTICE PERMIT APPROVAL

Responsible Department: Planning

Condition/Mitigation Monitoring Measure:

The applicant shall record a Permit Approval Notice. This notice shall state:

"A Combined Development Permit (Resolution Number ______) was approved by the Zoning Administrator for Assessor's Parcel Number 243-141-008-000 on December 11, 2025. The permit was granted subject to 12 conditions of approval which run with the land. A copy of the permit is on file with Monterey County HCD - Planning."

Proof of recordation of this notice shall be furnished to the Director of HCD - Planning prior to issuance of grading and building permits, Certificates of Compliance, or commencement of use, whichever occurs first and as applicable. (HCD - Planning)

Compliance or Monitoring Action to be Performed:

Prior to the issuance of grading and building permits, certificates of compliance, or commencement of use, whichever occurs first and as applicable, the Owner/Applicant shall provide proof of recordation of this notice to the HCD - Planning.

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3. PD003(B) - CULTURAL RESOURCES POSITIVE ARCHAEOLOGICAL REPORT

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

If archaeological resources or human remains are accidentally discovered during construction, the following steps will be taken:

There shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent human remains until the coroner of the county in which the remain are discovered must be contacted to determine that no investigation of the cause of death is required.

If the coroner determines the remains to be Native American:

- The coroner shall contact the Native American Heritage Commission and HCD Planning within 24 hours.
- The Native American Heritage Commission shall identify the person or persons from a recognized local tribe of the Esselen, Salinan, Costonoans/Ohlone and Chumash tribal groups, as appropriate, to be the most likely descendant.
- The most likely descendant may make recommendations to the landowner or the person responsible for the excavation work, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in Public Resources Code Section 5097.9 and 5097.993, Or

Where the following conditions occur, the landowner or his authorized representatives shall rebury the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance:

- 1. The Native American Heritage Commission is unable to identify a most likely descendant or the most likely descendant failed to make a recommendation within 24 hours after being notified by the commission.
- 2. The descendant identified fails to make a recommendation; or
- 3. The landowner or his authorized representative rejects the recommendation of the descendant, and the mediation by the Native American Heritage Commission fails to provide measures acceptable to the landowner.

 (HCD Planning)

Compliance or Monitoring Action to be Performed:

Prior to the issuance of grading or building permits or approval of Subdivision Improvement Plans, whichever occurs first, the Owner/Applicant, per the archaeologist, shall submit the contract with a Registered Professional Archaeologist for on-call archaeological services should resources be discovered during construction activities. Submit the letter to the Director of the HCD – Planning for approval.

Prior to the issuance of grading or building permits and/or prior to the recordation of the final/parcel map, whichever occurs first, the Owner/Applicant shall include requirements of this condition as a note on all grading and building plans, on the Subdivision Improvement Plans, in the CC&Rs, and shall be included as a note on an additional sheet of the final/parcel map.

Prior to Final, the Owner/Applicant, per the Archaeologist , shall submit a report or letter from the archaeologist summarizing their methods, findings, and recommendations if their services are needed during construction or if no resources were found.

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Print Date: 12/4/2025 2:20:30PM

4. PW0044 - CONSTRUCTION MANAGEMENT PLAN

Responsible Department:

Public Works

Condition/Mitigation Monitoring Measure:

The applicant shall submit a Construction Management Plan (CMP) to HCD-Planning and HCD-Engineering Services for review and approval. The CMP shall include measures to minimize traffic impacts during the construction/grading phase of the project.

CMP shall include, at a minimum, duration of the construction, hours of operation, truck routes, estimated number of truck trips that will be generated, number of construction workers, and on-site/off-site parking areas for equipment and workers and locations of truck staging areas. Approved measures included in the CMP shall be implemented by the applicant during the construction/grading phase of the project. (Public Works)

Compliance or Monitoring Action to be Performed:

- 1. Prior to issuance of the Grading Permit or Building Permit, Owner/Applicant/Contractor shall prepare a CMP and shall submit the CMP to the HCD-Planning and HCD- Engineering Services for review and approval.
- 2. On-going through construction phases Owner/Applicant/Contractor shall implement the approved measures during the construction/grading phase of the project.

5. PD006(A) - CONDITION COMPLIANCE FEE

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

The Owner/Applicant shall pay the Condition Compliance fee, as set forth in the fee schedule adopted by the Board of Supervisors, for the staff time required to satisfy conditions of approval. The fee in effect at the time of payment shall be paid prior to clearing any conditions of approval.

Compliance or Monitoring Action to be Performed:

Prior to clearance of conditions, the Owner/Applicant shall pay the Condition Compliance fee, as set forth in the fee schedule adopted by the Board of Supervisors.

PLN240159

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6. PD014(A) - LIGHTING - EXTERIOR LIGHTING PLAN

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

All exterior lighting shall be unobtrusive, down-lit, harmonious with the local area, and constructed or located so that only the intended area is illuminated and off-site glare is fully controlled. The lighting source shall be shielded and recessed into the fixture. The applicant shall submit three (3) copies of an exterior lighting plan which shall indicate the location, type, and wattage of all light fixtures and include catalog sheets for each fixture. The lighting shall comply with the requirements of the California Energy Code set forth in California Code of Regulations Title 24 Part 6. The exterior lighting plan shall be subject to approval by the Director of HCD - Planning, prior to the issuance of building permits.

(HCD - Planning)

Compliance or Monitoring Action to be Performed:

Prior to the issuance of building permits, the Owner/Applicant shall submit three copies of the lighting plans to HCD - Planning for review and approval. Approved lighting plans shall be incorporated into final building plans.

Prior to final/occupancy, the Owner/Applicant/Contractor shall submit written and photographic evidence demonstrating that the lighting has been installed according to the approved plan.

On an on-going basis, the Owner/Applicant shall ensure that the lighting is installed and maintained in accordance with the approved plan.

7. PD050 - RAPTOR/MIGRATORY BIRD NESTING

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

Any tree removal activity that occurs during the typical bird nesting season (February 22-August 1), the County of Monterey shall require that the project applicant retain a County qualified biologist to perform a nest survey in order to determine if any active raptor or migratory bird nests occur within the project site or within 300 feet of proposed tree removal activity. During the typical nesting season, the survey shall be conducted no more than 30 days prior to ground disturbance or tree removal. If nesting birds are found on the project site, an appropriate buffer plan shall be established by the project biologist. (HCD - Planning)

Compliance or Monitoring Action to be Performed:

No more than 30 days prior to ground disturbance or tree removal, the Owner/Applicant/Tree Removal Contractor shall submit to HCD -Planning a nest survey prepare by a County qualified biologist to determine if any active raptor or migratory bird nests occur within the project site or immediate vicinity.

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8. PD011 - TREE AND ROOT PROTECTION

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

Trees which are located close to construction site(s) shall be protected from inadvertent damage from construction equipment by fencing off the canopy driplines and/or critical root zones (whichever is greater) with protective materials, wrapping trunks with protective materials, avoiding fill of any type against the base of the trunks and avoiding an increase in soil depth at the feeding zone or drip-line of the retained trees. Said protection, approved by certified arborist, shall be demonstrated prior to issuance of building permits subject to the approval of HCD - Director of Planning. If there is any potential for damage, all work must stop in the area and a report, with mitigation measures, shall be submitted by certified arborist. Should any additional trees not included in this permit be harmed, during grading or construction activities, in such a way where removal is required, the owner/applicant shall obtain required permits. (HCD - Planning)

Compliance or Monitoring Action to be Performed: Prior to issuance of grading and/or building permits, the Owner/Applicant shall submit evidence of tree protection to HCD - Planning for review and approval.

During construction, the Owner/Applicant/Arborist shall submit on-going evidence that tree protection measures are in place through out grading and construction phases. If damage is possible, submit an interim report prepared by a certified arborist.

Prior to final inspection, the Owner/Applicant shall submit photos of the trees on the property to HCD-Planning after construction to document that tree protection has been successful or if follow-up remediation or additional permits are required.

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9. PD016 - NOTICE OF REPORT

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

Prior to issuance of building or grading permits, a notice shall be recorded with the Monterey County Recorder which states:

"The following reports have been prepared for the proposed project:

A "Phase I & Phase II Archaeological Assessment Report" (LIB240287) prepared by Susan Morley and Brenna Wheelis with Achasta Archaeological Services, Marina, CA, January 2025;

A "Geotechnical Investigation Report" (LIB240289) prepared by Lawrence Grice with Grice Engineering Inc., Salinas, CA, August 30, 2024;

A "Phase I Historical Resource Assessment Report" (LIB240290) prepared by Dana Supernowicz with Historic Resources Associates, Pebble Beach, CA, January 4, 2024;

A "Biological Assessment Report" (LIB250018) prepared by Rob Thompson with Thompson Wildland Management, Monterey, CA, September 21, 2023; and

A "Geological Report" (LIB250098) prepared by Robert Barminski with CapRock Geology, Inc., Aromas, CA, February 27, 2025, including Addendum to original report submitted on October 22, 2025, and is on file in Monterey County HCD - Planning. All development shall be in accordance with this report."

(HCD - Planning)

Compliance or Monitoring Action to be Performed:

Prior to the issuance of grading and building permits, the Owner/Applicant shall submit proof of recordation of this notice to HCD - Planning.

Prior to occupancy, the Owner/Applicant shall submit proof, for review and approval, that all development has been implemented in accordance with the report to the HCD - Planning.

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10. PDSP01 - LANDSCAPE PLAN AND FUEL MANAGEMENT PLAN (NON-STANDARD)

Responsible Department:

Planning

Condition/Mitigation Monitoring Measure:

The site shall be landscaped and shall incorporate fuel management. Prior to the issuance of building permits, a landscaping plan and fuel management plan shall be submitted to the Chief of HCD - Planning. A landscape plan review fee is required for this project. Fees shall be paid at the time of landscape plan submittal. The landscaping plan shall be in sufficient detail to identify the location, species, and size of the proposed landscaping materials and shall include an irrigation plan.

The fuel management plan shall comprise of a site plan illustrating how vegetation around proposed structures and roadways will be maintained to reduce fire fuel loads. The Fuel Management Plan shall include a "Green Zone" (0-30 feet) - Illustration on plans for maintaining vegetation (native and proposed) within 30 feet of all proposed structures/facilities, a "Management Zone" (30+ feet) - Illustration on plans showing maintenance of vegetation up to 100 feet from all proposed structures, or to the property line, whichever is closer, Tree Pruning/Removal and Emergency Vehicle Access. In very high fire hazard severity zones, a greater distance may be required by the local Fire Authority.

Before occupancy, landscaping shall be either installed or a certificate of deposit or other form of surety made payable to Monterey County for a cost estimate shall be submitted to the Monterey County HCD - Planning. All landscaped areas and fences shall be continuously maintained by the applicant; all plant material shall be continuously maintained in a litter-free, weed-free, healthy, growing condition. (HCD - Planning)

Compliance or Monitoring Action to be Performed:

Prior to issuance of building permits, the Owner/Applicant/Licensed Landscape Contractor/Licensed Landscape Architect shall submit a landscape plan and fuel management plan to HCD - Planning for review and approval. All landscape plans shall be signed and stamped by licensed professional under the following statement, "I certify that this landscaping and irrigation plan complies with all Monterey County landscaping requirements including use of native, drought-tolerant, non-invasive species; limited turf; and low-flow, water conserving irrigation fixtures."

Prior to occupancy, the Owner/Applicant/Licensed Landscape Contractor/Licensed Landscape Architect shall ensure that the landscaping shall be either installed or a certificate of deposit or other form of surety made payable to Monterey County for that cost estimate shall be submitted to the Monterey County HCD - Planning.

On an on-going basis, all landscaped areas and fences shall be continuously maintained by the Owner/Applicant; all plant material shall be continuously maintained in a litter-free, weed-free, healthy, growing condition.

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11. PDSP001 - COASTAL HAZARDS DEED RESTRICTION

Responsible Department: Planning

Condition/Mitigation Monitoring Measure:

"In accordance with Carmel Area Land Use Plan Hazards Policy 2.7.3.4, the owner shall record a deed restriction on the property describing the nature of the properties hazards (Coastal Hazards, including but not limited to waves, storms, flooding, landslide, bluff erosion, and earth movement, many of which will worsen with future sea level rise) and long-term maintenance requirements. The deed restriction shall say the following:

- "a. General Provisions. This deed restriction is being recorded to satisfy Condition No. 11 of the Combined Development Permit Approval (PLN240159), approved by the County of Monterey Zoning Administrator Resolution No. 25-. By accepting this permit, the property owner has accepted the following conditions and restrictions, which shall run with the land:
- b. Coastal Hazards. That the site is subject to coastal hazards, including but not limited to waves, storms, flooding, landslide, bluff erosion, and earth movement, many of which will worsen with future sea level rise.
- c. Assume Risks. To assume all risks to the Permittee and the properties that are the subject of this permit of injury and damage from such hazards in connection with this permitted development.
- d. Liability Waiver. To unconditionally waive any claim of damage or liability against the California Coastal Commission & the County of Monterey, and their officers, agents, and employees for injury or damage from such hazards.
- e. Indemnification. To indemnify and hold harmless the California Coastal Commission & the County of Monterey, and their officers, agents, and employees with respect to the County's approval of the project against any and all liability, claims, demands, damages, costs (including costs and fees incurred in defense of such claims), expenses, and amounts paid in settlement arising from any injury or damage due to such hazards. This indemnification obligation is in addition to, and cumulative of, the indemnification obligation imposed by the County of Monterey Zoning Administrator Resolution No. 25-, Condition No. 11. Property owner understands and agrees that both indemnification obligations shall be memorialized in the indemnification agreement.
- f. Permittee Responsible. That any adverse effects to property caused by the permitted project shall be fully the responsibility of the Permittee.
- g. Shoreline Armoring Prohibited. That no shoreline armoring shall ever be constructed to protect the development approved pursuant to this CDP, including in the event that the development is threatened with damage or destruction from coastal hazards in the future.
- h. Waiver of Rights to Construct Armoring. The Permittee hereby waives, on behalf of itself and all successors and assigns, any rights to construct such armoring that may exist under applicable law." (HCD-Planning)

Print Date: 12/4/2025 2:20:30PM Page 9 of 10 Compliance or Monitoring Action to be Performed: Prior to issuance of construction permits, owner/applicant shall record the deed restriction, and provide HCD-Planning with evidence that it has been recorded. Such evidence shall be in the form of a copy of the recorded document with the recorders seal.

12. CC01 INDEMNIFICATION

Responsible Department:

County Counsel-Risk Management

Condition/Mitigation Monitoring Measure: Owner/Applicant agrees as a condition and in consideration of approval of this discretionary development permit that it will, pursuant to agreement and/or statutory provisions as applicable, including but not limited to Government Code section 66474.9, defend, indemnify, and hold harmless the County of Monterey and/or its agents, officers, and/or employees from any claim, action, or proceeding against the County and/or its agents, officers, and/or or employees to attack, set aside, void, or annul this approval and/or related subsequent approvals, including, but not limited to, design approvals, which action is brought within the time provided for under law. Owner/Applicant shall reimburse the County for any court costs and attorney's fees that the County may be required by a court to pay as a result of such action.

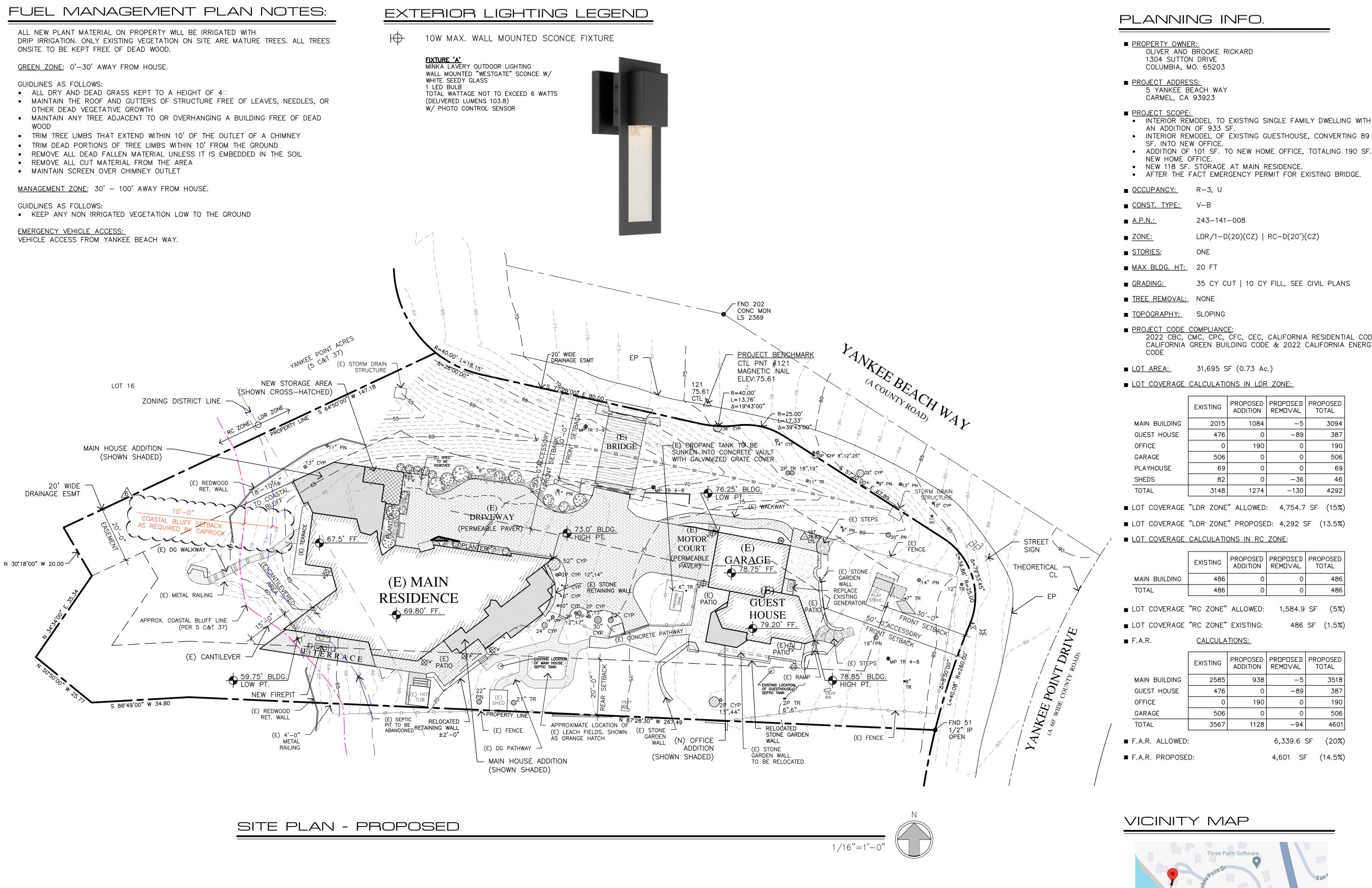
The County shall notify Owner/Applicant of any such claim, action, and/or proceeding as expeditiously as possible. The County may, at its sole discretion, participate in the defense of such action. However, such participation shall not relieve Owner/Applicant of his/her/its obligations under this condition. Regardless, the County shall cooperate fully in defense of the claim, action, and/or proceeding.

(County Counsel-Risk Management)

Compliance or Monitoring Action to be Performed:

This Indemnification Obligation binds Owner/Applicant from the date of approval of this discretionary development permit forward. Regardless, on written demand of the County Counsel's Office, Owner/Applicant shall also execute and cause to be notarized an agreement to this effect. The County Counsel's Office shall send Owner/Applicant an indemnification agreement. Owner/Applicant shall submit such signed and notarized Indemnification Agreement to the Office of the County Counsel for County's review and signature. Owner/Applicant shall then record such indemnification agreement with the County of Monterey Recorder's Office. Owner/Applicant shall be responsible for all costs required to comply with this paragraph including, but not limited to, notary costs and Recorder fees.

Print Date: 12/4/2025 2:20:30PM Page 10 of 10



PLANNING INFO

■ PROPERTY OWNER: OLIVER AND BROOKE RICKARD 1304 SUTTON DRIVE COLUMBIA, MO. 65203

> ■ PROJECT ADDRESS: 5 YANKEE BEACH WAY CARMEL, CA 93923

 INTERIOR REMODEL TO EXISTING SINGLE FAMILY DWELLING WITH AN ADDITION OF 933 SF.

SF. INTO NEW OFFICE. ADDITION OF 101 SF. TO NEW HOME OFFICE, TOTALING 190 SF.

NEW HOME OFFICE.

 NEW 118 SF. STORAGE AT MAIN RESIDENCE. AFTER THE FACT EMERGENCY PERMIT FOR EXISTING BRIDGE.

R-3, U

■ CONST. TYPE:

243-141-008

 $LDR/1-D(20)(CZ) \mid RC-D(20')(CZ)$

ONE

■ MAX BLDG. HT: 20 FT

35 CY CUT | 10 CY FILL, SEE CIVIL PLANS ■ GRADING:

■ TREE REMOVAL: NONE ▼ TOPOGRAPHY: SLOPING

■ PROJECT CODE COMPLIANCE

2022 CBC, CMC, CPC, CFC, CEC, CALIFORNIA RESIDENTIAL CODE CALIFORNIA GREEN BUILDING CODE & 2022 CALIFORNIA ENERGY

31,695 SF (0.73 Ac.) ■ LOT AREA:

■ LOT COVERAGE CALCULATIONS IN LDR ZONE:

	EXISTING	PROPOSED ADDITION	PROPOSED REMOVAL	PROPOSED TOTAL
MAIN BUILDING	2015	1084	-5	3094
GUEST HOUSE	476	0	-89	387
OFFICE	0	190	0	190
GARAGE	506	0	0	506
PLAYHOUSE	69	0	0	69
SHEDS	82	0	-36	46
TOTAL	3148	1274	-130	4292

■ LOT COVERAGE "LDR ZONE" ALLOWED: 4,754.7 SF (15%)

■ LOT COVERAGE "LDR ZONE" PROPOSED: 4,292 SF (13.5%)

■ LOT COVERAGE CALCULATIONS IN RC ZONE:

	EXISTING	PROPOSED ADDITION	PROPOSED REMOVAL	PROPOSED TOTAL
MAIN BUILDING	486	0	0	486
TOTAL	486	0	0	486

■ LOT COVERAGE "RC ZONE" ALLOWED: 1,584.9 SF (5%)

■ LOT COVERAGE "RC ZONE" EXISTING: 486 SF (1.5%)

CALCULATIONS:

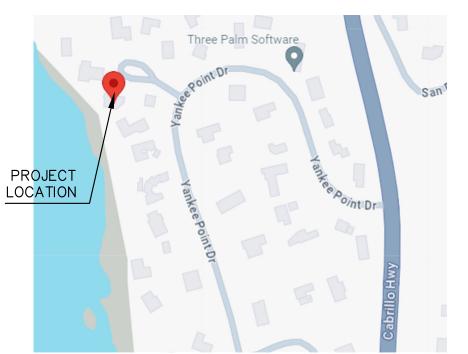
	EXISTING	PROPOSED ADDITION	PROPOSED REMOVAL	PROPOSED TOTAL
MAIN BUILDING	2585	938	-5	3518
GUEST HOUSE	476	0	-89	387
OFFICE	0	190	0	190
GARAGE	506	0	0	506
TOTAL	3567	1128	-94	4601

■ F.A.R. ALLOWED:

4,601 SF (14.5%) ■ F.A.R. PROPOSED:

6,339.6 SF (20%)

VICINITY MAP



JUN A. SILLANO, AIA



ARCHITECTURE + PLANNING + INTERIOR DESIGN

721 LIGHTHOUSE AVE PACIFIC GROVE CA. 93950

(831) 646-1261 FAX (831) 646-1290 EMAIL idg@idg-inc.net WEB idg-inc.net

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STAMPS:

PROJECT/CLIENT:

RICKARD RESIDENCE

PROJECT ADDRESS:

5 YANKEE BEACH WAY CARMEL, CA 93923

APN: 243-141-008

DATE: JUNE 4, 2024 APPLICATION REQ. SUBMITTAL

REVISIONS:

1\ OCTOBER 1, 2024 PLANNING DEPT. SUBMITTAL

<u>∕2\ NOVEMBER 20, 2024</u> PLANNING DEPT. SUBMITTAL

/3\ JANUARY 10, 2025 PLANNING DEPT. SUBMITTAL

4\ <u>FEBRUARY 26, 2025</u> PLANNING DEPT. SUBMITTAL

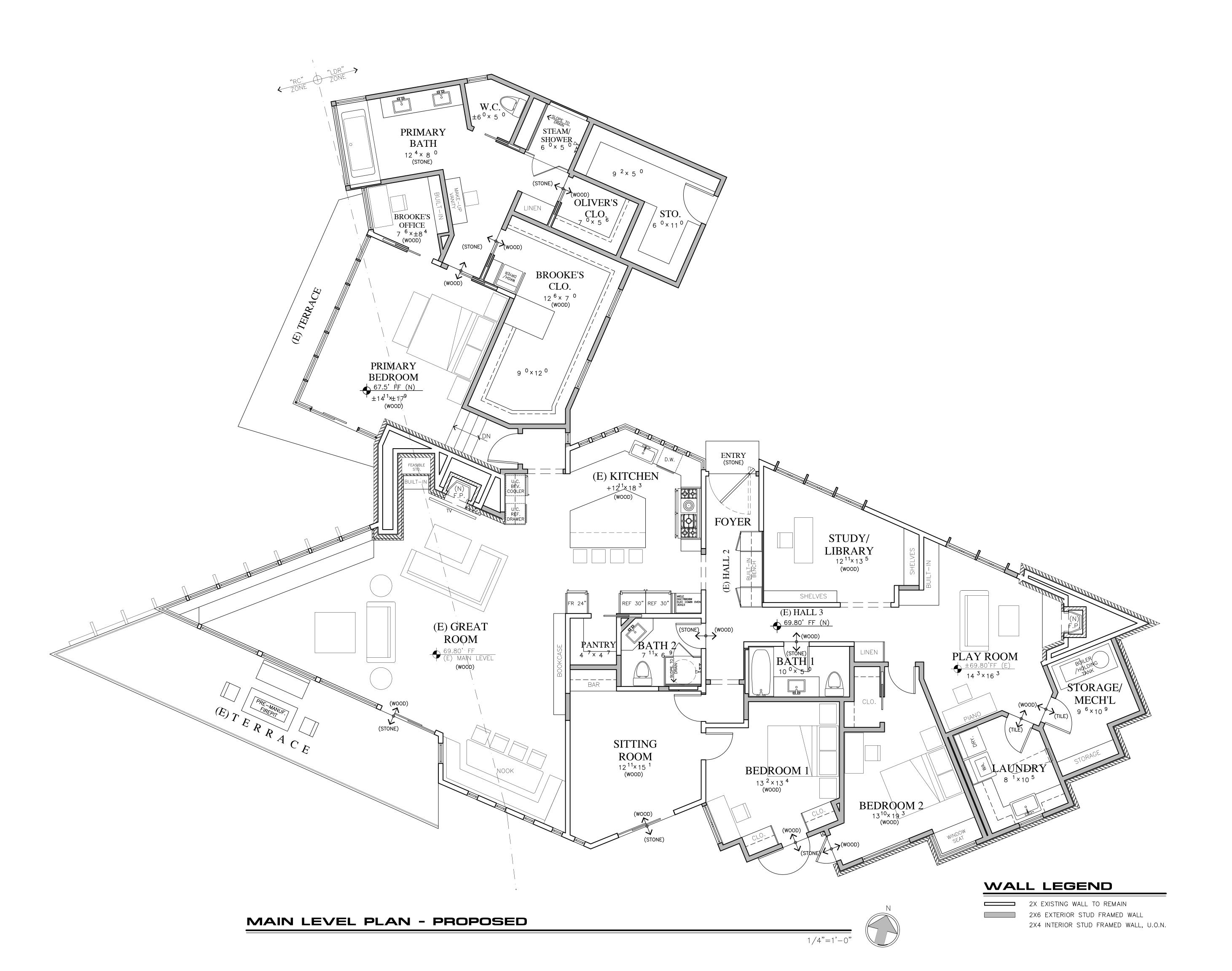
5 <u>SEPTEMBER 22, 2025</u> PLANNING DEPT. SUBMITTAL <u>∕6∖ OCTOBER 21, 2025</u>

SITE PLAN

PLANNING DEPT. SUBMITTAL

EXISTING

SHEET NO. A1.0



JUN A. SILLANO, AIA

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STAMPS:

PROJECT/CLIENT:

RICKARD RESIDENCE

PROJECT ADDRESS:

5 YANKEE BEACH WAY CARMEL, CA 93923

APN: 243-141-008

DATE: JUNE 4, 2024 APPLICATION REQ. SUBMITTAL

REVISIONS:

1 OCTOBER 1, 2024

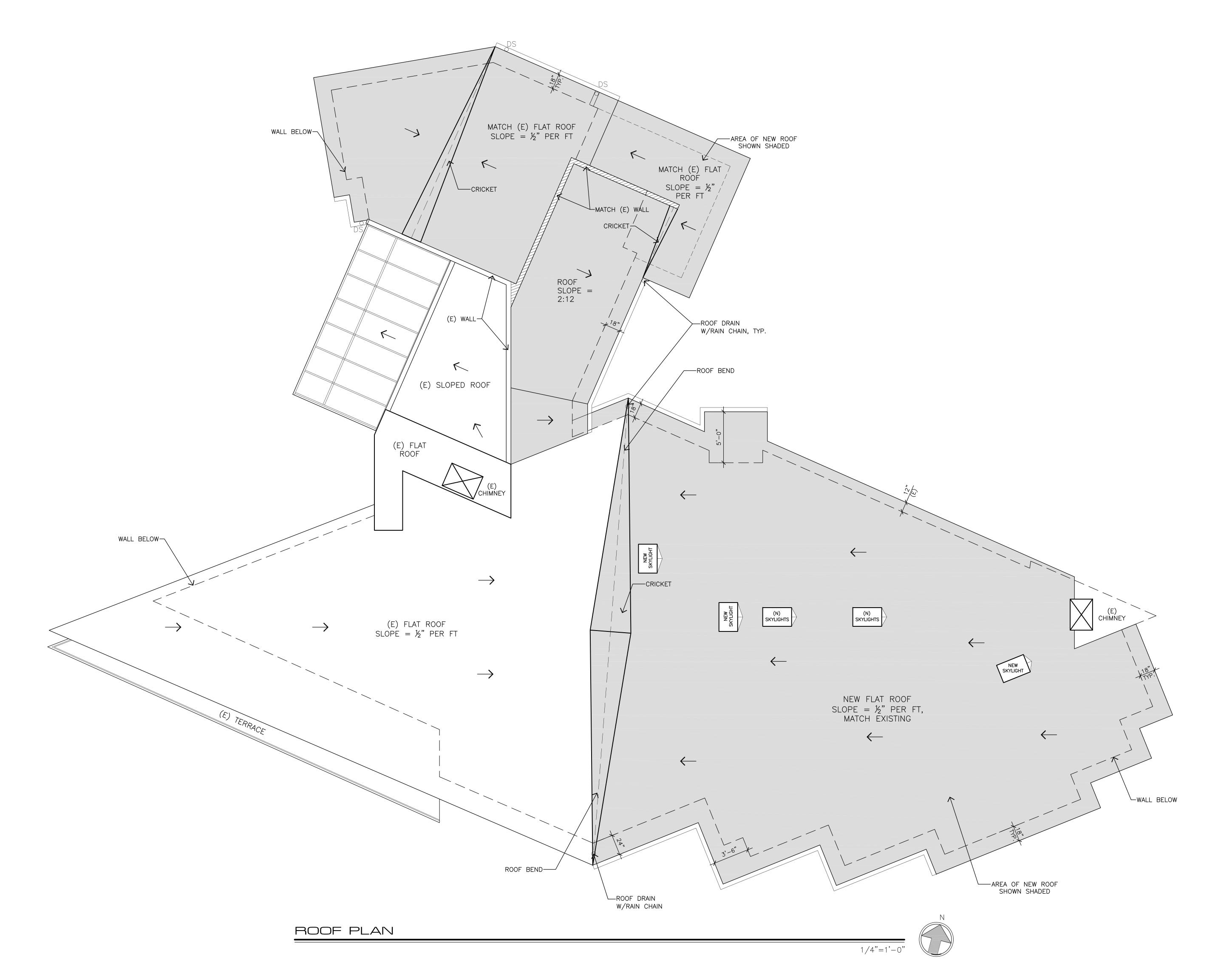
PLANNING DEPT. SUBMITTAL
NOVEMBER 20, 2024
PLANNING DEPT. SUBMITTAL
JANUARY 10, 2025
PLANNING DEPT. SUBMITTAL
FEBRUARY 26, 2025
PLANNING DEPT. SUBMITTAL

PLANNING DEPT. SUBMITTAL SEPTEMBER 22, 2025

MAIN LEVEL **PLAN**

SHEET NO.

A2.0



JUN A. SILLANO, AIA



ARCHITECTURE + PLANNING + INTERIOR DESIGN

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STAMPS:

PROJECT/CLIENT:

RICKARD RESIDENCE

PROJECT ADDRESS:

5 YANKEE BEACH WAY CARMEL, CA 93923

APN: 243-141-008

DATE: JUNE 4, 2024 APPLICATION REQ. SUBMITTAL

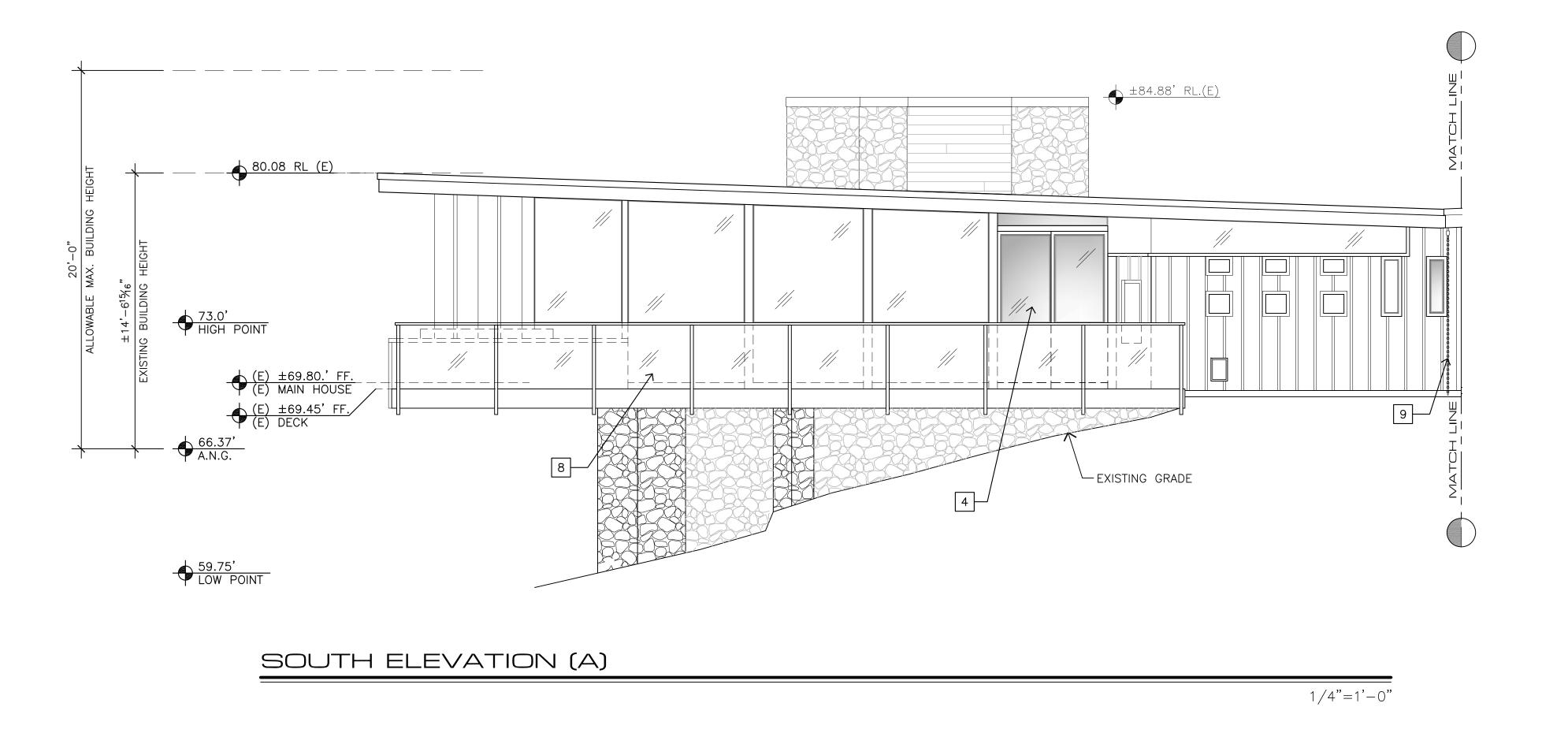
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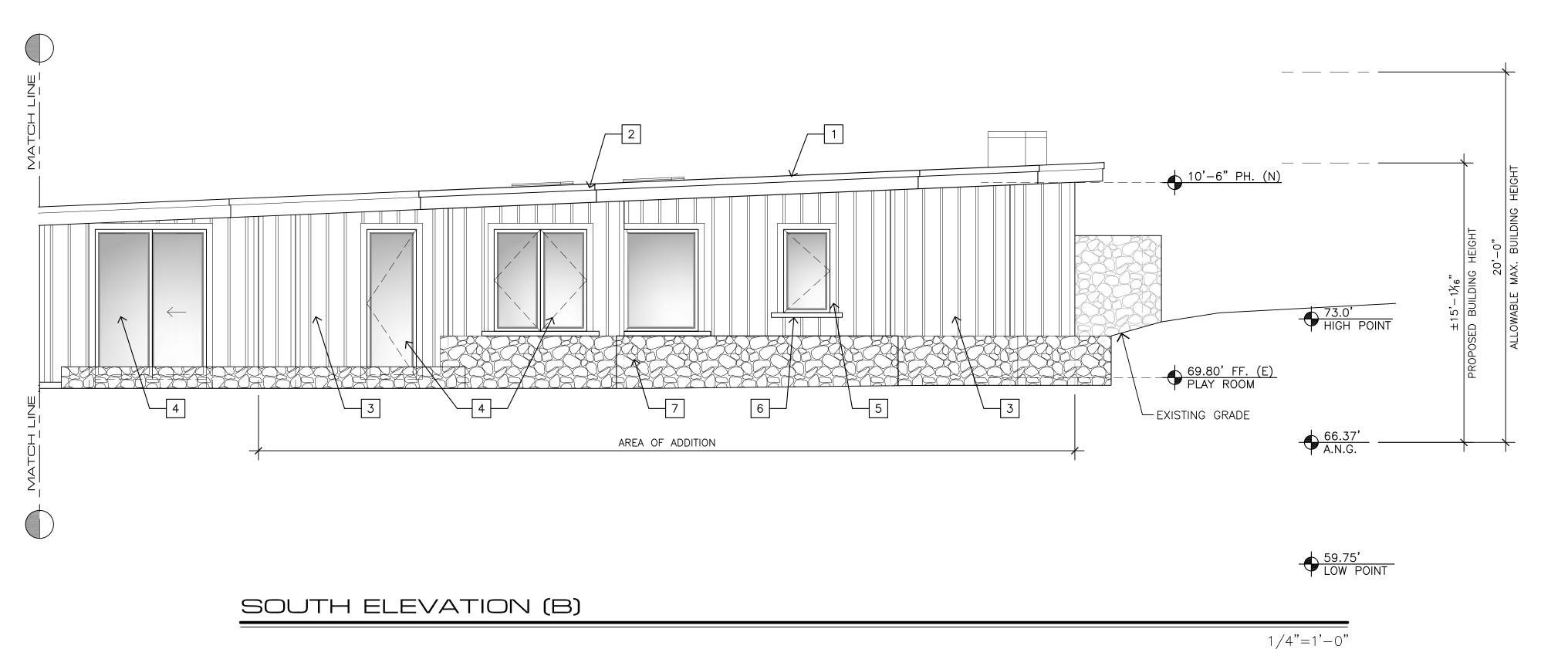
OCTOBER 1, 2024
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PLANNING DEPT. SUBMITTAL
FEBRUARY 26, 2025
PLANNING DEPT. SUBMITTAL
SEPTEMBER 22, 2025
PLANNING DEPT. SUBMITTAL

ROOF **PLAN**

SHEET NO.

A5.0



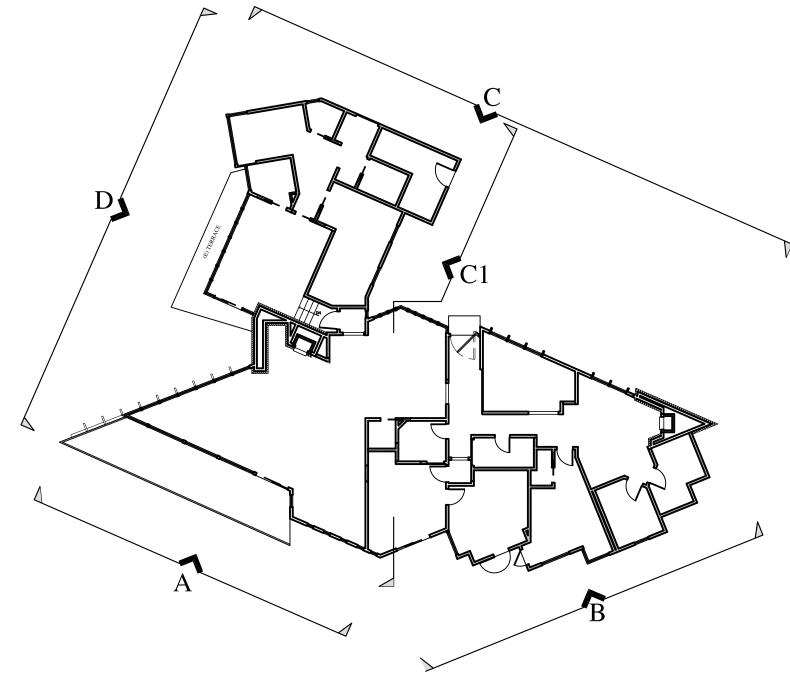


EXTERIOR FINISH LEGEND

- 2X12 CEDAR FASCIA WITH GALVANIZED METAL GUTTER & DOWNSPOUTS MATCH EXISTING

- METAL CLAD WOOD EXTERIOR DOORS & WINDOWS

 MATCH EXISTING
- 5 1X4 PAINTED CEDAR TRIM
- 7 RANDOM RIVER ROCK STONE MATCH EXISTING
- 8 REPAIR/REPLACE AS NEEDED, SIMULATED WOOD GUARDRAIL WITH ½" TEMPERED GLASS MATCH EXISTING



1/16"=1'-0"

KEY PLAN

- 1 BUILT UP ROOFING MATCH EXISTING
- JAMES HARDIE PAINTED BOARD AND BATTEN SIDING
- 6 4X PAINTED CEDAR SILL

PROJECT/CLIENT:

RICKARD RESIDENCE

JUN A. SILLANO, AIA

721 LIGHTHOUSE AVE PACIFIC GROVE CA. 93950

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EMAIL

WEB

DISCLAIMER:

STAMPS:

(831) 646-1261 (831) 646-1290

idg@idg-inc.net

idg-inc.net

PROJECT ADDRESS:

5 YANKEE BEACH WAY CARMEL, CA 93923

APN: 243-141-008

DATE: JUNE 4, 2024 APPLICATION REQ. SUBMITTAL

REVISIONS:

<u>OCTOBER 1, 2024</u>

PLANNING DEPT. SUBMITTAL
NOVEMBER 20, 2024
PLANNING DEPT. SUBMITTAL
JANUARY 10, 2025
PLANNING DEPT. SUBMITTAL
FEBRUARY 26, 2025
PLANNING DEPT. SUBMITTAL

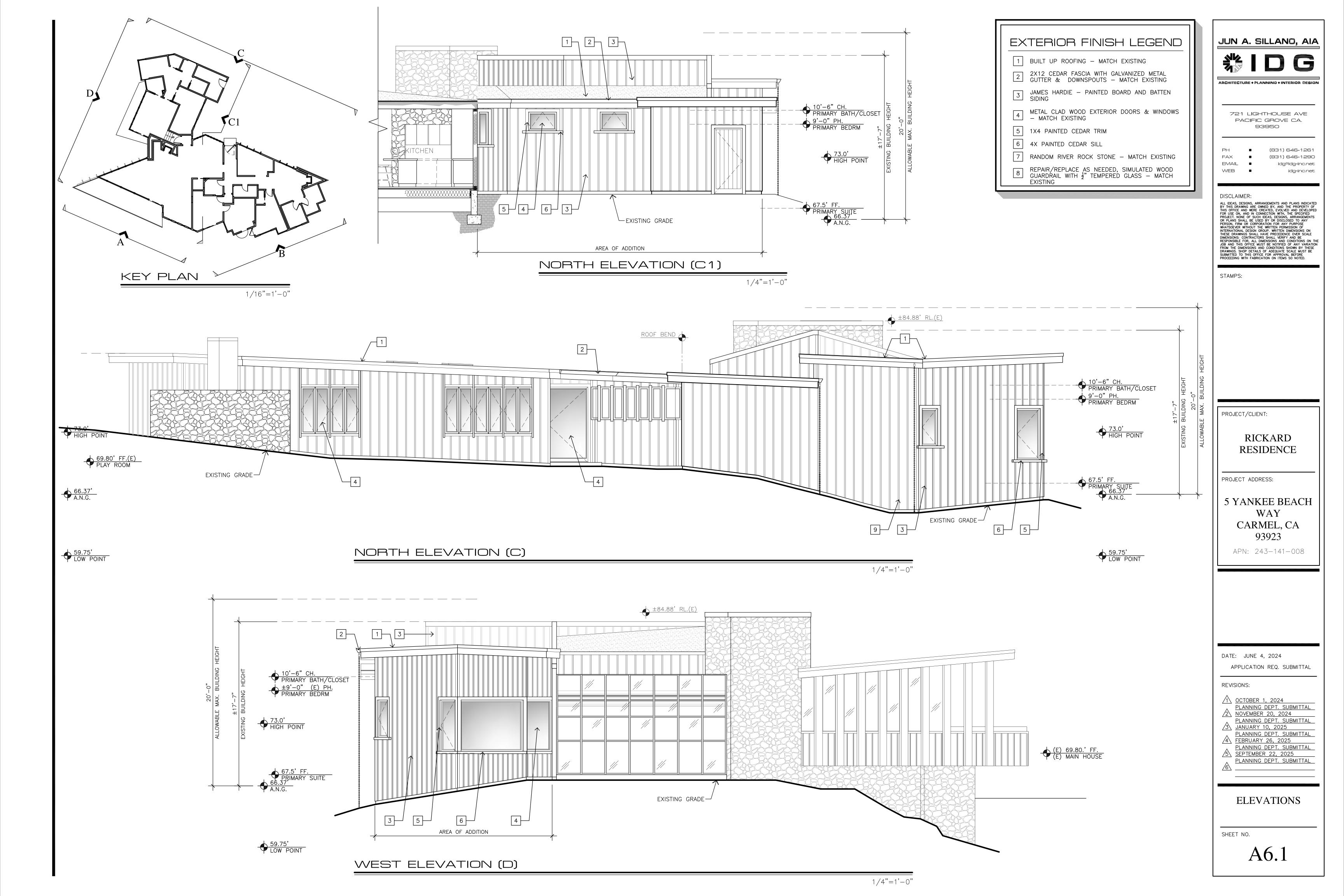
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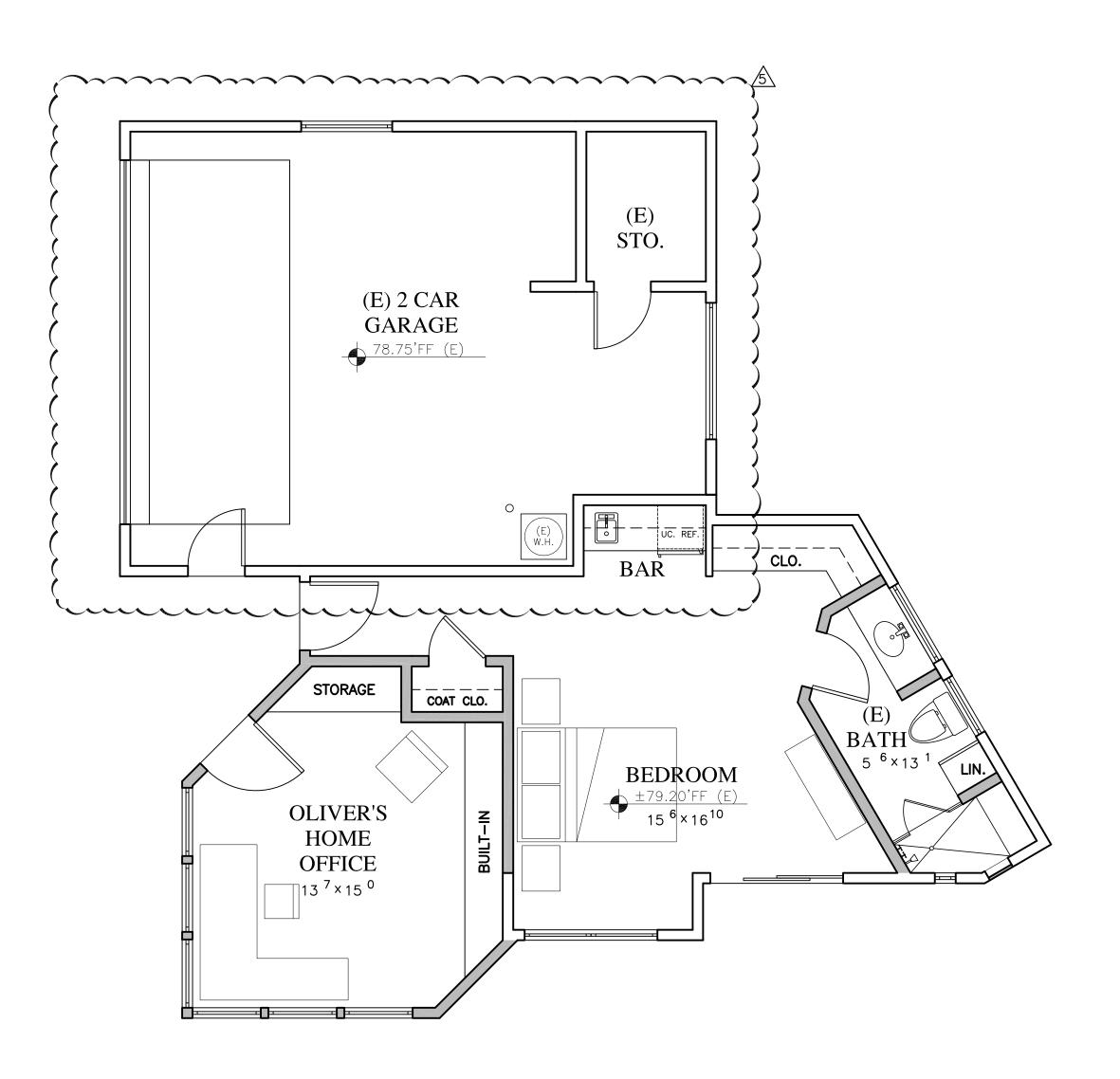
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ELEVATIONS

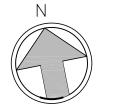
SHEET NO.

A6.0





GUESTHOUSE, OFFICE AND GARAGE - PROPOSED PLAN



1/4"=1'-0"

WALL LEGEND

2X EXISTING WALL TO REMAIN 2X6 EXTERIOR STUD FRAMED WALL 2X4 INTERIOR STUD FRAMED WALL, U.O.N. JUN A. SILLANO, AIA



721 LIGHTHOUSE AVE

PACIFIC GROVE CA.

ARCHITECTURE + PLANNING + INTERIOR DESIGN

93950 PH **8** (831) 646-1261

FAX • (831) 646-1290 EMAIL **S** idg@idg-inc.net WEB idg-inc.net

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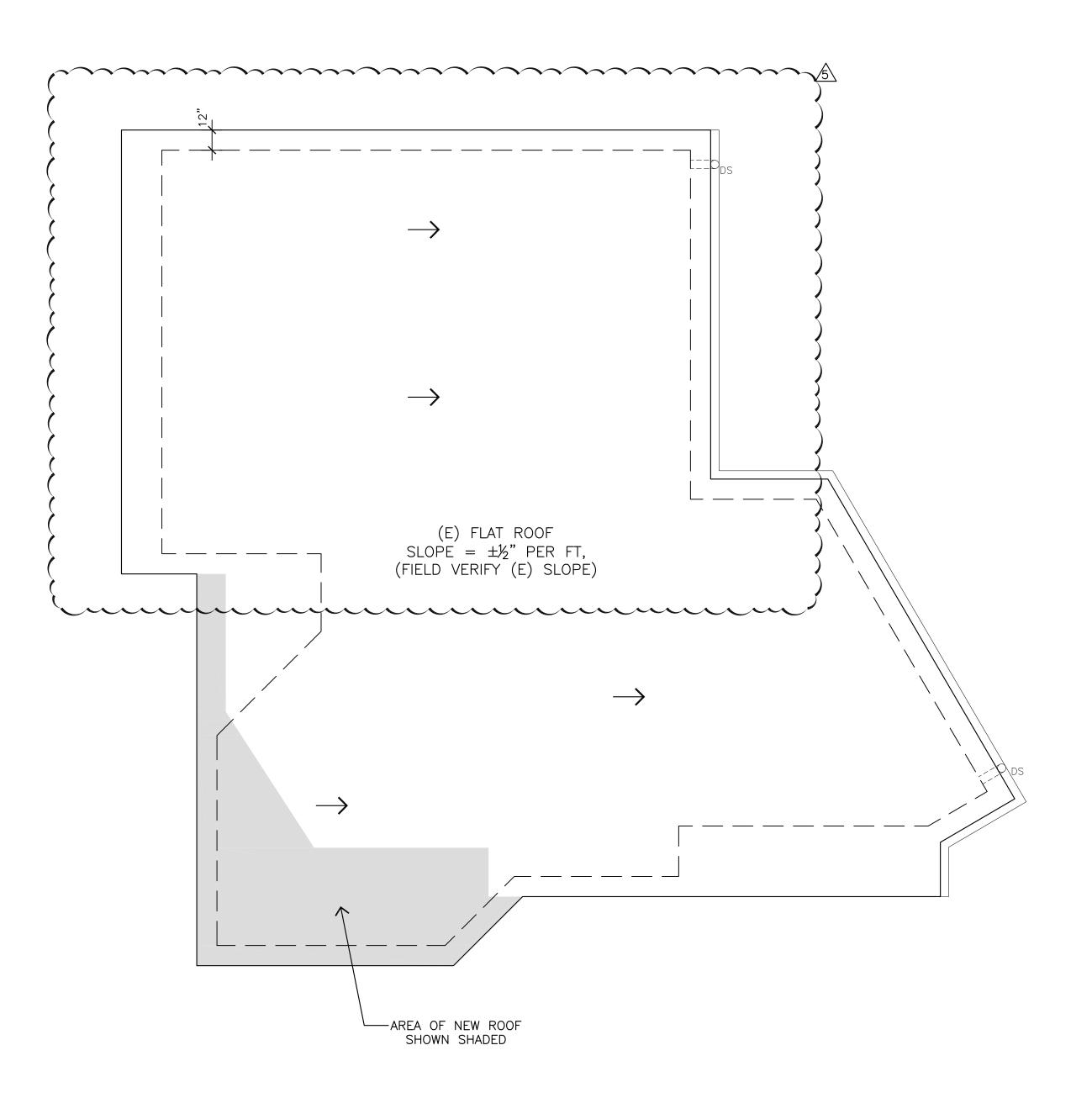
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PLANNING DEPT. SUBMITTAL 5 SEPTEMBER 22, 2025

GUESTHOUSE, OFFICE AND GARAGE PLAN

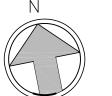
SHEET NO.

GH2.0



GUESTHOUSE, OFFICE AND GARAGE - ROOF PLAN





1/4"=1'-0"

WALL LEGEND

2X EXISTING WALL TO REMAIN 2X6 EXTERIOR STUD FRAMED WALL

2X4 INTERIOR STUD FRAMED WALL, U.O.N.

JUN A. SILLANO, AIA



ARCHITECTURE + PLANNING + INTERIOR DESIGN

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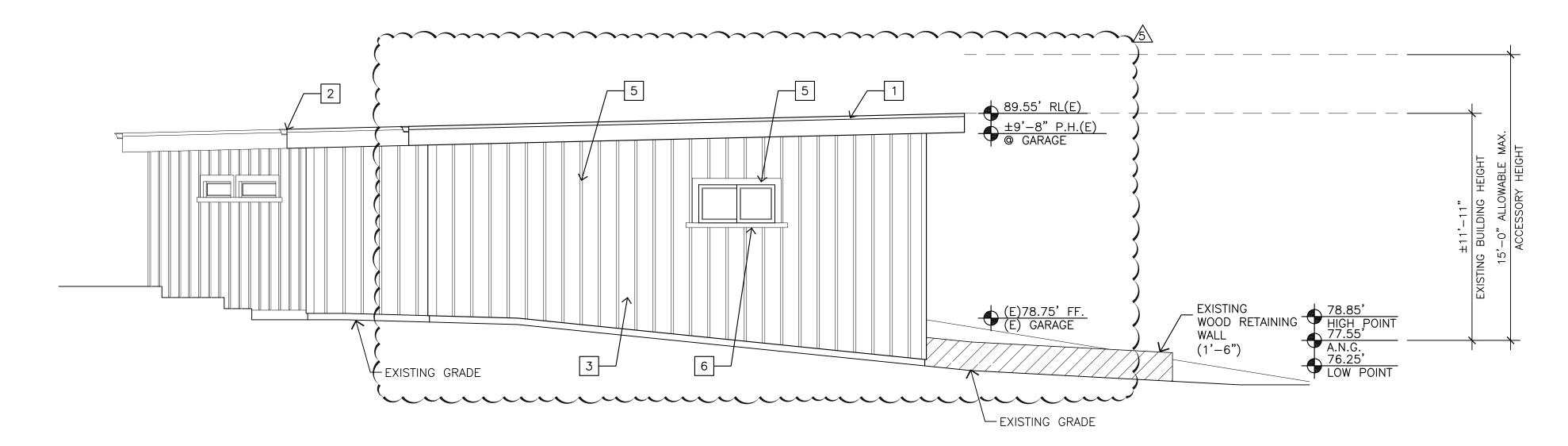
PLANNING DEPT. SUBMITTAL

PLANNING DEPT. SUBMITTAL
SEPTEMBER 22, 2025

GUESTHOUSE, OFFICE AND GARAGE **ROOF PLAN**

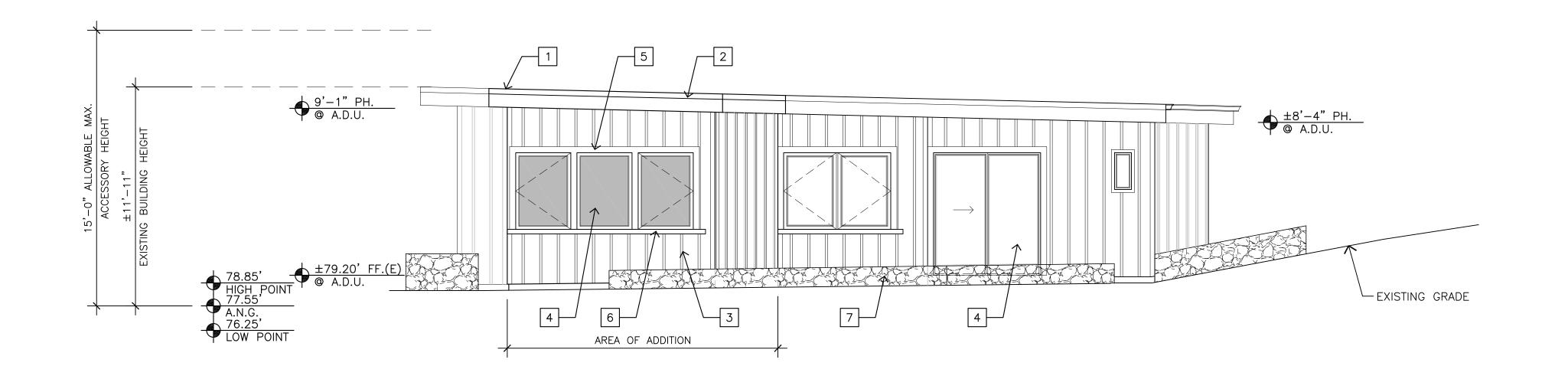
SHEET NO.

GH3.0



NORTH ELEVATION - GUESTHOUSE, OFFICE AND GARAGE

1/4"=1'-0"



SOUTH ELEVATION - GUESTHOUSE, OFFICE AND GARAGE

1/4"=1'-0"

EXTERIOR FINISH LEGEND

- 1 BUILT UP ROOFING MATCH EXISTING
- 2X12 CEDAR FASCIA WITH GALVANIZED METAL GUTTER & DOWNSPOUTS MATCH EXISTING
- JAMES HARDIE PAINTED BOARD AND BATTEN
- METAL CLAD WOOD EXTERIOR DOORS & WINDOWS

 MATCH EXISTING
- 5 1X4 PAINTED CEDAR TRIM
- 6 4X PAINTED CEDAR SILL
- 7 RANDOM RIVER ROCK STONE MATCH EXISTING

ARCHITECTURE + PLANNING + INTERIOR DESIGN

JUN A. SILLANO, AIA

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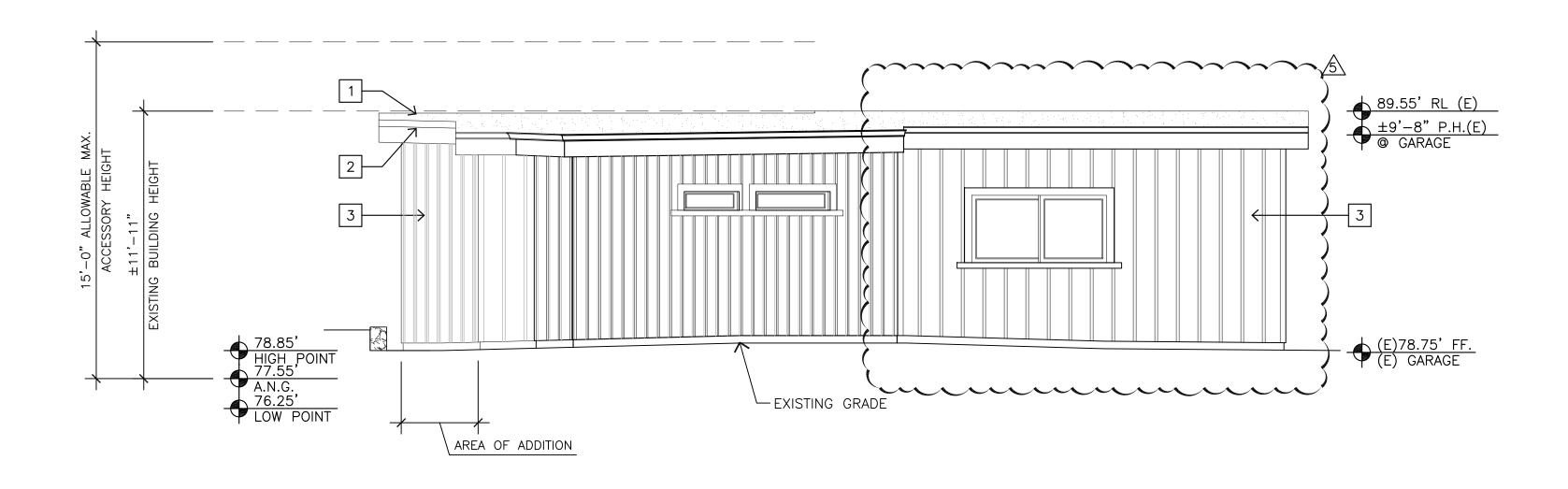
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PLANNING DEPT. SUBMITTAL SEPTEMBER 22, 2025

GUESTHOUSE, OFFICE

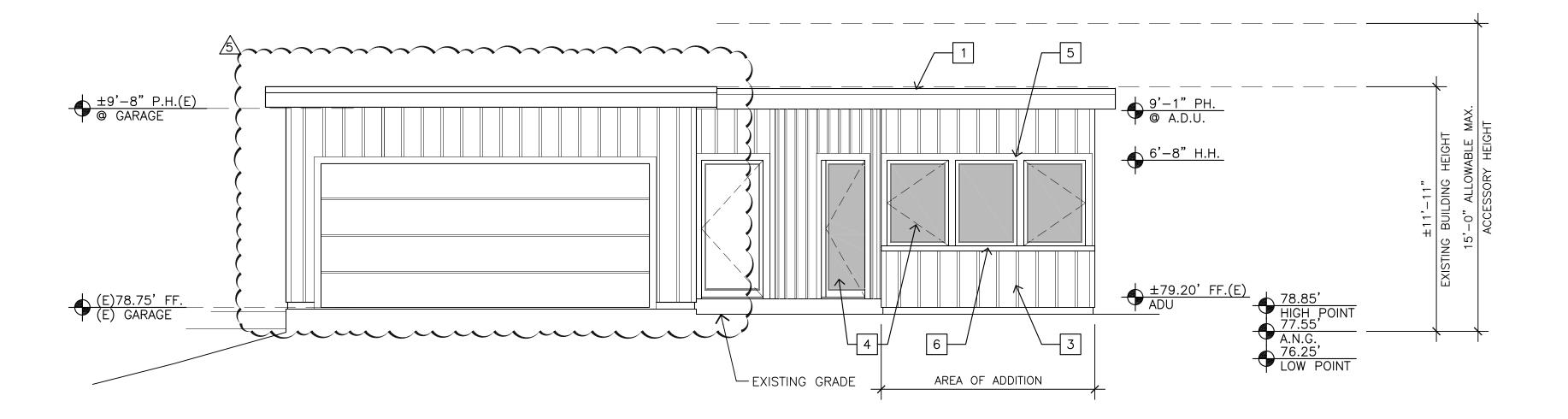
AND GARAGE ELEV. SHEET NO.

GH4.0



EAST ELEVATION - GUESTHOUSE, OFFICE AND GARAGE

1/4"=1'-0"



WEST ELEVATION - GUESTHOUSE, OFFICE AND GARAGE

1/4"=1'-0"



ARCHITECTURE + PLANNING + INTERIOR DESIGN

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STAMPS:

EXTERIOR FINISH LEGEND

1 BUILT UP ROOFING - MATCH EXISTING

5 1X4 PAINTED CEDAR TRIM

6 4X PAINTED CEDAR SILL

2X12 CEDAR FASCIA WITH GALVANIZED METAL GUTTER & DOWNSPOUTS — MATCH EXISTING

JAMES HARDIE — PAINTED BOARD AND BATTEN SIDING

METAL CLAD WOOD EXTERIOR DOORS & WINDOWS

- MATCH EXISTING

RANDOM RIVER ROCK STONE - MATCH EXISTING

PROJECT/CLIENT:

RICKARD RESIDENCE

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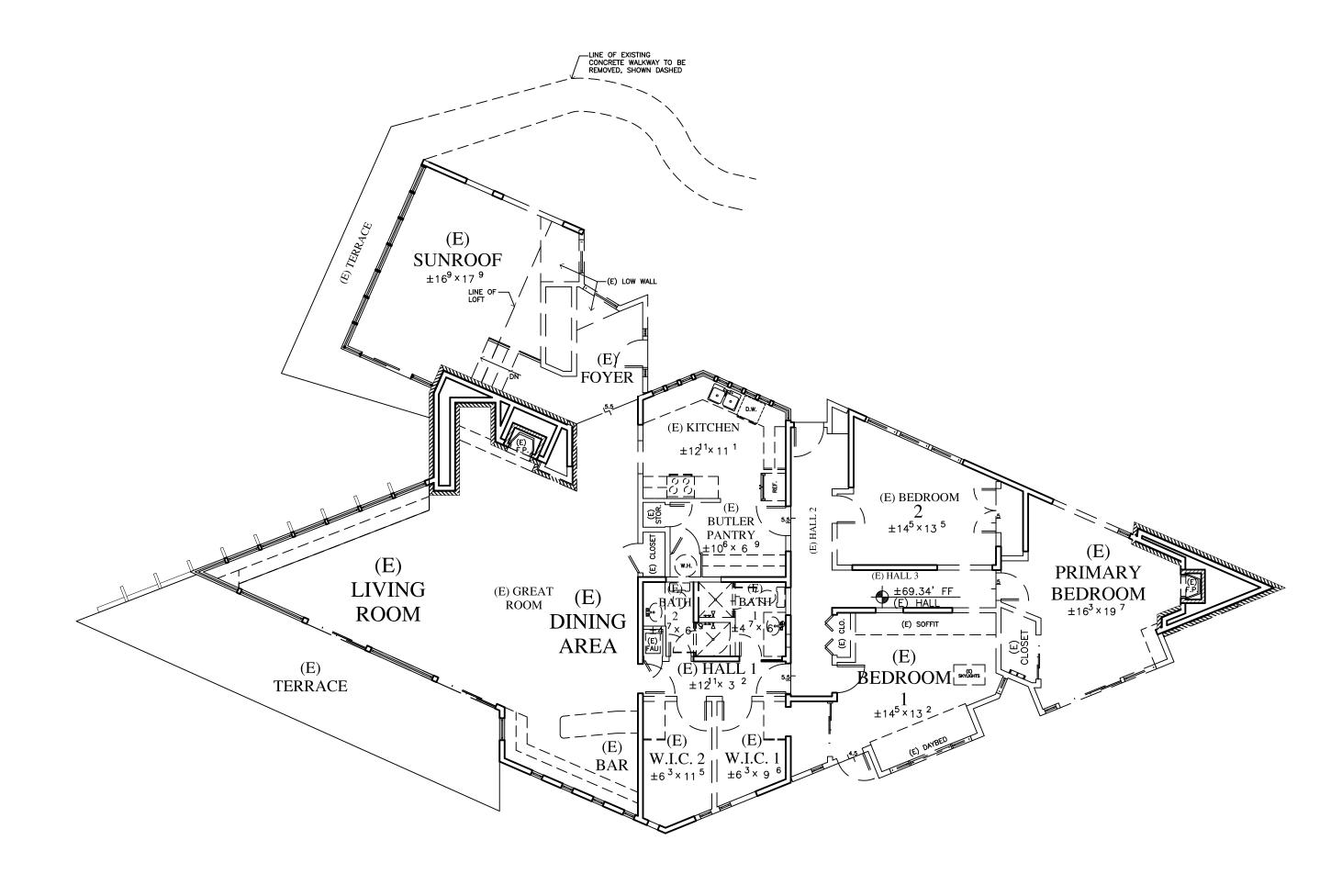
SEPTEMBER 22, 2025

GUESTHOUSE, OFFICE

AND GARAGE ELEV.

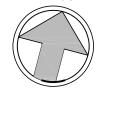
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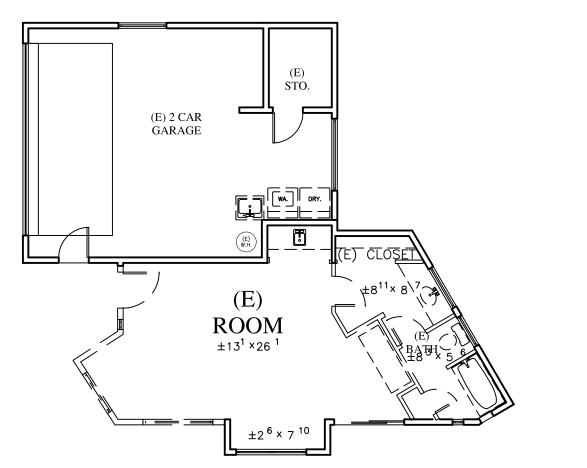
GH4.1



MAIN RESIDENCE EXISTING-DEMOLITION PLAN







GARAGE AND GUESTHOUSE EXISTING-DEMOLITION PLAN



WALL LEGEND

2X EXISTING WALL TO REMAIN (E) DOOR OR WINDOW TO BE REMOVED □□□□□□ 2X EXISTING WALL TO BE REMOVED

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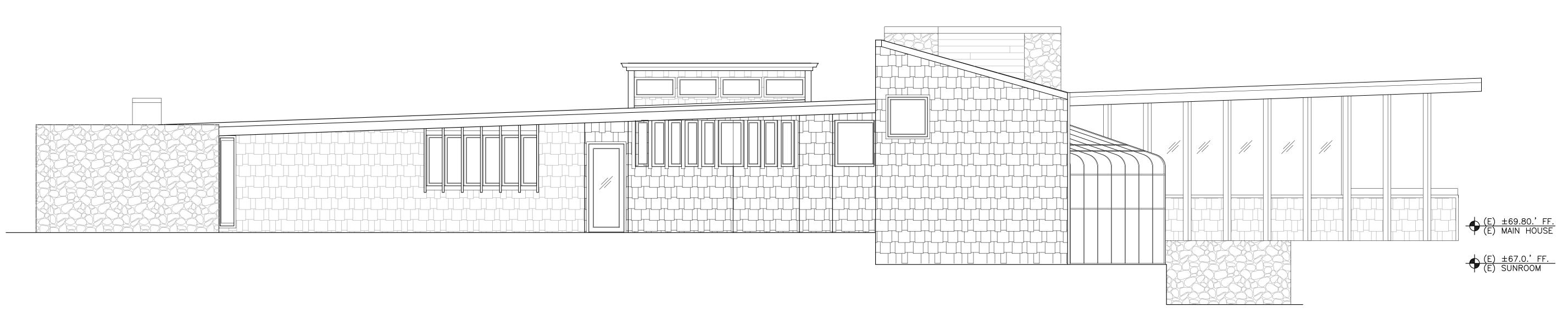
4\ FEBRUARY 26, 2025 PLANNING DEPT. SUBMITTAL

SEPTEMBER 22, 2025

MAIN & G.H. EXIST.-DEMO. PLAN

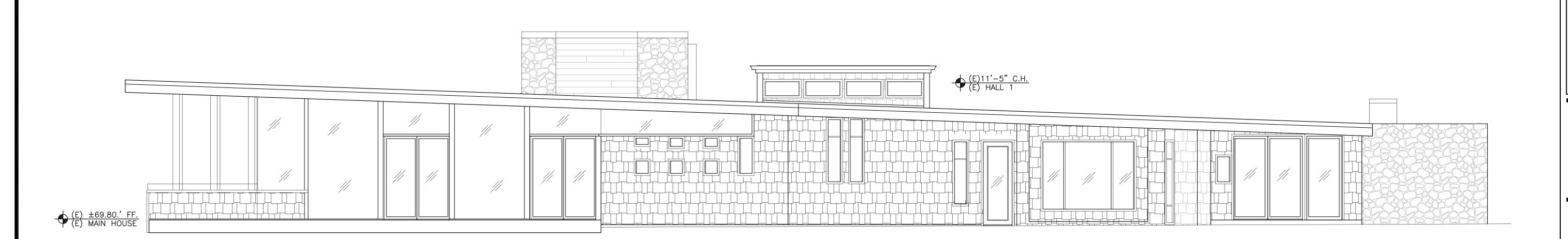
SHEET NO.

D1.0



NORTH ELEVATION - MAIN HOUSE EXISTING

1/4"=1'-0"



SOUTH ELEVATION - MAIN HOUSE EXISTING

1/4"=1'-0"



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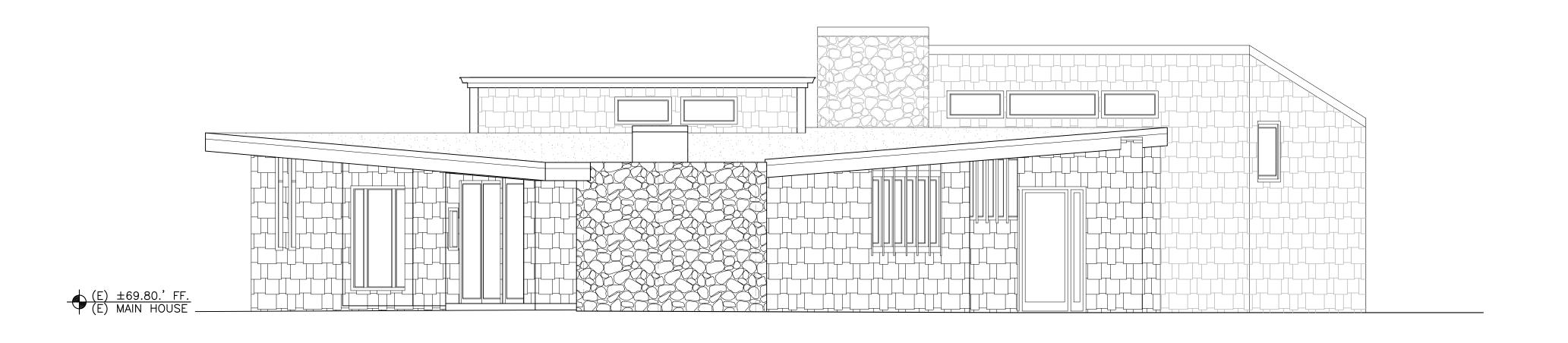
PLANNING DEPT. SUBMITTAL 5 SEPTEMBER 22, 2025

PLANNING DEPT. SUBMITTAL

MAIN HOUSE **EXISTING ELEVATION**

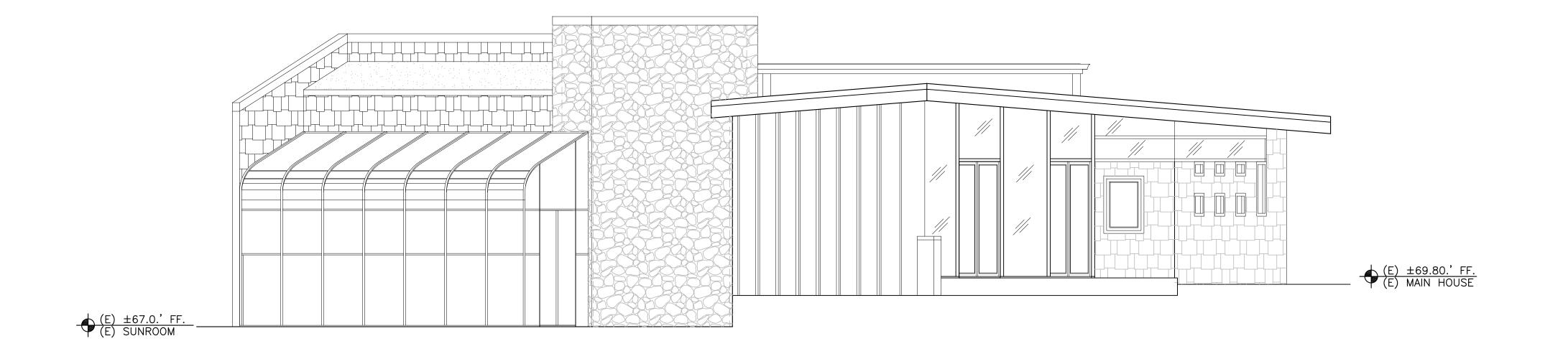
SHEET NO.

D2.0



EAST ELEVATION - MAIN HOUSE EXISTING

1/4"=1'-0"



WEST ELEVATION - MAIN HOUSE EXISTING

1/4"=1'-0"

JUN A. SILLANO, AIA



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ARCHITECTURE + PLANNING + INTERIOR DESIGN

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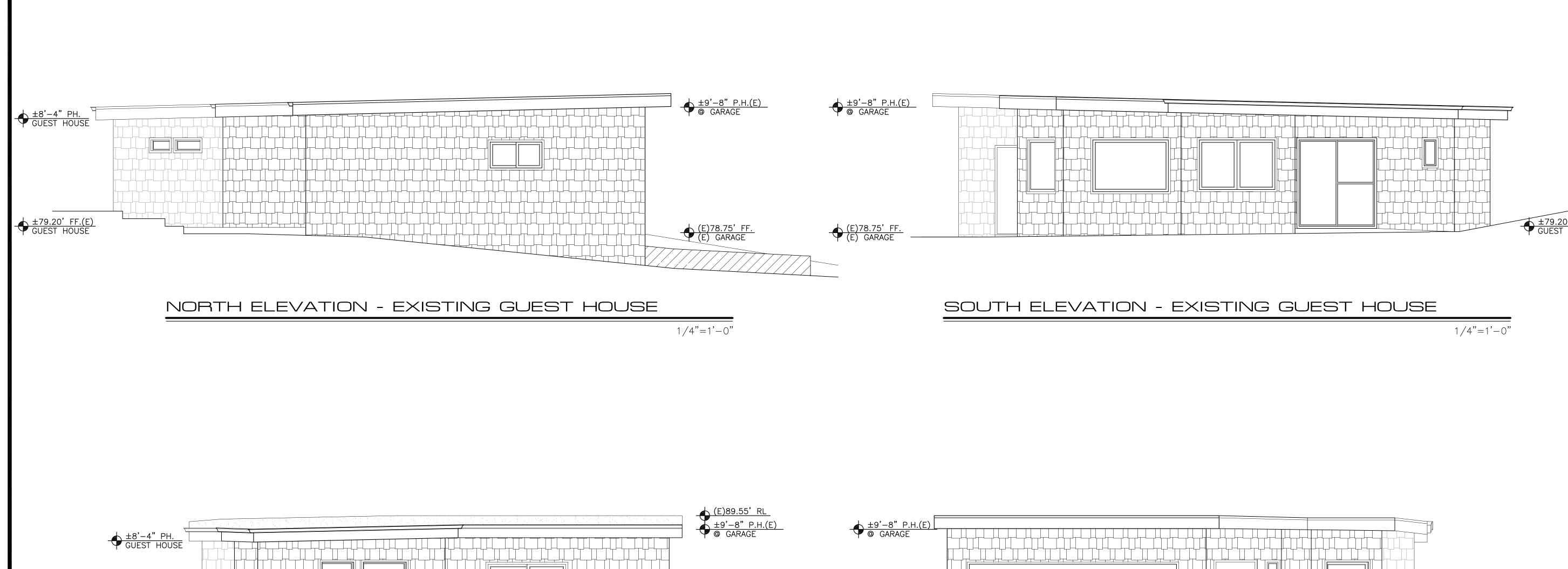
5 SEPTEMBER 22, 2025

EXISTING ELEVATION

SHEET NO.

D2.1

MAIN HOUSE



(E) 78.75' FF. (E) GARAGE

1/4"=1'-0"

(E) 78.75' FF. (E) GARAGE

WEST ELEVATION - EXISTING GUEST HOUSE

±79.20' FF.(E)
GUEST HOUSE -

EAST ELEVATION - EXISTING GUEST HOUSE

JUN A. SILLANO, AIA ARCHITECTURE + PLANNING + INTERIOR DESIGN 721 LIGHTHOUSE AVE PACIFIC GROVE CA. 93950 (831) 646-1261 (831) 646-1290 EMAIL WEB DISCLAIMER: ALL IDEAS, DESIGNS, ARRANGEMENTS AND PLANS INDICATED BY THIS DRAWING ARE OWNED BY, AND THE PROPERTY OF THIS OFFICE AND WERE CREATED, EVOLVED AND DEVELOPED FOR USE ON, AND IN CONNECTION WITH, THE SPECIFIED PROJECT. NONE OF SUCH IDEAS, DESIGNS, ARRANGEMENTS OR PLANS SHALL BE USED BY OR DISCLOSED TO ANY PERSON, FIRM OR CORPORATION FOR ANY PURPOSE WHATSOEVER WITHOUT THE WRITTEN PERMISSION OF INTERNATIONAL DESIGN GROUP. WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL HAVE PRECEDENCE OVER SCALE DIMENSIONS: CONTRACTORS SHALL VERIFY AND BE RESPONSIBLE FOR, ALL DIMENSIONS AND CONDITIONS ON THE JOB AND THIS OFFICE MUST BE NOTIFIED OF ANY VARIATION FROM THE DIMENSIONS AND CONDITIONS SHOW BY THESE DRAWINGS. SHOP DETAILS OF ADEQUATE SCALE MUST BE SUBMITTED TO THIS OFFICE FOR APPROVAL BEFORE. STAMPS: PROJECT/CLIENT:

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EXISTING ELEVATION GUEST HOUSE

SHEET NO.

D3.0

GRADING, DRAINAGE & EROSION CONTROL PLAN

RICKARD RESIDENCE

APN: 243-141-008

CARMEL, MONTEREY COUNTY, CALIFORNIA



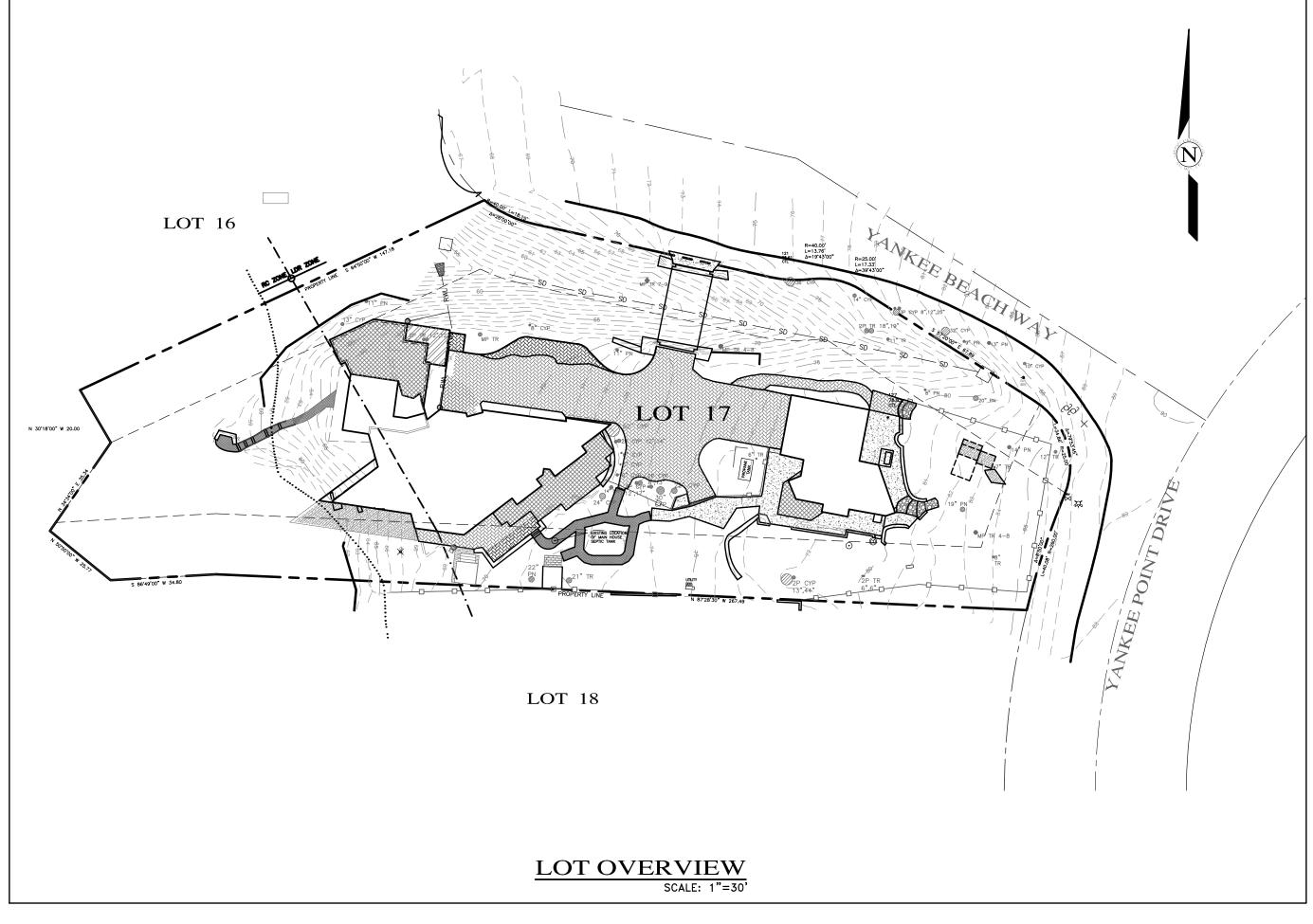
GENERAL NOTES:

- 1) PROJECT DESIGN BASED ON INFORMATION PROVIDED AND SHOWN ON THE SITE PLAN FOR THE RICKARD RESIDENCE A1.0 PREPARED BY INTERNATIONAL DESIGN GROUP, DATED 06/20/24, AND THE BASE TOPOGRAPHIC INFORMATION PREPARED BY LANDSET ENGINEERS, INC. ON MAY 8, 2024.
- 2) NOT ALL UNDERGROUND UTILITIES WERE LOCATED. ONLY VISIBLE FACILITIES ABOVE AND FLUSH WITH THE SURFACE ARE SHOWN. SUB-SURFACE UTILITY LINES DRAWN MAY NOT BE COMPLETE AND SHOULD BE VERIFIED BY FIELD RECONNAISSANCE. UNDERGROUND UTILITY LOCATIONS CAN BE OBTAINED FROM THE APPROPRIATE UTILITY COMPANIES, PUBLIC AGENCIES, OWNER'S AS-BUILT DRAWINGS, ETC., AND SHOULD BE THROUGHLY COMPILED AND DEEMED COMPLETE WITH THE PROJECT AREA, PRIOR TO ANY SITE DEVELOPMENT DESIGN AND/OR CONSTRUCTION.
- OR GEOLOGY INFORMATION, UNDERGROUND CONDITIONS, EASEMENTS, ZONING OR REGULATORY OR ANY OTHER ITEMS NOT SPECIFICALLY REQUESTED BY THE PROPERTY OWNER.
- 4) THIS MAP DOES NOT REPRESENT A BOUNDARY SURVEY.

GRADING & DRAINAGE NOTES:

- 1) ALL GRADING SHALL CONFORM TO THE MONTEREY COUNTY GRADING ORDINANCE AND EROSION CONTROL ORDINANCE. THE RECOMMENDATIONS FOUND IN THE PROJECT SOIL'S ENGINEERING INVESTIGATION. THE LATEST VERSION OF THE CALTRANS SPECIFICATIONS. THE GOVERNING PUBLIC AGENCIES. THE LATEST VERSION OF THE CALIFORNIA BUILDING CODE (CBC) AND THESE PLANS.
- 2) SURFACE ORGANICS SHALL BE STRIPPED AND STOCKPILED FOR LATER USE AS TOPSOIL MATERIAL ACTUAL GRADING SHALL BEGIN WITHIN 30 DAYS OF VEGETATION REMOVAL OR THE AREA SHALL BE PLANTED TO
- 3) NO ORGANIC MATERIAL SHALL BE PERMITTED IN FILLS EXCEPT AS TOPSOIL USED FOR SURFACE PLANT GROWTH ONLY AND WHICH DOES NOT EXCEED 4" IN DEPTH.
- 4) THERE ARE APPROXIMATELY 35 C.Y. OF CUT AND 10 C.Y. OF FILL WITH 25 C.Y. OF EXPORT. EXCAVATION SHALL BE USED FOR EMBANKMENT CONSTRUCTION, LANDSCAPE PURPOSES AND/OR HAULED OFF-SITE ON A LEGAL MANER. ADDITIONAL ON-SITE SPOILS GENERATED FROM FOUNDATIONS, UTILITY TRENCHES, SEPTIC CONSTRUCTION, ETC. IS NOT INCLUDED IN THE ABOVE REFERENCED QUANTITIES. IMPORT MATERIAL SHALL MEET THE REQUIREMENTS OF SELECT STRUCTURAL FILL AS NOTED IN THE SOIL'S

 17) ALL WORK IS SUBJECT TO APPROVAL BY THE PUBLIC WORKS SUPERINTENDENT INSPECTION AND REPORT AND BE APPROVED BY THE SOILS ENGINEER PRIOR TO PLACEMENT.
- 5) EMBANKMENT MATERIAL SHALL BE PLACED IN 8" LOOSE LIFTS, MOISTURE CONDITIONED, AND COMPACTED TO 90% MIN. REL. COMPACTION. ALL BASEROCK AND THE UPPER 12" OF SUBGRADE SHALL BE COMPACTED TO 95% MIN. REL. COMPACTION.
- 6) ALL CUT AND FILL SLOPES SHALL BE 2:1 OR FLATTER. STEEPER SLOPES MAY BE ALLOWED ONLY WITH THE PERMISSION OF THE SOIL'S ENGINEER.
- 7) PAD ELEVATIONS SHALL BE CERTIFIED TO 0.10', PRIOR TO DIGGING ANY FOOTINGS OR SCHEDULING ANY
- 8) DUST FROM GRADING OPERATIONS MUST BE CONTROLLED. CONTRACTOR SHALL PROVIDE ADEQUATE WATER TO CONTROL DUST DURING AND FOR GRADING OPERATIONS.
- 9) A COPY OF ALL COMPACTION TESTS AND FINAL GRADING REPORT SHALL BE SUBMITTED TO THE COUNTY OF MONTEREY PLANNING AND BUILDING INSPECTION DEPARTMENT AT SCHEDULED INSPECTIONS.
- 10) THE GROUND IMMEDIATELY ADJACENT TO FOUNDATIONS SHALL BE SLOPED AWAY FROM THE BUILDING AT 5% FOR A MINIMUM DISTANCE OF 10 FEET. IF PHYSICAL OBSTRUCTIONS OR LOT LINES PROHIBIT 10 FOOT OF HORIZONTAL DISTANCE, A 5% SLOPE SHALL BE PROVIDED TO AN APPROVED ALTERNATIVE METHOD OF DIVERTING WATER AWAY FROM THE FOUNDATION. SWALES USED FOR THIS PURPOSE SHALL BE SLOPED AT A MINIMUM OF 1% WHERE LOCATED WITHIN 5 FEET OF THE BUILDING FOUNDATION. IMPERVIOUS SURFACES WITHIN 10 FEET OF THE BUILDING FOUNDATION SHALL BE SLOPED AT A MINIMUM OF 2% AWAY FROM THE
- 11)ROOF DRAINAGE SHALL BE CONTROLLED BY GUTTER AND DOWN SPOUTS WITH SPLASH BLOCKS WHICH OUTLET INTO LANDSCAPE AREAS AND/OR CONNECTED INTO RAIN WATER LEADERS WHICH OUTLET INTO THE EXISTING STORM DRAIN SYSTEM. RAIN WATER LEADERS SHALL BE 4" PVC-SDR 35 PIPE, HAVE A MINIMUM SLOPE OF 1% AND A MINIMUM COVER OF 12". THE RAIN WATER LEADER TRENCHES SHOULD HAVE THEIR BEARING SURFACES FOUNDED BELOW AN IMAGINARY 1:1 (HORIZONTAL TO VERTICAL) LANE PROJECTED UPWARD FROM THE BOTTOM EDGE OF THE BUILDING FOOTINGS.
- 12) STORM WATER (SURFACE RUNOFF) SHALL BE KEPT ON ITS ORIGINAL STATE WITH EXISTING DRAINAGE PATTERNS MAINTAINED.
- 13) SUBSURFACE DRAINAGE FOR RETAINING WALLS IF REQUIRED WILL BE COLLECTED AND PIPED TO DAYLIGHT IN A NON-EROSIVE MANNER.



14) THE STORM DRAIN FACILITIES IF REQUIRED SHALL BE CHRISTY PRODUCTS OR APPROVED EQUAL. THE STORM DRAIN PIPE SHALL BE P.V.C. MIN. SDR 35.

15) ALL NEW UTILITY AND DISTRIBUTION LINES SHALL BE PLACED UNDERGROUND.

16) UTILITY TRENCHES WITHIN THE BUILDING PAD OR ANY NEW PAVED AREAS SHALL BE BACKFILLED WITH CLEAN, IMPORTED SAND AND THE TRENCH BACKFILL SHALL BE COMPACTED TO 95% MIN. RELATIVE COMPACTION. THE TOP 8" OF TRENCH SHALL BE CAPPED WITH NATIVE SOIL. IN NON-PAVED AREAS NATIVE BACKFILL SHALL BE USED AND COMPACTED TO 90% MIN. RELATIVE COMPACTION.

ACCEPTANCE.

18) NO LAND DISTURBING OR GRADING SHALL OCCUR ON THE SUBJECT PARCEL BETWEEN OCTOBER 15 AND APRIL 15 UNLESS AUTHORIZED BY THE DIRECTOR OF RMA — BUILDING SERVICES.

19) SPECIAL INSPECTIONS, BY A SPECIAL INSPECTOR, ARE REQUIRED DURING FILL PLACEMENT AND THAT PROPER MATERIALS AND PROCEDURES ARE USED IN ACCORDANCE WITH THE PROVISIONS OF THE APPROVED GEOTECHNICAL REPORT.

20) THE LOCATION, HEIGHT AND THE PLATE HEIGHTS OF THE NEW STRUCTURE MUST BE CERTIFIED BY A SURVEYOR TO BE IN CONFORMANCE WITH THE APPROVED PLANS.

21)IF DURING THE COURSE OF CONSTRUCTION, CULTURAL, ARCHAEOLOGICAL, HISTORICAL OR PALEONTOLOGICAL RESOURCES ARE UNCOVERED AT THE SITE (SURFACE OR SUBSURFACE RESOURCES) WORK SHALL BE HALTED IMMEDIATELY WITHIN 50 METERS (165 FEET) OF THE FIND UNTIL A QUALIFIED PROFESSIONAL ARCHAEOLOGIST CAN EVALUATE IT. MONTEREY COUNTY RMA - PLANNING DEPARTMENT AND A QUALIFIED ARCHAEOLOGIST SHALL BE IMMEDIATELY CONTACTED BY THE RESPONSIBLE INDIVIDUAL PRESENT ON-SITE. WHEN CONTACTED, THE PROJECT PLANNER AND THE ARCHAEOLOGIST SHALL IMMEDIATELY VISIT THE SITE TO DETERMINE THE EXTENT OF THE RESOURCES AND TO DEVELOP PROPER MITIGATION MEASURES REQUIRED FOR RECOVERY.

STORM WATER CONTROL NOTES:

- 1) THE PROJECT IS NOT LOCATED WITHIN THE MUNICIPAL GENERAL PERMIT BOUNDARY AS DEFINED BY THE CALIFORNIA STATE WATER QUALITY CONTROL BOARD ORDER No. 2013-0001-DWQ; THEREFORE, THE POST-CONSTRUCTION STORM WATER MANAGEMENT REQUIREMENTS (PCRs) FOR DEVELOPMENT PROJECTS IN THE CENTRAL COAST REGION DO NOT APPLY.
- 2) THIS PROJECT WILL CREATE AND/OR REPLACE COLLECTIVELY OVER THE ENTIRE PROJECT 1,206 SF OF IMPERVIOUS SURFACES AND WILL DISTURB A LAND AREA OF 1,656 SQ.FT.

TOTAL LOT AREA = 31,695 SQ.FT. NEW/REPLACE IMPERVIOUS AREA = 1,377 SQ.FT. TOTAL AREA OF DISTURBANCE = 1,830 SQ.FT.

> **GRADING QUANTITIES:** CUT = 15 C.Y.

FILL = 15 C.Y.NET = 0 C.Y.

INDEX TO SHEETS

LEGEND:

Inspection item:

Site stripping and

Subexcavation, fill

placement, and compact

Surface and subsurface

drainage improvements

Utility trench

backfill compaction

Baserock subgrade

compaction

NEW:

──── ─── STORM DRAIN LINE

- RWL --- RWL --- RWL --- RAIN-WATER LEADER

13.20 SPOT ELEVATION

ROOF DOWNSPOUT/SPLASH BLOCK

ROCK/BIOFILTRATION BASIN

NOTE: PRIOR TO FINAL INSPECTION, THE OWNER/APPLICANT SHALL PROVIDE CERTIFICATION FROM THE

GEOTECHNICAL INSPECTION SCHEDULE

Who will conduct | When the Inspection | Inspection

Throughout grading operations

Prior to placement of

forms and reinforcin

During backfill

Prior to pavement

WITH THE RECOMMENDATIONS IN THE PROJECT SOIL ENGINEERING INVESTIGATION

Soil's Engineer

Soil's Engineer

Soil's Engineer

Soil's Engineer

Soil's Engineer

Soil's Engineer

Soil's Engineer | During backfill

PROJECT GEOTECHNICAL ENGINEER THAT ALL DEVELOPMENT HAS BEEN CONSTRUCTED IN ACCORDANCE

MAIN FLOOR HOUSE FOOTPRINT ADDITION

PERMEABLE CONCRETE PAVER SURFACE

SHEET C1 COVER SHEET

SHEET C2 TOPOGRAPHIC MAP/EXISTING CONDITIONS

GRADING, DRAINAGE & UTILITY PLAN

EROSION & SEDIMENT CONTROL PLAN CONSTRUCTION MANAGEMENT PLAN

ABBREVIATIONS:

HIGH DENSITY POLYETHELENE

HIGH POINT

LOW POINT

MAXIMUM

LINEAR FEET

AD.	DREVIATIONS:		
AB AC BLDG BRDY BOT CB CL C/L Q CO DI DW ELEV EL EP EX (E) FC FD FF FG FDC FH FC FL F/L FL	AGGREGATE BASE ASPHALT CONCRETE BUILDING BOUNDARY BOTTOM CATCH BASIN CENTERLINE CLEAN OUT DRAIN INLET DRIVEWAY ELEVATION EDGE OF PAVEMENT EXISTING FACE OF CURB FACE OF DIKE FINISHED FLOOR FINISHED GRADE FIRE DEPARTMENT CONNECTION FIRE HYDRANT FACE OF CURB FICON LINE	SS LAT SSMH STA S/W	PUBLIC UTILITIES EASEMENT POLYVINYL CHLORIDE RADIUS RIGHT OF WAY RELATIVE COMPACTION REINFORCED CONCRETE PIPE RELATIVE RETAINING WALL SLOPE STORM DRAIN SHOULDER SANITARY SEWER SANITARY SEWER LATERAL SANITARY SEWER MANHOLE STATION SIDEWALK
FP FW	FINISHED PAD FACE OF WALL		
GB GR	GRADE BREAK GRATE	TD	TOP OF DIKE
UDDE	LUCH DENCITY DOLVETHELENE	TW	TOP OF WALL

TYPICAL UTILITY POLE

WATER WATER SERVICE

CONTACT INFORMATION:

PRIMARY: OWNER
MR. & MRS. OLIVER & BROOKE RICKARD 1304 SUITTON DRIVE COLUMBIA, MO 65203

SECONDARY: ARCHITECT INTERNATIONAL DESIGN GROUP ATTN: MS. AMY L. DENNEY 721 LIGHT HOUSE AVE. PEBBLE BEACH, CA 93950 PH (831)646-1261

YANKEE BEACH WAY CARMEL, CA 93923

SCALE: AS SHOWN DATE: OCTOBER 2024 JOB NO. 2761-02

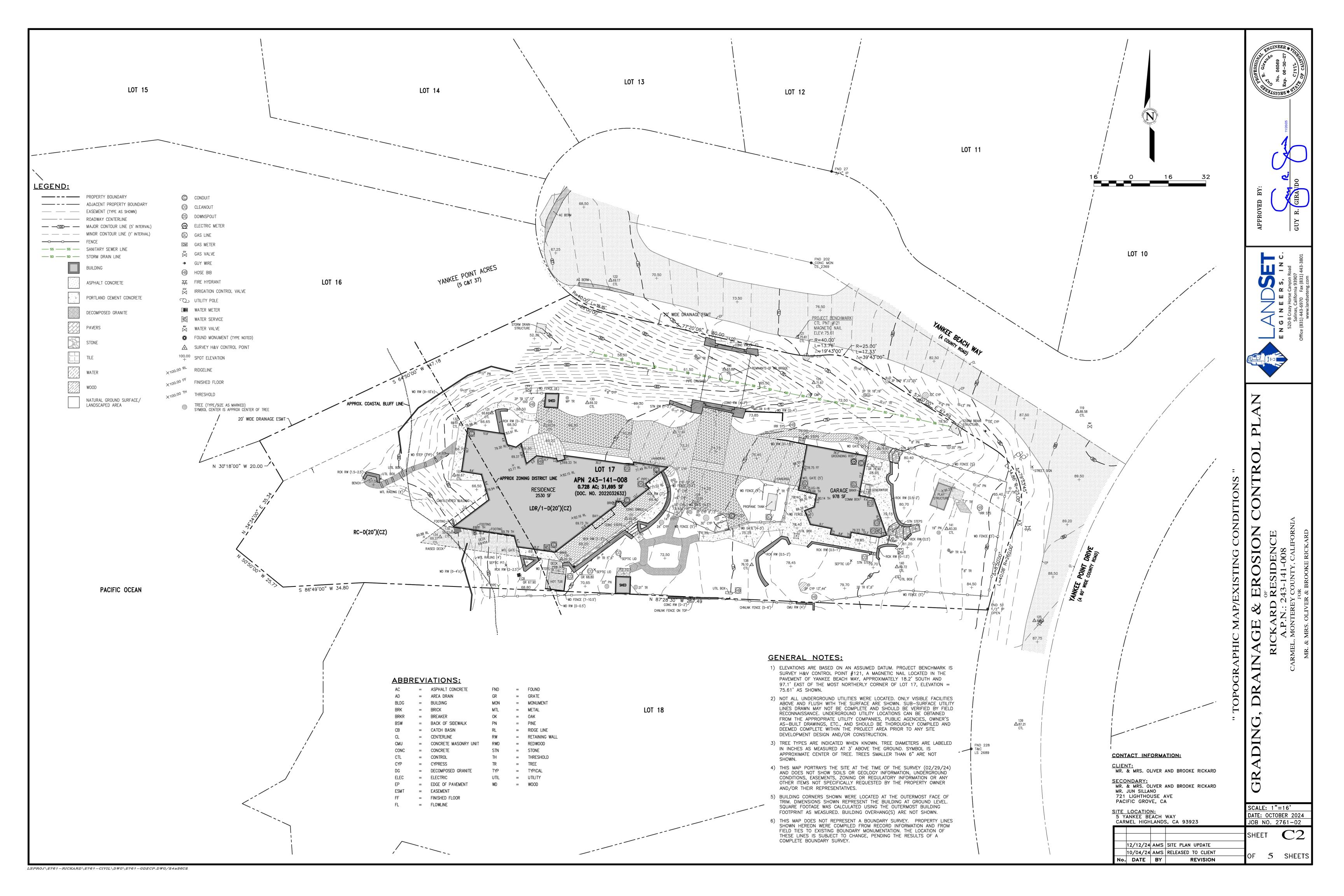
12/12/24 AMS SITE PLAN UPDATE 10/04/24 AMS RELEASED TO CLIENT No. DATE BY REVISION

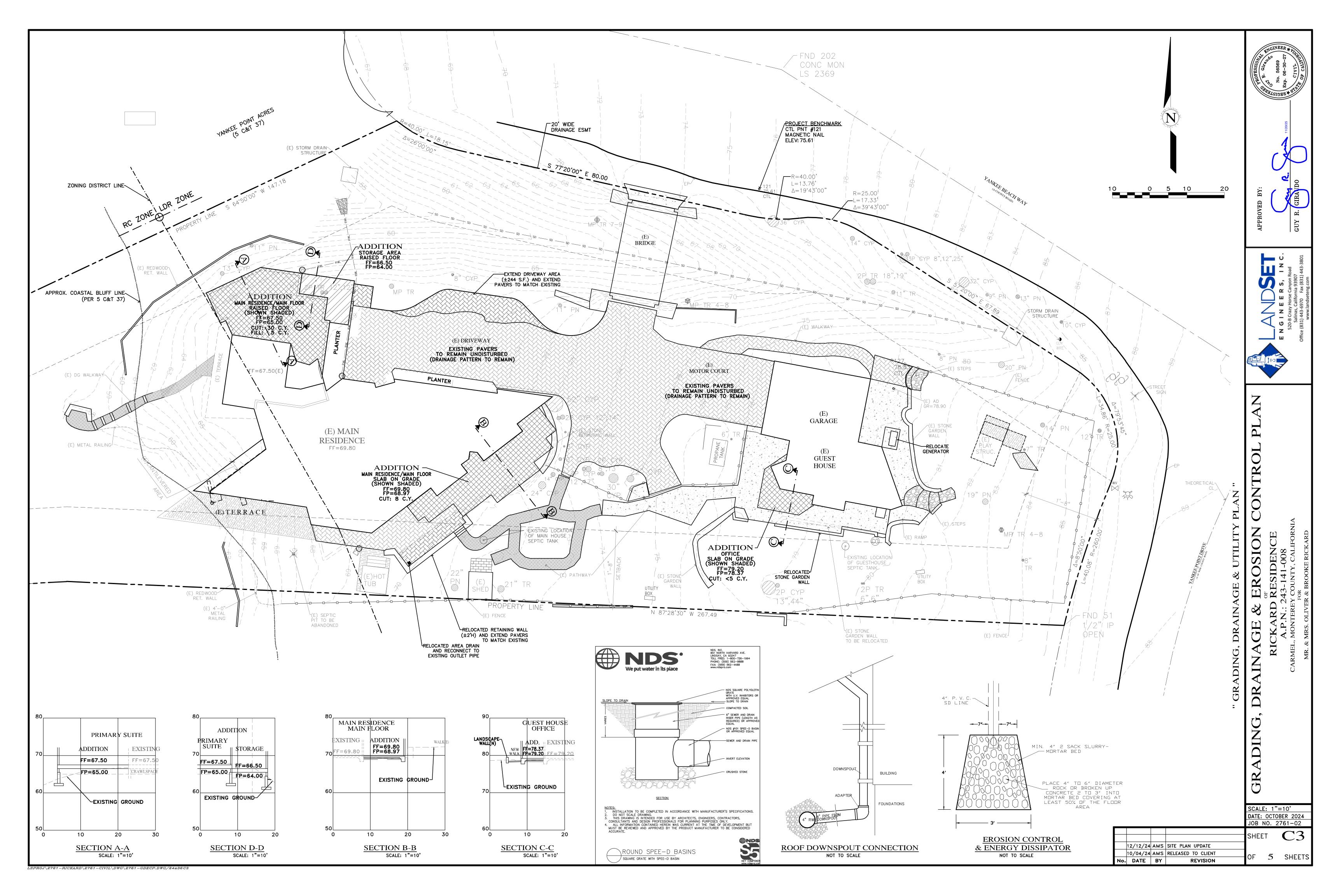
OF 5 SHEETS

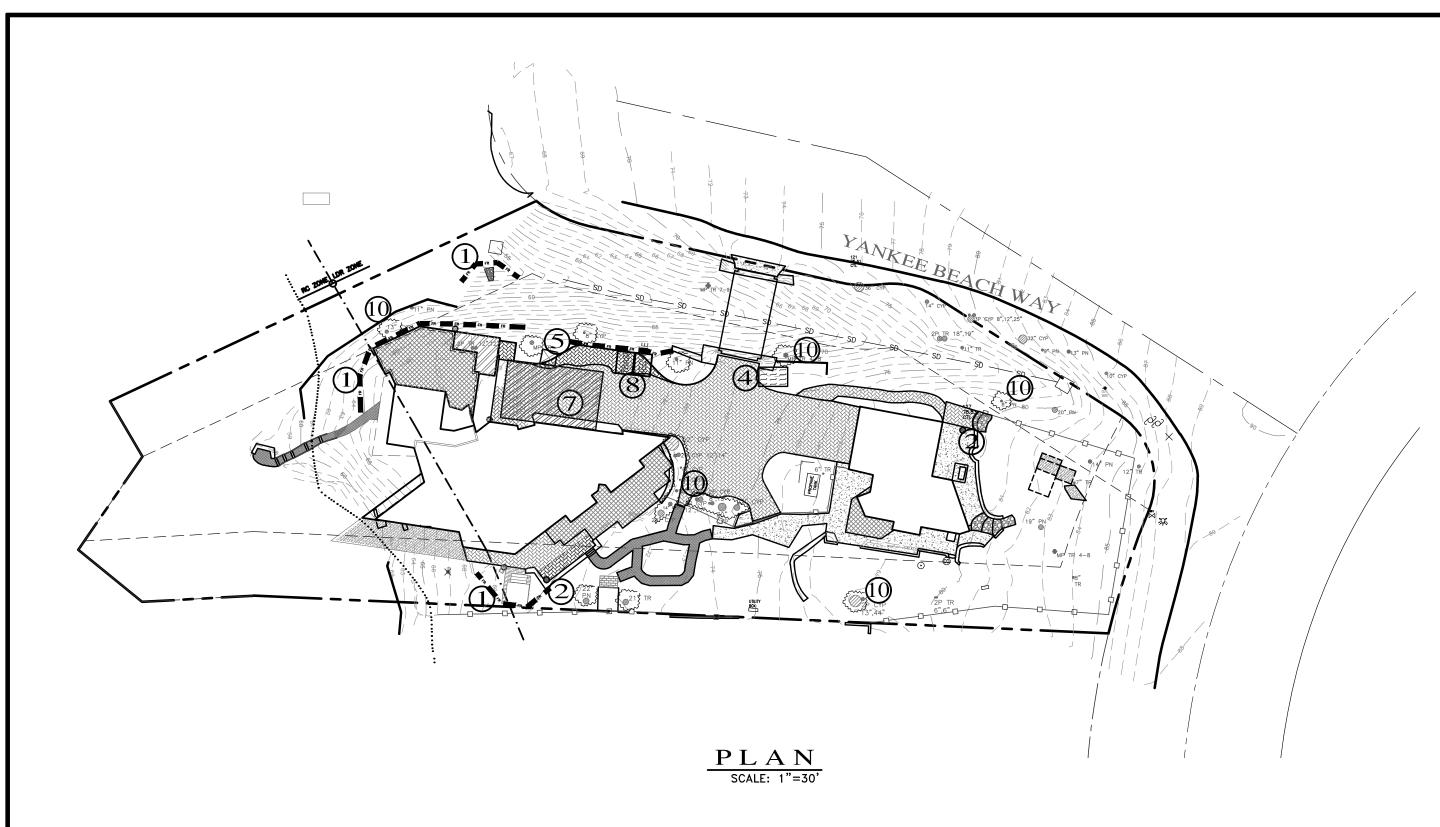
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Date completed:

J.H.







EROSION & SEDIMENT CONTROL NOTES:

- 1) ALL EROSION CONTROL MEASURES SHALL CONFORM WITH THE COUNTY OF MONTEREY EROSION
- 2) EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN EFFECT FOR ANY CONSTRUCTION DURING THE RAINY SEASON, APPROX. OCTOBER 15 TO APRIL 15. EROSION CONTROL PLAN SHALL BE PREPARED AND SUBMITTED FOR APPROVAL BY SEPT. 15 OF ANY OR EACH CALENDAR YEAR THAT CONSTRUCTION MAY EXTEND BEYOND OCTOBER 15.
- 3) ALL SLOPES SHALL BE PROTECTED WITH STRAW MULCH OR SIMILAR MEASURES TO PROTECT AGAINST EROSION UNTIL SUCH SLOPES ARE PERMANENTLY STABILIZED.
- 4) RUNOFF SHALL BE DETAINED OR FILTERED BY BERMS. VEGETATED FILTER STRIPS. AND/OR CATCH BASINS TO PREVENT THE ESCAPE OF SEDIMENT FROM THE SITE.
- 5) EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN PLACE AT THE END OF EACH DAY'S
- 6) EROSION CONTROL PLANTINGS AND MULCH SHALL BE CLOSELY MONITORED THROUGHOUT THE WINTER AND ANY RUNOFF PROBLEMS CORRECTED PROMPTLY. SEE LANDSCAPE ARCHITECT'S PLAN FOR PERMANENT PLANTINGS AND TREE SCHEDULES.
- 7) DISTURBED SURFACES NOT INVOLVED IN THE IMMEDIATE GRADING OPERATIONS MUST BE PROTECTED BY MULCHING AND/OR OTHER EFFECTIVE MEANS OF SOIL PROTECTION.
- 8) ALL ROADS AND DRIVEWAYS SHALL HAVE DRAINAGE FACILITIES SUFFICIENT TO PREVENT EROSION ON OR ADJACENT TO THE ROADWAY OR ON THE DOWNHILL PROPERTIES.
- 9) DRAINAGE CONTROL MEASURES SHALL BE MAINTAINED AND IN PLACE AT THE END OF EACH DAY AND CONTINUOUSLY THROUGHOUT THE LIFE OF THE PROJECT DURING WINTER OPERATIONS.
- 10) REVEGETATION SHALL CONSIST OF A MECHANICALLY APPLIED HYDROMULCH SLURRY OR HAND SEEDED WITH A STRAW MULCH COVER. MULCH SHALL BE ANCHORED BY AN APPROVED METHOD SUCH AS PUNCHING, TACKING, OR THE USE OF JUTE NETTING, AS DEEMED NECESSARY FOR THE SITE CONDITIONS TO ALLOW FOR GERMINATION AND ENABLE ADEQUATE GROWTH TO BE
- 11) CHECK DAMS, SILT FENCES, FIBER ROLLS OR OTHER DESIGNS SHALL BE INCORPORATED TO CATCH ANY SEDIMENT UNTIL AFTER THE NEWLY EXPOSED AREAS ARE REVEGETATED SUFFICIENTLY TO CONTROL EROSION, EROSION CONTROL PLANTINGS AND MULCH SHALL BE CLOSELY MONITORED THROUGHOUT THE WINTER AND ANY RUNOFF PROBLEMS SHALL BE CORRECTED PROMPTLY. ALL EROSION AND/OR SLIPPAGE OF THE NEWLY EXPOSED AREAS SHALL BE REPAIRED BY THE PERMITTEE AT THEIR EXPENSE.
- 12) THE GRASS SEED SHALL BE PROPERLY IRRIGATED UNTIL ADEQUATE GROWTH IS ESTABLISHED AND MAINTAINED TO PROTECT THE SITE FROM FUTURE EROSION DAMAGE. ALL NEWLY EXPOSED (DISTURBED) AREAS SHALL BE SEEDED WITH THE FOLLOWING EROSION CONTROL MIX: BROMUS CARINATUS (CALIFORNIA BROME), VULPIA MICROSTACHYS (NUTTALL'S FESCUE), ELYMUS GLAUCUS (BLUE WILD RYE), HORDEUM BRACHYANTHERUM (MEADOW BARLEY), FESTUCA RUNRA'MOLATE BLUE AND A MIXTURE OF LOCALLY NATIVE WILDFLOWERS.
- 13) THE DIRECTOR OF BUILDING INSPECTION (BUILDING OFFICIAL) SHALL STOP OPERATIONS DURING PERIODS OF INCLEMENT WEATHER IF HE OR SHE DETERMINÉS THAT EROSION PROBLEMS ARE NOT BEING CONTROLLED ADEQUATELY.
- 14) GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR EROSION AND SEDIMENT CONTROL BMP INSTALLATION AND MAINTENANCE AND SHALL PROVIDE FULL PARTICULARS TO COUNTY RMA-ENVIRONMENTAL SERVICES PRIOR TO BEG. WORK.

Material Delivery and Storage

Prevent, reduce, or eliminate the discharge of pollutants from

watercourses by minimizing the storage of hazardous materials

material delivery and storage to the stormwater system or

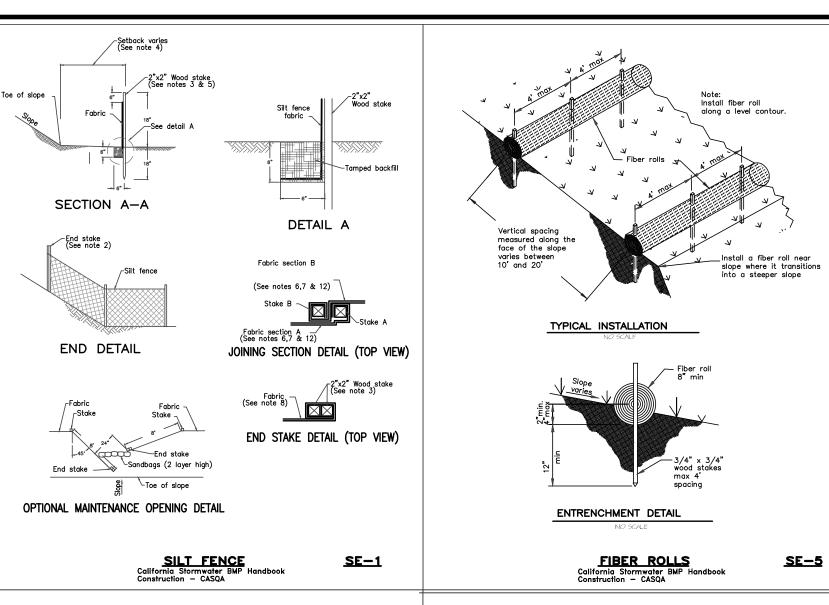
onsite, storing materials in watertight containers and/or a

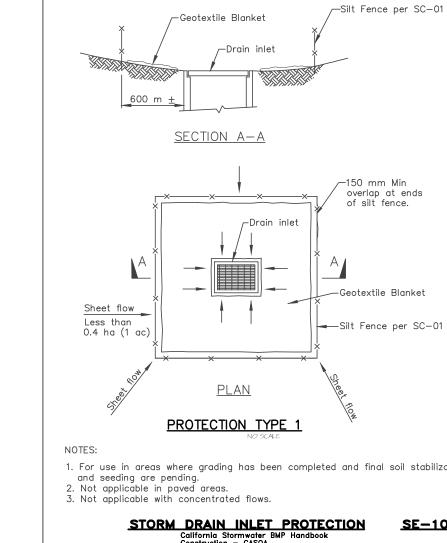
containment, conducting regular inspections, and training

completely enclosed designated area, installing secondary

Description and Purpose

employees and subcontractors.

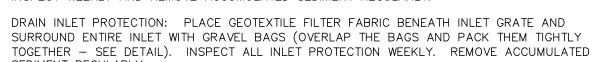


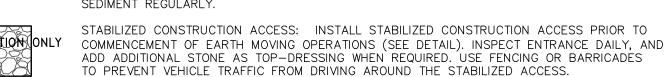


LEGEND:

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FIBER ROLLS: THE CONTRACTOR SHALL MAINTAIN A STOCKPILE OF FIBER ROLLS ONSITE, AS THEY CAN BE USED ALONG ERODIBLE SLOPES, ALONG STOCKPILE PERIMETERS, DOWNSLOPE OF EXPOSED SOIL AREAS, AND TO DELINEATE/PROTECT STAGING AREAS. FIBER ROLLS MUST BE TRENCHED INTO THE SOIL AND STAKED (STAKES SPACED MAX. 4' ON CENTER), SEE DETAIL. INSTALL FIBER ROLLS ALONG LEVEL CONTOURS, AND TURN THE ENDS UPHILL. INSPECT WEEKLY AND REMOVE ACCUMULATED SEDIMENT REGULARLY.





CONCRETE WASHOUT: WASHOUT MUST BE LOCATED A MINIMUM OF 50 FEET FROM STORM DRAINS, OPEN DITCHES, OR WATER BODIES. DISCONTINUE USE WHEN WASHOUT WASTES REACH 75% OF THE WASHOUT CAPACITY. ALLOW WASHOUT WASTES TO HARDEN, BE BROKEN UP, AND THEN DISPOSED OF PROPERLY.

SANITARY/SEPTIC WASTE MANAGEMENT: PORTABLE TOILETS WILL BE PROVIDED AND MAINTAINED ONSITE FOR THE DURATION OF THE PROJECT. ALL PORTABLE TOILETS WILL BE EQUIPPED WITH A SECONDARY CONTAINMENT TRAY, AND SHALL BE LOCATED A MINIMUM OF 50' FROM ALL OPERATIONAL STORM DRAIN INLETS. WEEKLY MAINTENANCE SHALL BE PROVIDED AND WASTES LEGALLY DISPOSED OF OFF-SITE

STOCKPILE MANAGEMENT: SOIL STOCKPILES MUST BE COVERED OR STABILIZED (I.E. WITH FOR NO PROPERTY SOIL BINDERS) IMMEDIATELY IF THEY ARE NOT SCHEDULED TO BE USED WITHIN 14 DAYS. ACTIVE SOIL STOCKPILES SHALL BE WATERED TWICE DAILY TO AVOID WIND EROSION. SURROUND ALL STOCKPILES WITH FIBER ROLLS OR SILT FENCE. STOCKPILES OF "COLD MIX", TREATED WOOD. AND BASIC CONSTRUCTION MATERIALS SHOULD BE PLACED ON AND COVERED WITH PLASTIC SHEETING OR COMPARABLE MATERIAL AND SURROUNDED BY A BERM..

> CONTRACTOR'S STAGING AREA: THE CONTRACTOR'S STAGING AREA SHALL BE SURROUNDED BY FIBER ROLLS. THE STAGING AREA WILL BE USED TO STORE DELIVERED MATERIALS, AND FOR OVERNIGHT EQUIPMENT PARKING/FUELING. STORED CONSTRUCTION MATERIALS SHALL BE MAINTAINED IN THEIR ORIGINAL CONTAINERS, AND COVERED AT ALL TIMES. PETROLEUM PRODUCTS AND HAZARDOUS MATERIALS SHALL BE STORED WITHIN SECONDARY CONTAINMENT STRUCTURES OR A STORAGE SHED. EQUIPMENT FUELING AND MAINTENANCE WILL ONLY OCCUR WITHIN THE DESIGNATED STAGING AREA. DRIP PANS OR ABSORBENT PADS MUST BE USED DURING ALL FUELING OR MAINTENANCE ACTIVITIES. AN AMPLE SUPPLY OF SPILL

CLEANUP MATERIALS SHALL BE MAINTAINED IN THE STAGING AREA AT ALL TIMES. WASTE MANAGEMENT: SOLID WASTES WILL BE LOADED DIRECTLY ONTO TRUCKS FOR TRASH OFF-SITE DISPOSAL. WHEN ON-SITE STORAGE IS NECESSARY, SOLID WASTES WILL BE STORED RECYCLE IN WATERTIGHT DUMPSTERS IN THE GENERAL STORAGE AREA OF THE CONTRACTOR'S YARD. DUMPSTERS AND/OR TRASH BINS SHALL BE COVERED AT THE END OF EACH WORK DAY. HAZARDOUS WASTES SHALL NOT BE STORED ONSITE. CONSTRUCTION DEBRIS AND GENERAL LITTER WILL BE COLLECTED DAILY AND WILL NOT BE ALLOWED NEAR DRAINAGE INLETS OR

> GRAVEL BAG CHECK DAM: GRAVEL BAGS SHALL CONSIST OF WOVEN POLYPROPYLENE, POLYETHYLENE OR POLYAMIDE FABRIC, MIN. UNIT WEIGHT OF 40Z/SY. BAGS SHALL BE A MINIMUM OF 18" LONG X 12" WIDE X 3" THICK, FILLED WITH 1/2" - 1" CRUSHED ROCK. TIGHTLY ABUT BAGS AND CONSTRUCT CHECK DAM AT LEAST 3 BAGS WIDE X 2 BAGS HIGH. INSPECT CHECK DAM REGULARLY AND REMOVE ACCUMULATED SEDIMENT.

TREE PROTECTION: TREE PROTECTION SHALL CONSIST OF ORANGE PLASTIC MESH FENCING, AND SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF EARTH-MOVING OPERATIONS (SEE DETAIL). INSTALL FENCING ALONG THE DRIP LINE OF TREES, AND INSTRUCT EMPLOYEES AND SUBCONTRACTORS TO HONOR PROTECTIVE DEVICES. TREE INJURIES SHALL BE ATTENDED TO BE A LICENSED AND CERTIFIED ARBORIST.

SILT FENCE: SILT FENCE SHALL CONSIST OF WOVEN GEOTEXTILE FABRIC WITH A MINIMUM WIDTH OF 36 INCHES. WOOD STAKES SHALL BE COMMERCIAL QUALITY LUMBER, SPACED A FOR INFORMATION ONLY MAXIMUM OF 6' APART AND DRIVEN SECURELY INTO THE GROUND (SEE DETAIL). FENCING FABRIC SHALL BE KEYED INTO THE SOIL AS PER MANUFACTURER'S RECOMMENDATIONS. INSTALL SILT FENCE ALONG LEVEL CONTOURS. TURN THE ENDS OF THE SILT FENCE UPHILL TO PREVENT WATER FROM FLOWING AROUND THE FENCE. INSPECT SILT FENCE DAILY, AND

MAKE REPAIRS IMMEDIATELY.

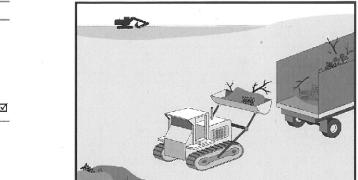
### TABLE 1706 6 REQUIRED VERIFICATION AND INSPECTION OF SOILS

| TABLE 1706.6 REQUIRED VERIFICATION AND INSPECTION OF SOILS                                                          |                                     |                                       |  |
|---------------------------------------------------------------------------------------------------------------------|-------------------------------------|---------------------------------------|--|
| VERIFICATION AND INSPECTION TASK                                                                                    | CONTINUOUS<br>DURING TASK<br>LISTED | PERIODICALLY<br>DURING TASK<br>LISTED |  |
| 1. Verify material below shallow foundations are adequate to achieve the design bearing capacity                    |                                     | Х                                     |  |
| 2. Verify excavations are extended to proper depth and have reached proper material                                 |                                     | X                                     |  |
| 3. Perform classification and testing of compacted fill materials                                                   |                                     | X                                     |  |
| 4. Verify use of proper materials, densities and lift sicknesses during placement and compaction of compacted fill. | Х                                   |                                       |  |
| 5. Prior to placement of compacted fill, observe subgrade and verify that site has been prepared properly.          |                                     | Х                                     |  |

CONSTRUCTION INSPECTION REQUIREMENTS

- A-PRIOR TO COMMENCEMENT OF ANY LAND DISTURBANCE, THE OWNER/APPLICANT SHALL SCHEDULE AN INSPECTION WITH HCD-ENVIRONMENTAL SERVICES TO ENSURE ALL NECESSARY SEDIMENT CONTROLS ARE IN PLACE AND THE PROJECT IS COMPLIANT WITH MONTEREY COUNTY GRADING AND EROSION CONTROL REGULATIONS.
- B-DURING CONSTRUCTION THE OWNER/APPLICANT SHALL SCHEDULE AN INSPECTION WITH HCD-ENVIRONMENTAL SERVICES TO UPDATE COMPACTION TEST RECORDS, INSPECT DRAINAGE DEVICE INSTALLATION, REVIEW THE MAINTENANCE AND EFFECTIVENESS OF BMP's INSTALLED, AS WELL AS, TO VERIFY THAT POLLUTANTS OF CONCERN ARE NOT DISCHARGED FROM THE SITE.
- C-PRIOR TO FINAL INSPECTION, THE OWNER/APPLICANT SHALL SCHEDULE AN INSPECTION WITH HCD-ENVIRONMENTAL SERVICES TO CONDUCT A FINAL GRADING INSPECTION, COLLECT FINAL GEOTECHNICAL LETTER OF CONFORMANCE, ENSURE THAT ALL DISTURBED AREAS HAVE BEEN STABILIZED AND THAT ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES THAT ARE NO LONGER NEEDED HAVE BEEN REMOVED.

### Solid Waste Management



**Description and Purpose** and training employees and subcontractors.

**Description and Purpose** 

arranging for regular service and disposal.

Proper sanitary and septic waste management prevent the

discharge of pollutants to stormwater from sanitary and septic

waste by providing convenient, well-maintained facilities, and

This best management practice covers only material delivery and storage. For other information on materials, see WM-2, Potential Alternatives Material Use, or WM-4, Spill Prevention and Control. For information on wastes, see the waste management BMPs in this

WM-1

Erosion Control

C Tracking Control

WE Wind Erosion Contro

Primary Category

Oil and Grease

Secondary Category

Non-Stormwater

Management Control

Waste Management and

**Targeted Constituents** 

Materials Pollution Control

SE Sediment Control

Solid waste management procedures and practices are designed to prevent or reduce the discharge of pollutants to stormwater from solid or construction waste by providing designated waste collection areas and containers, arranging for regular disposal,

Sanitary/Septic Waste Management WM-9

Potential Alternatives

Categories

EC Erosion Control

SE Sediment Control

TC Tracking Control

WE Wind Erosion Control

Primary Category

Secondary Category

Non-Stormwater

Management Control

Waste Management and

Targeted Constituents

**Potential Alternatives** 

Oil and Grease

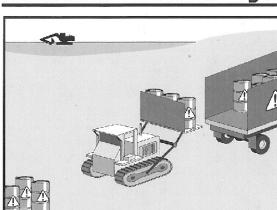
Organics

Materials Pollution Control

Oil and Grease

Organics

**Targeted Constituents** 



**Description and Purpose** Prevent or reduce the discharge of pollutants to stormwater from hazardous waste through proper material use, waste disposal, and training of employees and subcontractors.

Nutrients Oil and Grease

Potential Alternatives

EC Erasian Control

SE Sediment Control

TC Tracking Control

NS Non-Stormwater

WE Wind Erosion Control

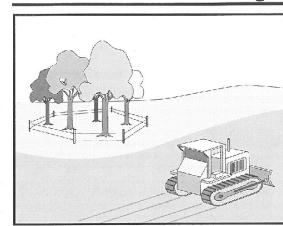
☑ Primary Objective

Secondary Objective

Management Control

Waste Management and

Materials Pollution Control

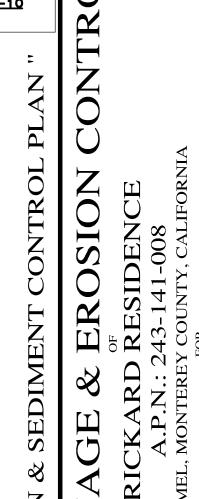


**Description and Purpose** Carefully planned preservation of existing vegetation minimizes the potential of removing or injuring existing trees, vines,

**Targeted Constituents** Metals Oil and Grease

> SCALE: AS SHOWN DATE: OCTOBER 2024 JOB NO. 2761-02

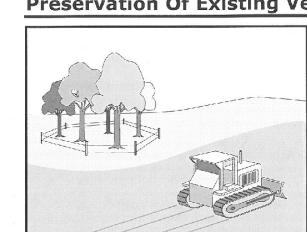
12/12/24 AMS SITE PLAN UPDATE 10/04/24 AMS RELEASED TO CLIENT No. DATE BY REVISION



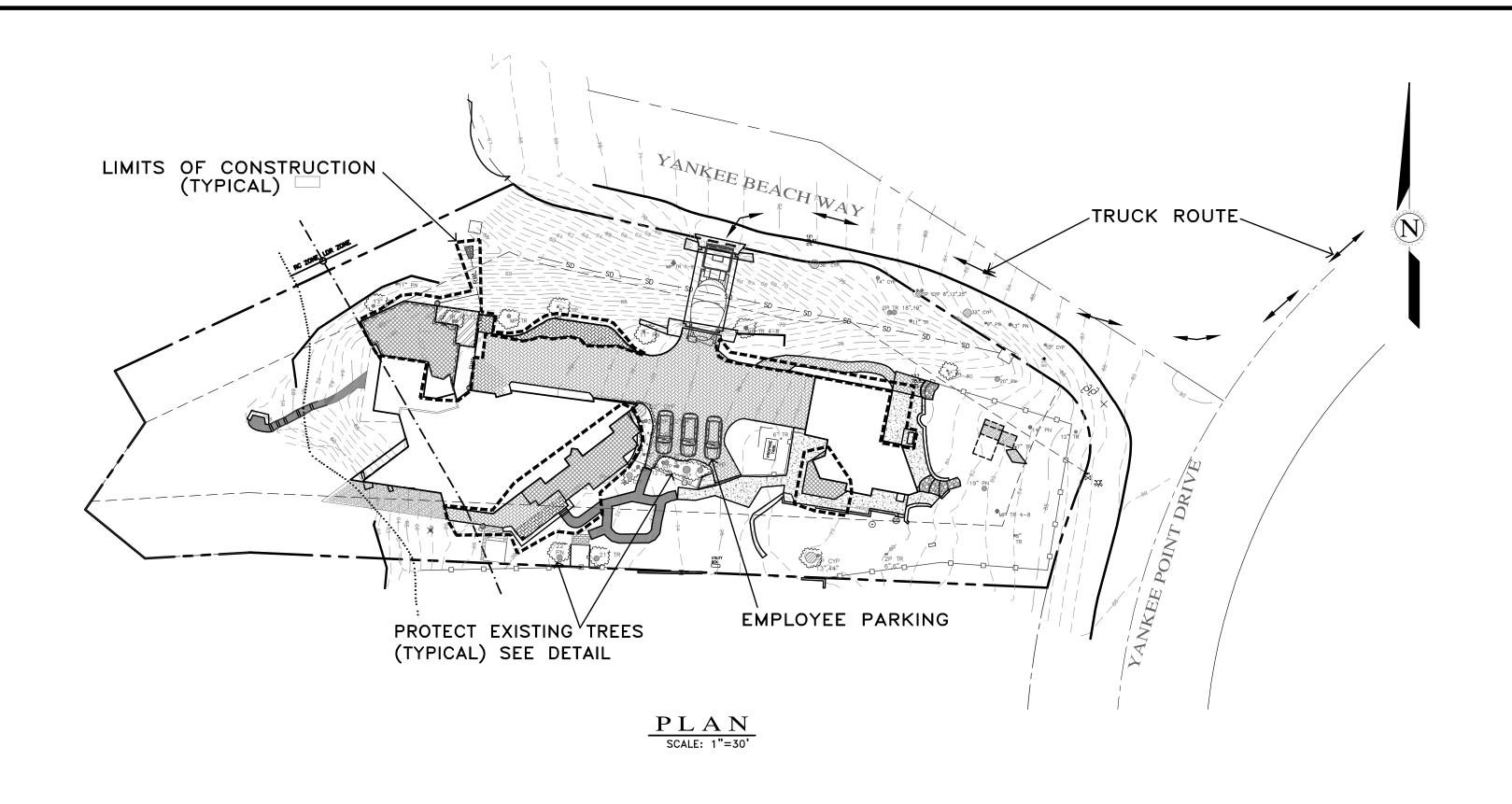
1. For use in areas where grading has been completed and final soil stabilizatio WM-6 **Hazardous Waste Management** EC Erosion Control SE Sediment Control SE Sediment Control TC Tracking Control TC Tracking Control WE Wind Erosion Control WE Wind Erosion Control Non-Stormwater Management Control Waste Management and Waste Management and Materials Pollution Control Materials Pollution Contro ☑ Primary Objective Primary Objective Secondary Objective

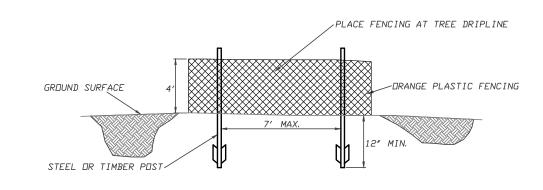
■ Secondary Objective **Targeted Constituents** 

**Preservation Of Existing Vegetation EC-2** 



shrubs, and grasses that protect soil from erosion.





FENCING (ESA) DETAIL

EARTHWORK QUANTITIES PER CIVIL ENGINEERING PLANS BY LANDSET ENGINEERS, INC.: 15 CY CUT

15 CY FILL

DEMOLISH EXISTING HARDSCAPE AND OFFHAUL DEBRIS: EXISTING DRIVEWAY TO BE USED FOR EQUIPMENT STAGING AND TEMPORARY STOCKPILE AREA IF REQUIRED.

PERFORM MINOR GRADING, CONSTRUCT STRUCTURE ADDITIONS, AND INSTALL UNDERGROUND UTILITIES: EXISTING DRIVEWAY AREA TO BE USED FOR MATERIAL AND EQUIPMENT STAGING.

### INSTALL NEW LANDSCAPING.

SEE ARCHITECTURAL AND CIVIL PLANS FOR EROSION CONTROL AND DEMOLITION NOTES.

CONSTRUCTION EQUIPMENT AND MATERIALS SHALL NOT BE STAGED ON YANKEE BEACH WAY AT ANY TIME DURING CONSTRUCTION. MATERIAL DELIVERIES SHALL BE SCHEDULED SUCH THAT THEY ARE USED PROMPTLY, AND MATERIAL STORAGE IS MINIMIZED. ALL CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE STORED IN A DESIGNATED AREA ON THE SUBJECT PROPERTY.

THE HAUL ROUTE TO THE SITE IS FROM HIGHWAY 1 TO YANKEE POINT DRIVE TO YANKEE BEACH WAY. (HAUL TRUCKS EXIT IN THE SAME FASHION.) VEHICLES SHALL NOT BE LEFT UNATTENDED WHILE IN QUEUE (IF NECESSARY) ON YANKEE BEACH WAY. CONTRACTOR TO ENSURE THAT HEIGHT RESTRICTIONS WITHIN THE DRIVEWAY AREA SHALL BE ADDRESSED BEFORE CONSTRUCTION VEHICLES ENTER THE SITE. SEE DETAILS B AND C, TRUCK ROUTING PLANS.

IN THE EVENT THAT MATERIAL DELIVERIES CAUSE ANY STREETS ALONG THE HAUL ROUTE TO BE PARTIALLY BLOCKED BY DELIVERY TRUCKS OR LOADING/UNLOADING OPERATIONS, A FLAGMAN SHALL BE PRESENT TO DIRECT TRAFFIC AROUND THE LANE OBSTRUCTION. THE FLAGMAN SHALL BE PRESENT AT ALL TIMES DURING WHICH DELIVERY/CONSTRUCTION OPERATIONS MAY IMPACT TRAFFIC ON THE HAUL ROUTE AND SURROUNDING STREETS.

LIMITED EMPLOYEE PARKING ON-SITE. EMPLOYEES SHALL USE PUBLIC PARKING LOTS AND CARPOOL TO JOBSITE IF POSSIBLE. ON-SITE PARKING SHALL BE IN LEGAL SPACES ALONG YANKEE BEACH WAY, OBEYING ALL PARKING LAWS. PARKING IS PROHIBITED IN ALL NATURAL AREAS WHICH ARE NOT CURRENTLY PAVED OR GRAVEL.

<u>LIMITS OF CONSTRUCTION:</u> ALL CONSTRUCTION SHALL TAKE PLACE WITHIN THE BORDER AS SHOWN. EXISTING CYPRESS, PINE, AND OAK TREES LOCATED WITHIN THE LIMITS SHOWN SHALL BE SURROUNDED BY ORANGE PROTECTIVE FENCING (SEE DETAIL).

### TRUCK TRIP GENERATION CHART:

| CATEGORY                           | NO. OF TRUCK<br>TRIPS | TOTAL DAYS |
|------------------------------------|-----------------------|------------|
| DEMOLITION/CLKEARING               | 4                     | 2          |
| GRADING & SOIL<br>REMOVAL (EXPORT) | 0                     | 0          |
| ENGINEERING<br>MATERIALS (IMPORT)  | 1                     | 1          |
| TOTALS                             | 5                     | 3          |

### 8TRUCK TRIP GENERATION NOTES:

1. TRUCK TRIPS FOR THE GRADING/SOIL REMOVAL IS BASED UPON 8 CUBIC YARDS PER TRUCKLOAD WITH AN AVERAGE OF 5 TRUCK LOADS PER DAY. 2. THERE IS NOT SURPLUS SOIL MATERIAL THAT WILL BE EXPORTED OFF THE

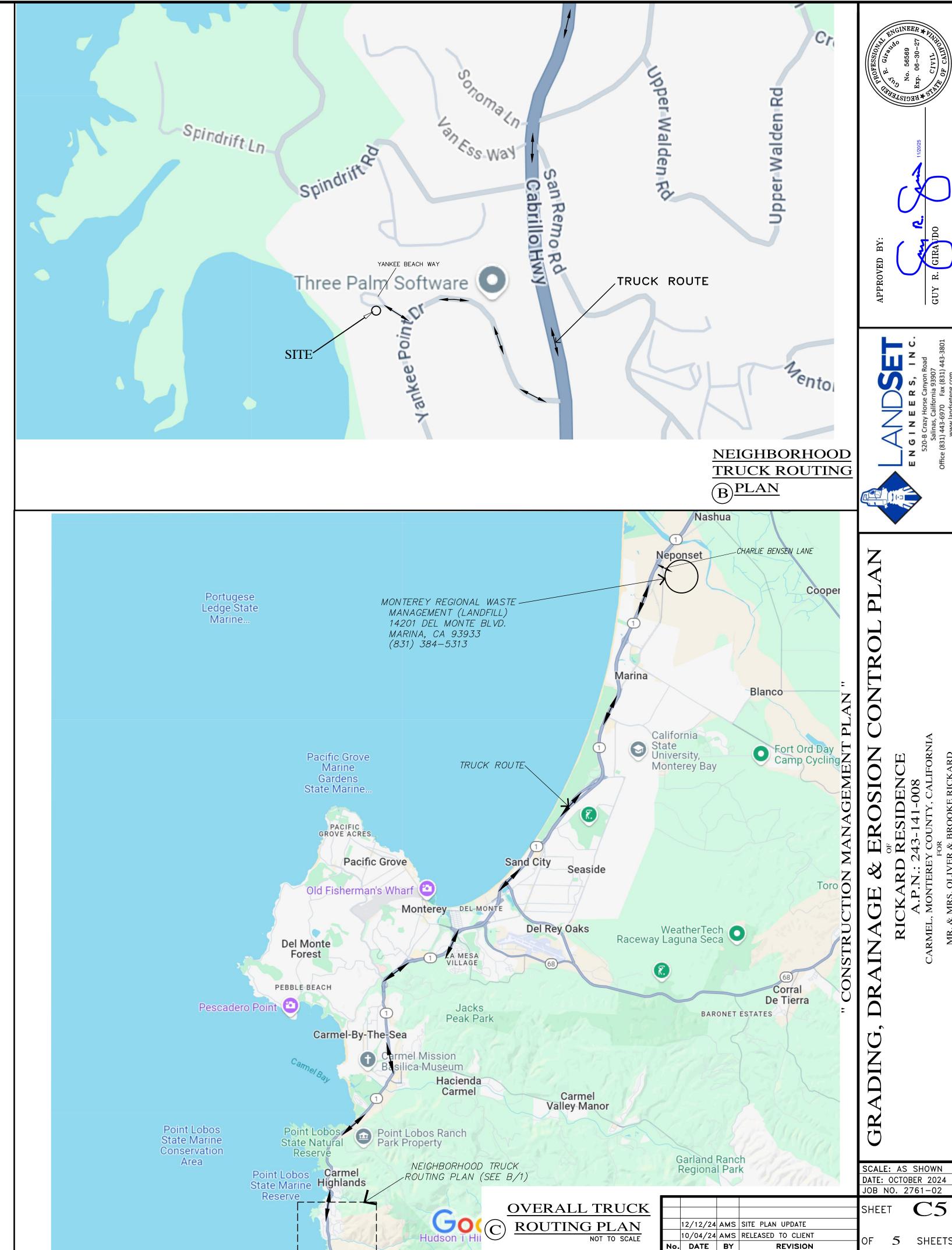
3. GRADING OPERATIONS SHALL TAKE APPROXIMATELY 3 WORKING DAYS TO

4. THE AMOUNT OF GRADING PER DAY WILL VARY, THE AVERAGE BETWEEN 10 & 50 CUBIC YARDS.

### NUMBER OF EMPLOYEES/DAY: 4-10

### HOURS OF OPERATION/DAY: 8

PROJECT SCHEDULING: PROJECTED START DATE 27 JANUARY 2025, 2 WORKING DAYS TO COMPLETE GRADING, MONDAY THRU FRIDAY, 8:00 A.M. - 4:30 P.M. TOTAL PROJECT DURATION IS APPROXIMATELY



### **COLOR AND MATERIAL SAMPLES FOR**

### RICKARD RESIDENCE

5 Yankee Beach Way Carmel, CA APN: 243-141-008

### ALL COLORS AND MATERIALS TO MATCH EXISTING



MAIN HOUSE



GARAGE AND GUESTHOUSE

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